

# THE KINGSOUEL CROSSING (CORRIDOR) COMMUNITY REDEVELOPMENT PLAN

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# KingSoutel Crossing (Corridor) Community Redevelopment Plan

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## Introduction

### Overview

The KingSoutel Community Redevelopment Area is located in the northwest planning area of the City of Jacksonville. (*Refer to Map 1, "Location Map"*). The redevelopment area is located on parcels surrounding the New Kings Road I-295 interchange and the Pritchard Road and I-295 interchange as well as the parcels in between the two (2) I-295 interchanges which front and surround New Kings Road, Soutel Road, and Pritchard Road. Additionally, the redevelopment area includes parcels surrounding Soutel Boulevard to the east of New Kings Road, extending to Norfolk Boulevard.

The KingSoutel Crossing (Corridor) - Community Redevelopment Plan had its beginning in the Northwest Jacksonville Vision Plan. Area residents welcomed opportunities for more restaurants, quality housing choices, and thriving retail streets, while retaining the friendliness, history, and sense of community that sets their neighborhoods apart. The guiding themes of the Northwest Vision Plan are as follows:

- Create attractive gateways that establish a quality of character and identity;
- Create new town centers at key locations with access to transit; and
- Strengthen existing neighborhoods.

A further impetus for redevelopment planning was the 2003 Social Compact Study prepared by the Jacksonville Economic Development Commission ("JEDC") and Jacksonville Chamber of Commerce. In evaluating the predominantly African-American northwest area, a DrillDown Analysis based upon home values/ownership, development patterns and income was performed using a non-traditional method. The DrillDown Analysis indicates that real buying power exists in the study area as well as the surrounding Lincoln Villas and Sherwood Neighborhoods. The major conclusion of the study found that the published data probably significantly undercounted the "actual" purchasing power.

A Buxton Report was conducted in 2005 to ascertain the "potential for designated areas to attract new retailers and restaurants". Of three (3) sites examined, the intersection of Pritchard and Old Kings Road possessed the market with the highest number of estimated households as well as total number of dominant segment households. Stores and businesses not present in the immediate neighborhood include general merchandise, apparel & accessory, and home furniture, furnishings & equipment stores. Missing business services are bank and legal services. Existing home improvement businesses, food stores, business services, motion picture services, and amusement services are significantly smaller than the typical sized establishment found in the larger 5-mile radius. This indicates a latent demand for larger and generally more price competitive stores and services.

## KingSoutel Crossing (Corridor) Community Redevelopment Plan

The City of Jacksonville (“City”) is widely known for its emphasis on neighborhood renewal and preservation as well as citizen based approach to planning. The nearby Lincoln Villas Sherwood Forest Neighborhood Action Plan (Ordinance No. 2008-78-E) is important. The plan recognizes that local commercial areas also need renewal and new retail is needed along the corridor for neighborhood programs to be successful.

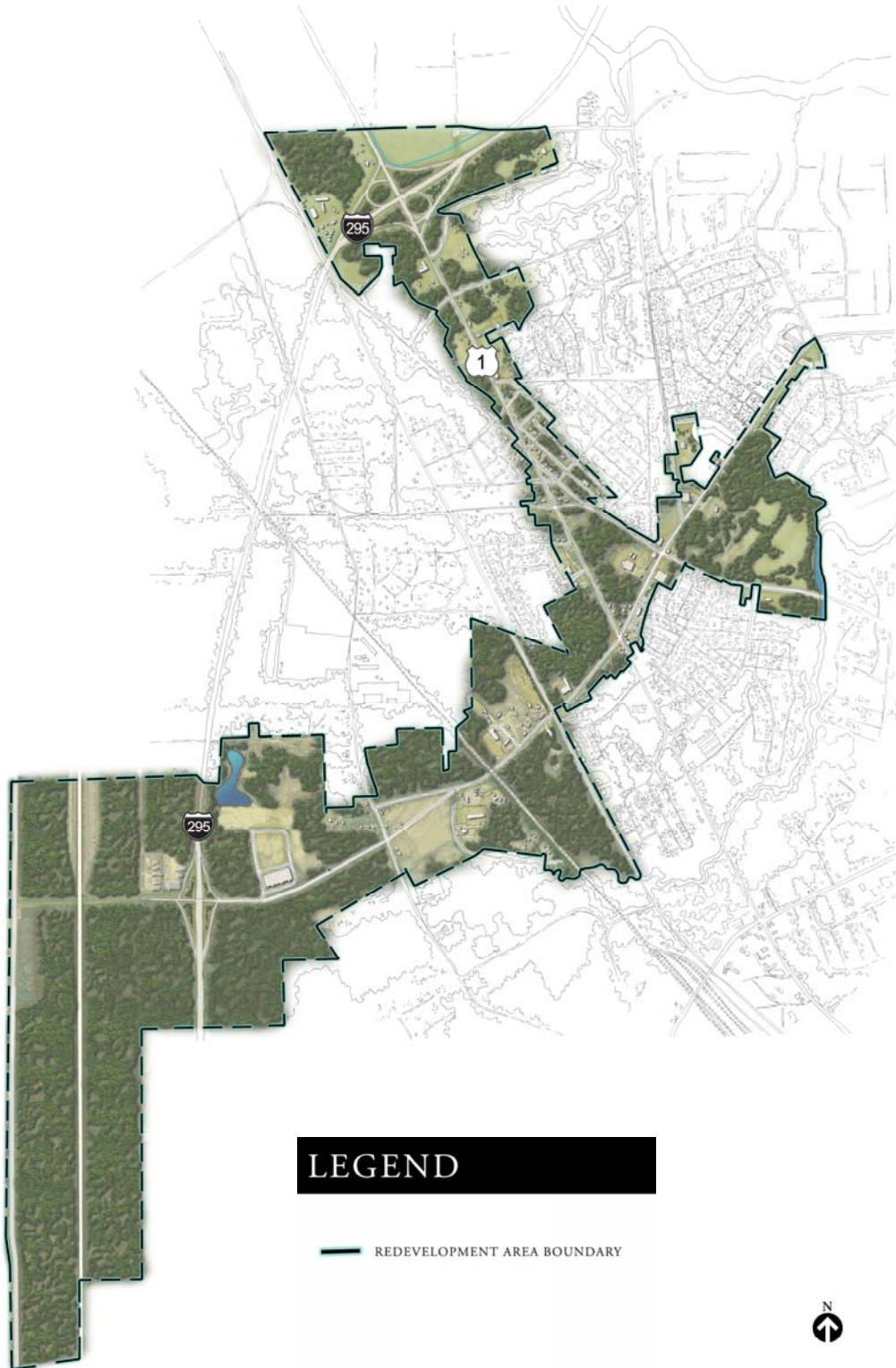
As a result of these previous efforts, the City initiated detailed evaluations of the potential corridor redevelopment to determine the following:

- Do blighting conditions support Florida Statutory requirements for creating a redevelopment program?
- Are market forces sufficient to support a successful redevelopment program?

**Map 1. – Location Map**



Map 2. – KingSoutel Crossing Redevelopment Area



### Authority for Redevelopment

The City of Jacksonville has extensive experience in managing redevelopment programs. New redevelopment programs must be established under the *Florida Statutes, Section 163, Part III, Community Redevelopment Act (“Act”)*. The Act sets forth the legal process by which the City may establish a community redevelopment agency, and provide financing and regulatory tools to accomplish the goal of improving a blighted area by either eliminating or reducing blight. The Act further requires that The City Council make a determination, based upon objective evidence, that the subject area meets the definition of a “blighted area” and certain specific statutory criteria.

The blighting conditions for the project area were professionally evaluated. Two (2) “Finding of Necessity” (Blight Analysis) Reports were presented to the City Council and provided the objective basis for adoption of “Resolutions of Necessity”. The City Council established a “Finding of Necessity” for the Soutel/Moncrief Retail Redevelopment Area, Resolution 2006-357-A, on 05/09/06. Subsequently, the City expanded the potential redevelopment area and established a “Finding of Necessity” for the New Kings Road/Soutel/Pritchard Road Corridor Area, Resolution 2007-726-A, on 07/24/07. In adopting the Resolutions the City Council established the following:

- Based on thorough analysis of existing conditions within the two (2) areas, the conditions of blight exist as they relate to specific statutory criteria.
- The rehabilitation, conservation, or redevelopment, or a combination thereof, of these areas is necessary in the interest of public health, safety, morals, or welfare of the residents of the area.

These reports and associated City Council Resolutions are hereby incorporated, by reference, into this plan.

- Please refer to Appendix 1. Soutel/Moncrief Redevelopment Retail Area
  - Finding of Necessity (Blight Analysis) Report
  - Finding of Necessity Resolution
- Please refer to Appendix 2. New Kings Road Corridor Area
  - Finding of Necessity (Blight Analysis) Report
  - Finding of Necessity Resolution



## Corridor Redevelopment Issues

The Community Redevelopment Plan must be structured to eliminate or reduce blight conditions through implementation of its goals, objectives, programs, and projects.

### Soutel/Moncrief Retail Redevelopment Area

The approved Finding of Necessity for the “Soutel/Moncrief Retail Redevelopment Area identified the following issues:

- Deterioration of site or other improvements: Deteriorated conditions include building/structural conditions and developed site conditions.
- Government Owned Property: Large parcels of government-owned property currently used for park purposes have adverse brownfield issues resulting in underutilization.
- Assessed real property with adverse property values of real estate in the area have failed to show any appreciable increase.
- Residential and commercial vacancy rates are higher than the City total.
- Incidence of crime in the area is higher than the City total.

### New Kings Road Corridor

The approved Finding of Necessity for the New Kings Road Corridor identified the following issues:

- Inadequate street layout, roadways, or public transportation facilities include:
  - Lack of sidewalks (along the corridor and to the neighborhoods).
  - Inadequate bus shelters.
  - The Pritchard Road and Soutel Road off-set roadways that fail to meet modern standards and old platted roads that have been bisected by New Kings Road, thereby creating unusable parcels.
- Faulty lot layout combined with diversity of ownership include:
  - Substandard lot/parcel sizes exist that inhibit development meeting modern market requirements.
  - Inadequate sized parcels in relation to requirements of current development codes.
  - When combined with diversity of parcel ownership, assemblage of parcels that would create catalyst projects is inhibited.
- Deterioration of site or other improvements include:
  - Deteriorated conditions include building/structural conditions.
  - Developed site conditions (parking lots, signage, drainage, landscaping) and deficiencies (including potable water, sewer, drainage, and fire hydrant spacing) all lead to unsafe and unsanitary conditions.

- Lack of growth in tax base include:
  - Loss of assessed and market value in properties exhibited by vacant deteriorating buildings as well as undeveloped and/or underdeveloped properties.
- Deterrents to sound future growth and development:
  - Economic disuse.
  - Property assemblage problems.
  - Visual blight as deterrent to investment.

### Boundary Determination

The “KingSoutel Crossings” Community Redevelopment Area Plan combines the two (2) blighted areas. The corridor runs from the I-295 and New Kings Road Interchange Area on the north, south to the New Kings Road and Soutel intersection area, east to the Soutel and Norfolk Boulevard intersection area, and west to the I-295 Pritchard Road Interchange Area. (*Refer to Map 1. “The KingSoutel Crossing Redevelopment Area Map”.*) (*Refer to Appendix 3 “Redevelopment Area Legal Description”.*)

The boundary of the KingSoutel Crossing Redevelopment Area was determined after careful evaluation of much larger areas surrounding the corridor. Final determination for parcels to be included in the boundary was based upon the following criteria:

- Statutory criteria pertaining to site conditions.
- Consideration of future corridor redevelopment potential that would assist in the elimination or reduction of blight. I-295 provides intermodal access that makes the area suitable to capture economic impacts of the port.
- Consideration of sound planning principles for the continuity of future land use based on land use pattern and potentials, the transportation system, and efficient provision of government services and facilities.
- Consideration of irregular, small, or surrounding parcels that might, at some time, be assembled for a larger more efficient use of land.
- Locations providing a logical terminus for the boundaries, such as roads and property lines, reflecting potential accessibility and marketability for quality redevelopment or development.

### Approach to Corridor Redevelopment

Because corridor redevelopment programs cover lengthy stretches of roadway, it is often difficult to select where to begin investing limited public resources. MSCW reviewed successful corridor redevelopment programs in communities around the country. This review has provided some “lessons learned” as to how other communities have grappled with the difficult issues involved with revitalizing their suburban roadway corridors.

The findings are summarized as follows:

- Adoption of a “Graphic” Corridor Vision.
- Plan Organization was based upon Character Districts.
- Market Analysis identified locations for “Placemaking”/Catalyst Projects.
- Public investment priorities focused on leveraging Catalyst Projects.
- Public investments “set the table” for private investments and included:
  - Streetscaping at key locations.
  - Landscaped medians, where appropriate.
  - Gateway features.
- Improved pedestrian connectivity to neighborhoods.
- Improved bus transit to the larger community.
- Traffic “taming” and calming of traffic at key locations.
- Adoption of design themes and concepts for both public and private projects.
- Design standards for private “Catalyst/Placemaking” projects and included:
  - Mixed-Use - Success is in the mix of uses.
  - Pedestrian Orientation - Main street/village layout

## Market and Potential Projects

### Overview

An Economic Market Analysis was performed for the KingSoutel Crossing Corridor building on the Social Compact – Drilldown Study and the Buxton Study. Along the corridor there are differing types of markets including highway, regional, community and neighborhood. The Market Analysis was performed as a snapshot of a point in time. Obviously, market conditions have changed because of unprecedented adverse global financial impacts that have dramatically altered development potential. We have experienced such impacts in the past. Florida has always recovered and the pace of development will increase yet again. Thus, the projections stated herein should be viewed accordingly. There may be another one (1) or two (2) years until a recovery begins. The pace of recovery may not be aggressive. We suggest the projections contained herein be viewed with a two (2) to three (3) year lag for residential, office, and retail. Obviously, the City of Jacksonville is benefiting from the expansion of the port and the increased demand for industrial-related facilities.

The two (2) I-295 Interchanges at the north and southwest boundaries of the corridor are primarily regional markets serving a larger number of households within a 15-minute drive time area. A combined analysis of the interchange area indicates that there are approximately 111,000+ households in that market area with an additional 5,000+ by 2010. Locations along the corridor at major road intersections may also serve some regional markets.

Beyond the interchange influence areas, the corridor predominantly can serve a regional market at the intersection of major roads and also serves a more local (community and neighborhood) market. A point was selected along the corridor to allow evaluation of 5-mile radius to determine the size of this market. Table 1. shows the current and projected population and associated household data for the defined market area. Households and median incomes are expected to grow significantly.

**Table 1. - Local Market Population, Households and Income**

	2007	*2012	*2020
Population	133,651	143,669	161,625
Households	50,687	54,927	62,163
Estimated New Households	0	4,240	7,236
Median Household Income	\$31,128	\$35,460	\$44,227

\* Note: Add three (3) years to projections.

## Housing Demand

There is a significant demand for rental and ownership housing along the corridor that could be incorporated into mixed-use projects.

- By 2012 (to 2015), approximately 1,275 new households can afford to purchase a house of over \$178,000 or rent an apartment. Another 2,600 households can meet this requirement by 2020 (to 2023). Single-family or condo units will have an expected demand for over 3,200 new units by 2020 (to 2023).

## Retail and Entertainment Demand

There is a large and growing retail and entertainment demand for interchange influence areas and major intersections for regional projects. The demand for certain big box uses is especially strong. The community and neighborhood retail demand is also present.

- In the corridor market area, households are significantly underserved by retail and entertainment services. It is recognized, by retailers, that area residents will drive farther for goods and services, but they desire more local businesses.
- For the interchange and regional market, based upon projected growth and national spending averages, by 2020 (to 2023) over 1.16 million new square feet of retail space will be needed to serve the growing population.
- In addition to pent up demand, the potential new demand for community and neighborhood retail generated by growth is approximately 175,000 square feet by 2012 (to 2015) and an additional 297,366 square feet by 2020 (2023).

## Local Serving Office Space

Local serving offices are the professional and business services typically driven by population growth. This includes jobs such as insurance agents, real estate agents, financial planners, small practice medical, dental offices, etc.

- The existing market is underserved. The demand for office in the regional market in this area could be over 600,000 additional square feet by 2012 (to 2015), and much more by 2020 (to 2023).
- Blight and perception has dramatically hindered office development growth and must be addressed to successfully attract investment.

## Major Market Conclusions

Market demand for development for a mix of regional and community uses near the interchanges is strong and growing. Additionally, a mix of regional, community and neighborhood uses along the corridor is strong. The current market is for a horizontal mix of uses rather than a vertical mix, and successful projects will likely contain a big box anchor.

The potential for investment along the corridor will be greatly enhanced if conditions of physical, aesthetic, and economic blight are successfully mitigated.

- Catalyst project must be large and well designed to attract clients over time.
- Opportunities for economic success will be from mixed-use and pedestrian-oriented, “Placemaking Projects”.

### Potential Market Based Catalyst Projects

Each district exhibits varying market conditions and has unique development opportunities. Based on the market analysis, ten (10) potential catalyst project sites were identified for analysis. (*Refer to Map 3. “Potential Catalyst Project Sites”.*)

Potential catalyst **sites #2, #6, and #7** are extremely well located and have strong regional and interchange market potential. Although, they have development potential, they were deleted from further consideration as “placemaking” projects due to environmental (wetlands) constraints, which limit the size of usable land.

For the remaining sites, market-based development programs were generated and further classified as being of a short, mid, or long-term time horizon.

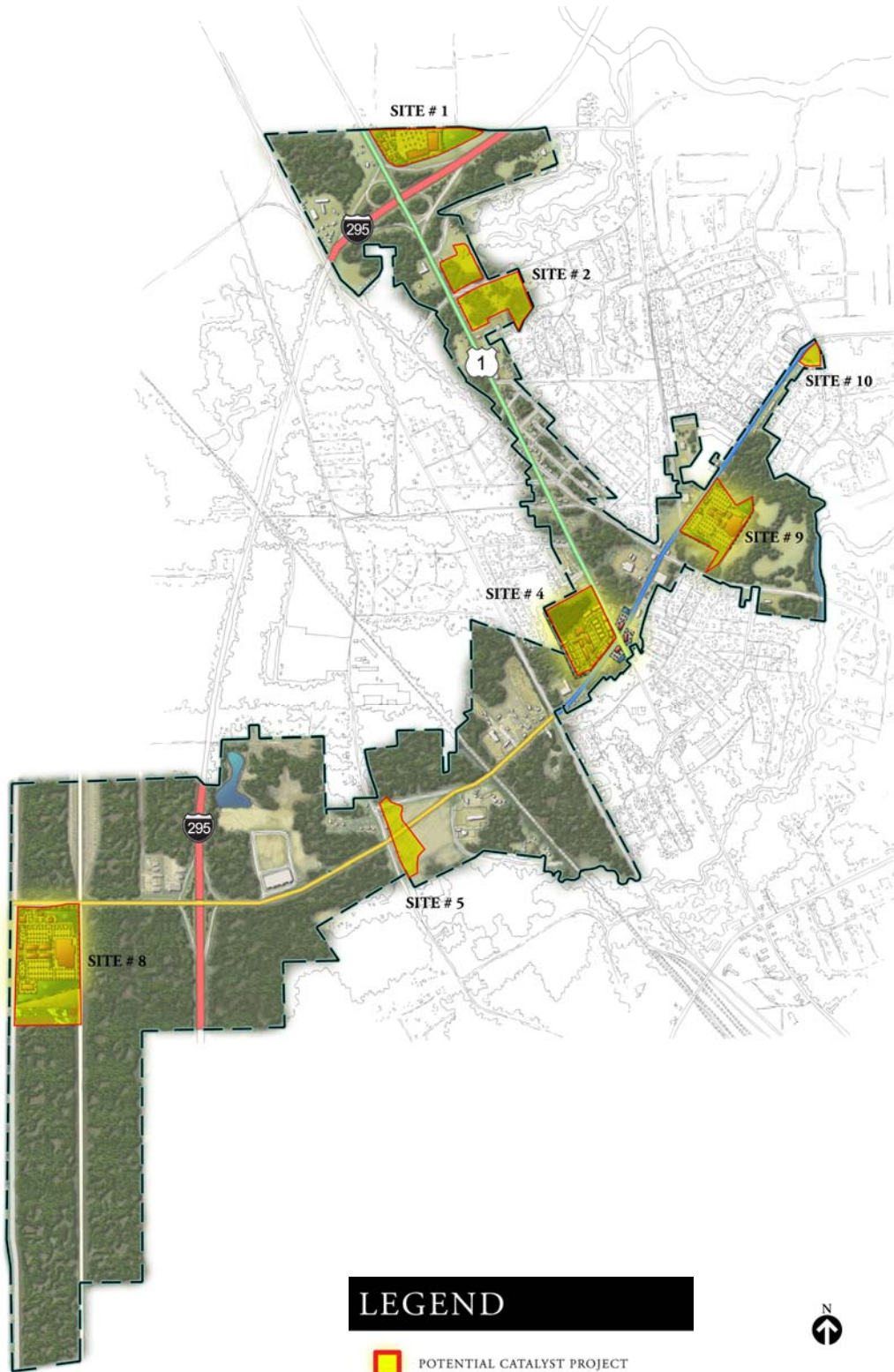
Unfortunately, **site #1** is not sized and configured to be a catalyst project but has strong potential for short-term regional/highway-oriented development and tax base potential.

Potential catalyst **sites #3 and #5** are well located for community and neighborhood markets, but the assembly of a suitably sized parcel under unified ownership appears to be difficult because of the multiple ownerships.

The following sites appear to offer the best opportunities for the development of catalyst projects based upon market conditions and potential for site assemblage:

- **Site #4:** Community mixed-use center potential over the mid term. Its market may be impacted if **Site #9** develop is developed first.
- **Site #8:** Regional mixed-use center potential over the mid-term.
- **Site #9:** Community and neighborhood **Site #4** mixed-use center potential. Its market may be impacted if is developed first.
- **Site #10:** Neighborhood mixed-use center potential but requires assemblage and some demolition of portions of an existing center.

Map 3. – Potential Catalyst Project Sites



### Character Districts

As a basis for organizing plan recommendations, four (4) unique districts were determined. (*Refer to Map 4. - "Character District Map", below.*)

#### **New Kings District**

New Kings Road (US-1) is a major gateway to the City's Downtown and will continue to primarily be an automobile-oriented commercial environment. Some of the older development adds to visual blight. Efforts to spur redevelopment along this highway corridor have already begun with recent roadway improvements, providing a suburban streetscape with curbed medians and landscaping. New development should meet urban standards and interchange Gateway improvements be constructed so that the area can live up to the quality perception envisioned in its moniker "New Kings Road".

#### **Soutel Place District**

The Soutel Place district once served as a more vibrant commercial area for the surrounding neighborhoods. The district's name responds to history and should reinforce the area's branding to help it become a successful pedestrian-oriented, commercial area with a mix of shops, entertainment, civic and recreational uses and housing. Soutel Drive should become a pedestrian shopping street with an urban level streetscape. The development of "placemaking" projects should be pursued to serve as catalyst for other investments.

#### **Old Kings District**

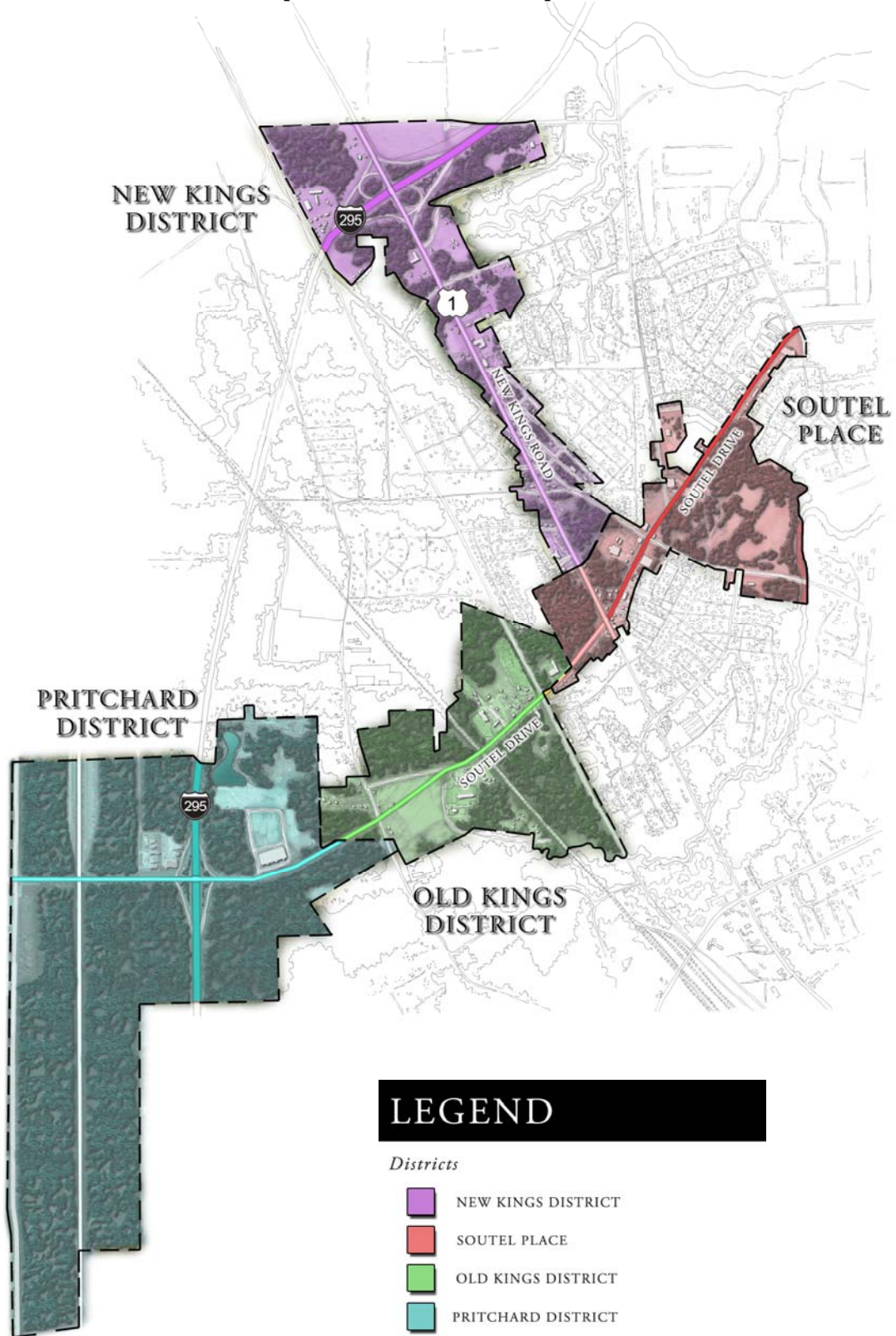
The Old Kings Road area was bypassed by traffic for many years and the mix of small houses and larger lots is reminiscent of rural Florida. The two (2) rail lines transacting the area provide local jobs and allow this district to become an emerging industrial area. Now that Soutel/Pritchard is being reconstructed, the area will come under more intensive development pressure and is extremely well situated to provide workforce housing to the surrounding industrial areas. The new improvements that straighten the Soutel/Pritchard connection and provide curbed medians should be beautified as a suburban streetscape.

#### **Pritchard District**

The Pritchard Road interchange has created tremendous industrial development opportunities in surrounding areas because of greater highway access and large land holdings. Pritchard Road lands have great potential for new development of all types and applications of good design standards, and quality public facilities that can help uplift the surrounding areas. The interchange should be enhanced to become a major gateway to surrounding areas. A "placemaking" project appears viable and should be pursued.



Map 4. – Character District Map



## Corridor Vision & Master Redevelopment Plan

Creating a “sense of place” and the reduction of blight are important components of the Corridor Redevelopment Plan. This plan focuses on projects that support greater pedestrian orientation, providing a mix of retail shopping, entertainment, work places, higher intensity housing, and civic facilities that address the every day needs of the surrounding community. This focus is reflected in the following:

### Corridor Redevelopment Vision Statement

- Support redevelopment of catalyst “placemaking” projects,
- Encourage “placemaking” projects, through targeted public investments, and
- Assist investment in small businesses.

Below, the Corridor “Redevelopment Master Plan” (“Plan”) (*refer to Map 5*) graphically presents an overview of the Corridor Vision and Master Plan recommendations including:

#### Private Sector

Private realm projects are the primary responsibility of a private sector developer or business owner. The Plan indicates three (3) types of “placemaking” projects:

##### Major Catalyst Projects

Highly themed mixed-use, pedestrian-oriented projects that are large enough to become catalyst which will spur investment in and redevelopment of surrounding properties.

##### Rehabilitation Projects

Small projects involving the rehabilitation of existing properties in areas suitable for creating a larger pedestrian-oriented environment along the public sidewalks.

##### Minor Redevelopment Projects

Smaller projects recommended for development of vacant parcels in areas suitable for creating a larger, pedestrian-oriented environment along the public sidewalks.

## Public Sector

Public Realm projects are the primary responsibility of the City and its agencies. Proposed public investments are identified and include the following projects:

### Gateways

Gateway projects that introduce the District, neighborhood, or project should include architectural features, signage or landscaping or some combination thereof, designed to become a landmark in the community. Gateways are proposed for three levels of projects as follows:

#### *Interchange*

Large suburban type projects for the improvement of the two (2) I-295 Interchanges that introduce the traveling highway public to the redevelopment area, and includes landscaping, signage, and lighting.

#### *Major Street Intersection*

Highly designed urban projects located at the intersection of two major roads that introduce regional through traffic to a District and may include intersection paving, signage, lighting landscaping and architectural features.

#### *Minor Street Intersection*

Smaller highly designed urban projects located at the intersection of major streets and side streets that introduce neighborhood traffic to the District and serve to calm traffic.

### Wayfinding:

Wayfinding should enhance the branding of the Character Districts, providing a logo and color pallet that is unique for each district and is integrated into a unified design for the following:

#### *Signage*

Unique and easily readable directional signage to civic and other significant facilities.

#### *Master Arm Treatment*

Design elements for branding and signage.

#### *Transit Shelter*

Design elements for branding and signage.

### Streetscapes

Streetscapes that create a more pedestrian friendly environment and tame traffic will include the following: *(Note: Wayfinding and gateways listed above are projects that can be pursued either separately or as components of a streetscape project.)*

#### *Hardscapes*

The framework of a streetscape project including roadway paving, curbs, medians, formal spacing of historic styled pedestrian ornamental lights, historic-styled site furniture, improved sidewalks, cross walks, and intersections.

#### *Site Furnishings*

The amenities of a streetscape project that include benches, trash receptacles, kiosk, transit shelters, wayfinding, and street lighting.

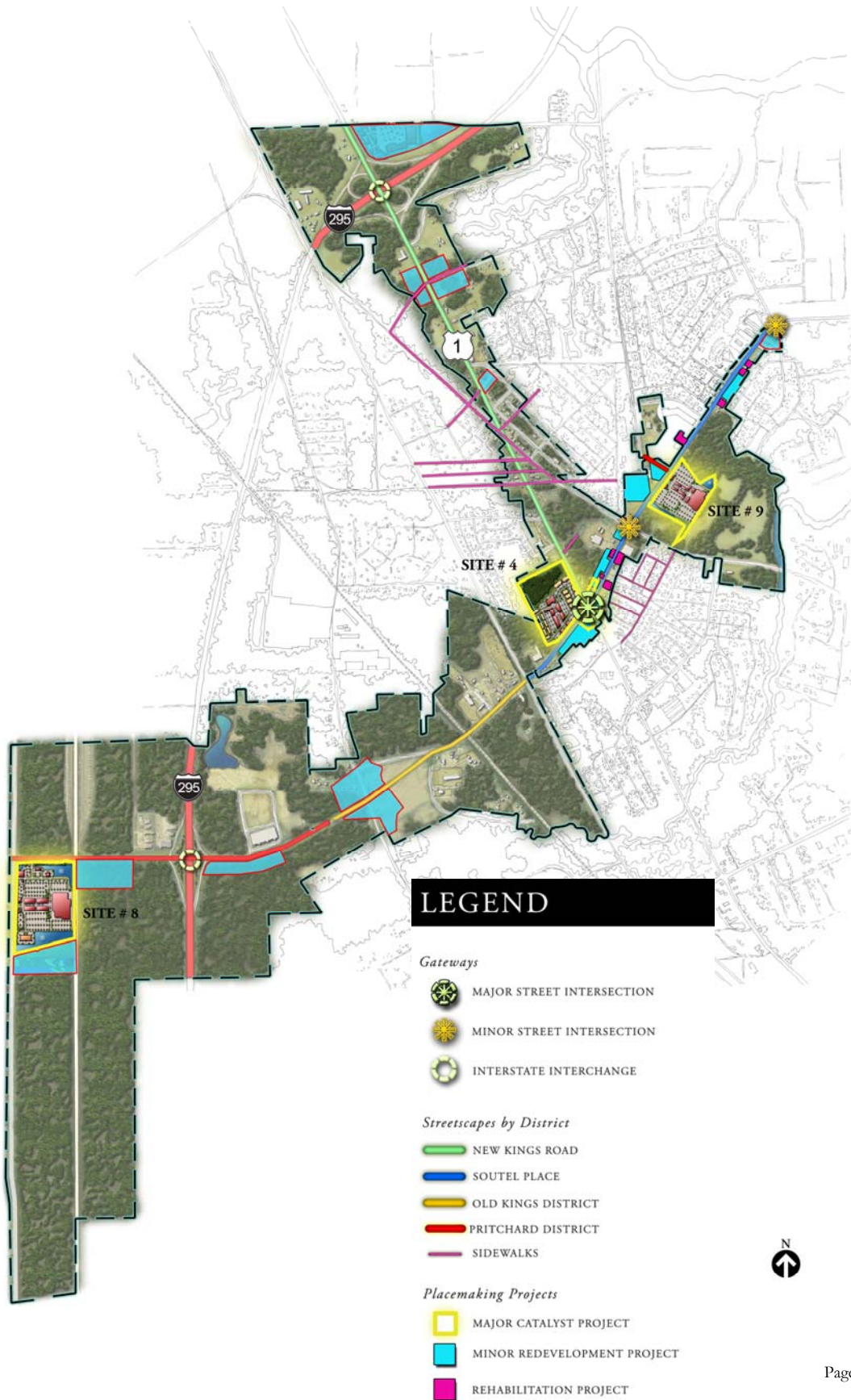
#### *Landscaping*

The natural, softening component of the pedestrian streetscape including irrigation systems, formal spacing of street/shade trees, ground cover, and shrubs.

#### *Sidewalk Connectivity*

Adequately-sized and safe sidewalks that connect the corridor to the surrounding neighborhoods are important to business success.

Map 5. – Corridor Vision and Master Plan Map



### District “Placemaking” Plan

#### Overview

The urban design plan provides the detailed description of public and private sector initiatives that will be necessary for the successful redevelopment of the corridor. The focus is on coordinating limited public investments to create the highest potential to leverage private investments. Thus, the market-based development potential to create Major Catalyst Projects should guide public investment strategy. These “placemaking” projects should be based on the following design principles:

- A retail and entertainment core with a mixed-use of office, civic, recreational, and, preferably, residential uses in the mix, or nearby.
- A high level of thematic design with articulation of attached storefronts and a focus on pedestrian integration of the mix of uses.
- A main street design concept, either internal to the project or woven into the fabric of an existing pedestrian commercial street, and with storefronts located close to the main street.
- A public gathering place in the form of a square or plaza accessed by wide sidewalks.

The “placemaking” projects that appear to have significant development potential and could serve as catalyst projects - met the following criteria:

- A location with a strong market for an appropriate mix of uses.
- Parcel(s) with appropriate size or the realistic potential for assemblage.

The public projects and cost estimates to support the private investments are provided for each District. These public project costs provided, herein, are the estimated hard costs of the projects and do not include soft costs, financing costs, or the costs associated with improving supportive infrastructure or basic services.

#### New Kings District

The New Kings District is a major gateway to the City’s Downtown and will continue to primarily be an automobile-oriented commercial environment. Efforts to spur redevelopment along this highway corridor have already begun with recent roadway improvements providing curbed medians and landscaping.

Map 6. – New Kings Road District Map



**Private Projects**

Major Catalyst Projects

A major catalyst project was not recommended for the New Kings District. Available parcels and the potential for assemblages did not appear to offer realistic opportunities to package a parcel of the necessary size to support a “placemaking” project. If assemblage of a twenty-five (25) +/- acre site occurs, a “placemaking” project meeting the corridor design standards should be supported. The proposed Placemaking Project in the Soutel Place District at the New Kings Road and Soutel Drive intersection is well located to serve the New Kings District.

Minor Redevelopment Projects and Rehabilitation Projects

There is market support for retail, office, and housing in the District, and a variety of smaller development and rehabilitation projects are appropriate.

**Public Projects and Cost**

Gateway Interchange

The New Kings Road and I-295 Interchange area is proposed for a public project to create a Gateway into the Corridor Redevelopment Area leading to Downtown Jacksonville. The project includes landscaping and site fixture, and, if fully landscaped, could cost approximately \$4 million because of the size of the interchange area. This cost could be dramatically reduced by focusing the investment on key locations that would have the greatest visual impact.





Figure 1. – Gateway Interchange – New Kings Road Master Plan



## KingSoutel Crossing (Corridor) Community Redevelopment Plan

**Table 2. – New Kings Road Gateway Interchange Cost Estimate**

ITEM	Size	Qty.	Cost	Total
<b>Landscape and Irrigation</b>				
Ex. Landscape Grubbing and Removal	SY	60484	5.00	302,420.00
Landscape Soil Improvements	SF	544359	0.75	408,269.25
Canopy Tree	Ea.	100	1,200.00	120,000.00
Understory Tree	Ea.	45	450.00	20,250.00
Palm	Ea.	408	300.00	122,400.00
Signature Palm	Ea.	50	4,500.00	225,000.00
Shrub Area	SF	357901	3.50	1,252,653.50
Sod Area (near shr. or tree install area)	SF	186458	0.35	65,260.30
Irrigation	SF	544359	1.25	680,448.75
<b>SUB-TOTAL</b>				<b>\$3,196,701.80</b>

ITEM	Size	Qty.	Cost	Total
<b>Site Furniture, Wayfinding, and Lighting</b>				
District Directional Signs	LS	1	20,000.00	20,000.00
Mast Arm Signage	Ea.	4	80,000.00	320,000.00
Mast Arm Structural Base/Footer	Ea.	4	8,000.00	32,000.00
<b>SUB-TOTAL</b>				<b>\$372,000.00</b>

**Interchange Gateway - New Kings District** **\$3,568,701.80**

ITEM	Size	Qty.	Cost	Total
<b>General Conditions</b>				
Mobilization (5%)				<b>178,435.09</b>
Contingency (10%)				<b>356,870.18</b>
Maintenance of Traffic (4%)				<b>142,748.07</b>
<b>SUB-TOTAL</b>				<b>\$678,053.34</b>

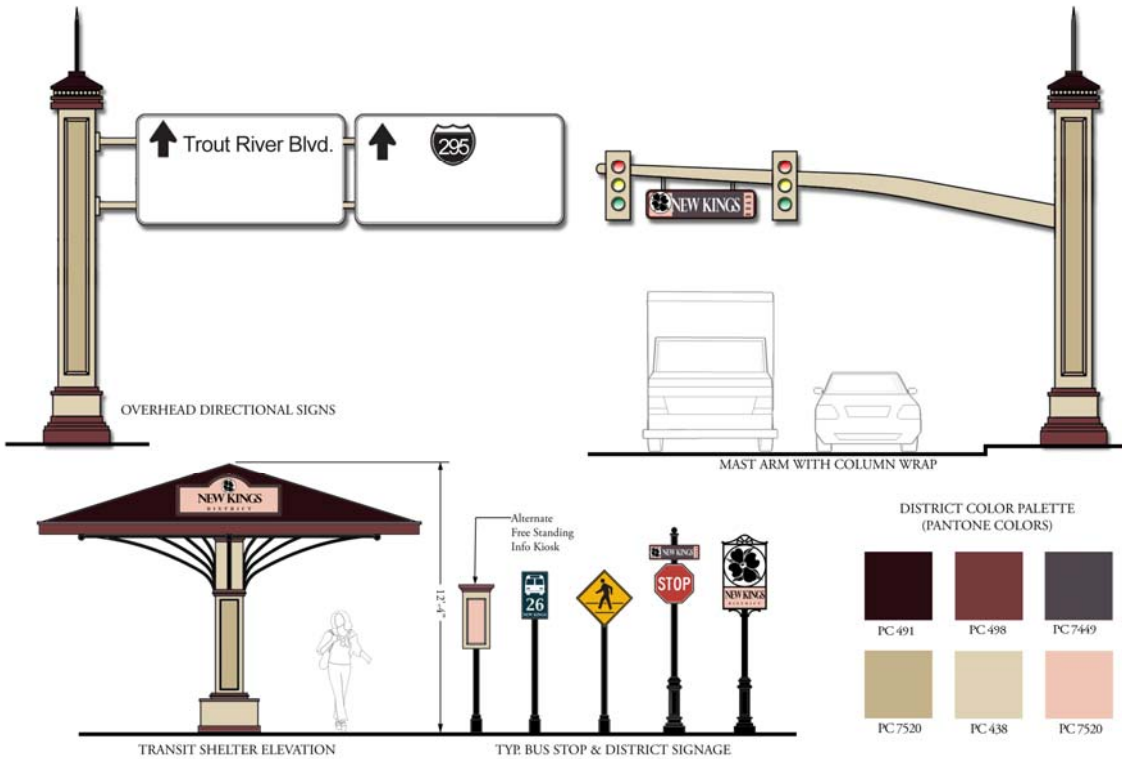
**Interchange Gateway - New Kings District** **\$4,246,755.14**

# KingSoutel Crossing (Corridor) Community Redevelopment Plan

## Wayfinding

The wayfinding program will establish a higher quality brand for the District.

Figure 2. – New Kings Road Wayfinding Program



# KingSoutel Crossing (Corridor) Community Redevelopment Plan

**Table 3. – New Kings Road Wayfinding Cost Estimate**

ITEM	Size	Qty.	Cost	Total
<b>Site Furniture, Wayfinding, and Lighting</b>				
Bus Stop Signs and District ID Signs	Ea.	16	2,500.00	40,000.00
Transit Shelters (@ Max 1 Mile)	Ea.	4	45,000.00	180,000.00
Ash/Waste Receptacle (@ 300' int.)	Ea.	60	950.00	57,000.00
Benches	Ea.	60	1,600.00	96,000.00
<b>SUB-TOTAL</b>				<b>\$373,000.00</b>

**New Kings District - New Kings Road Streetscape \$373,000.00**

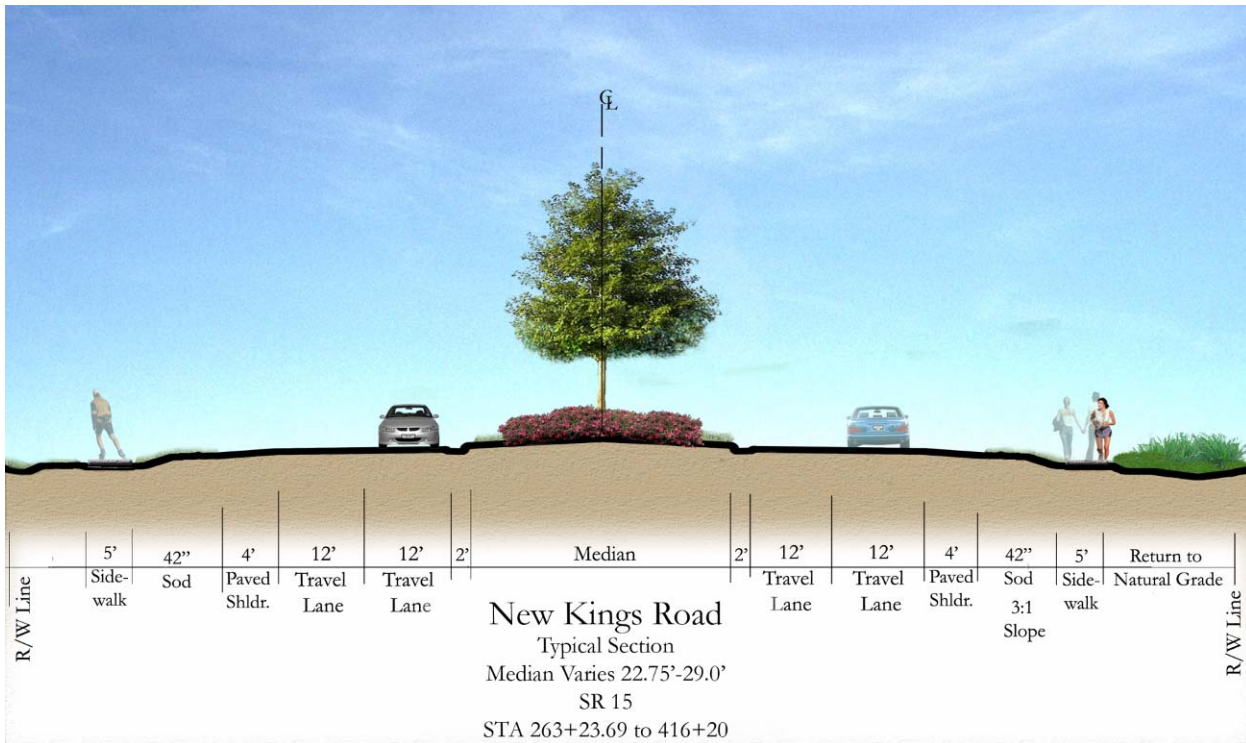
ITEM	Size	Qty.	Cost	Total
<b>General Conditions</b>				
Mobilization (5%)				18,650.00
Contingency (10%)				37,300.00
Maintenance of Traffic (4%)				14,920.00
<b>SUB-TOTAL</b>				<b>\$70,870.00</b>

**New Kings District - New Kings Road Streetscape \$443,870.00**

Streetscape

A streetscape program has already been implemented with the landscaping of the medians. Over time the median landscaping could be enhanced.

**Figure 3. – New Kings Road Wayfinding Program**



# KingSoutel Crossing (Corridor) Community Redevelopment Plan

## Sidewalks

**Table 4. – New Kings Road Sidewalks**

	ROAD NAME	PROJECT LIMIT	IMPROVEMENT	Linear Feet**	Cost/LF ***	COST \$
1	New Kings Road (East)	Wagner Rd.- Lorida Ave.	Construct 5-Foot Sidewalk	6,864	\$100	686,400
2	New Kings Road (West)	Wagner Rd.- Lorida Ave.	Construct 5-Foot Sidewalk	6,336	\$100	633,600
3	Gilchrist Road (East)	New Kings Rd- Sibbald Rd.	Construct 5-Foot Sidewalk	7,580	\$100	758,000
4	Gilchrist Road (West)	Moncrief Dinsmore Rd.-New Kings Rd	Construct 5-Foot Sidewalk	2,480	\$100	248,000
5	Redpole Avenue (East)	New Kings- Moncrief-Dinsmore Rd	Construct 5-Foot Sidewalk	360	\$100	36,000
6	Redpole Avenue (West)	Thrasher Ave- New Kings Rd.	Construct 5-Foot Sidewalk	3,900	\$100	390,000
7	Finch Avenue (West)	Thrasher Ave- New Kings Rd.	Construct 5-Foot Sidewalk	1,680	\$100	168,000
8	Flicker Avenue (East)	New Kings- Moncrief-Dinsmore Rd	Construct 5-Foot Sidewalk	580	\$100	58,000
9	Flicker Avenue (West)	CSX RR - New Kings Rd.	Construct 5-Foot Sidewalk	3,900	\$100	390,000
10	McCleod Street (East)	New Kings Rd. - Pipit Ave.	Construct 5-Foot Sidewalk	1,980	\$100	198,000
11	Moncrief Road (West)	New Kings Rd-CSX RR	Construct 5-Foot Sidewalk	3,900	\$100	390,000
12	Liston Road (East)	Moncreif Rd.- Pipit Ave	Construct 5-Foot Sidewalk	2,112	\$100	211,200
13	Dekle Avenue (East)	New Kings Rd -Liston Rd	Construct 5-Foot Sidewalk	5,220	\$100	522,000
14	Moncrief Dinsmore Rd (East)	Moncrief Rd.- New Kings Rd	Construct 5-Foot Sidewalk	2,980	\$100	298,000
15	Moncrief Dinsmore Rd (West)	New Kings Rd -Gilchrist Rd	Construct 5-Foot Sidewalk	5,800	\$100	580,000
16	Hull Street (West)	Moncrief Rd - Ageson	Construct 5-Foot Sidewalk	1,056	\$100	105,600
	<b>TOTAL</b>			<b>56,728</b>		<b>*5,672,800</b>
						<b>**2,836,400</b>

\* Both sides of the street

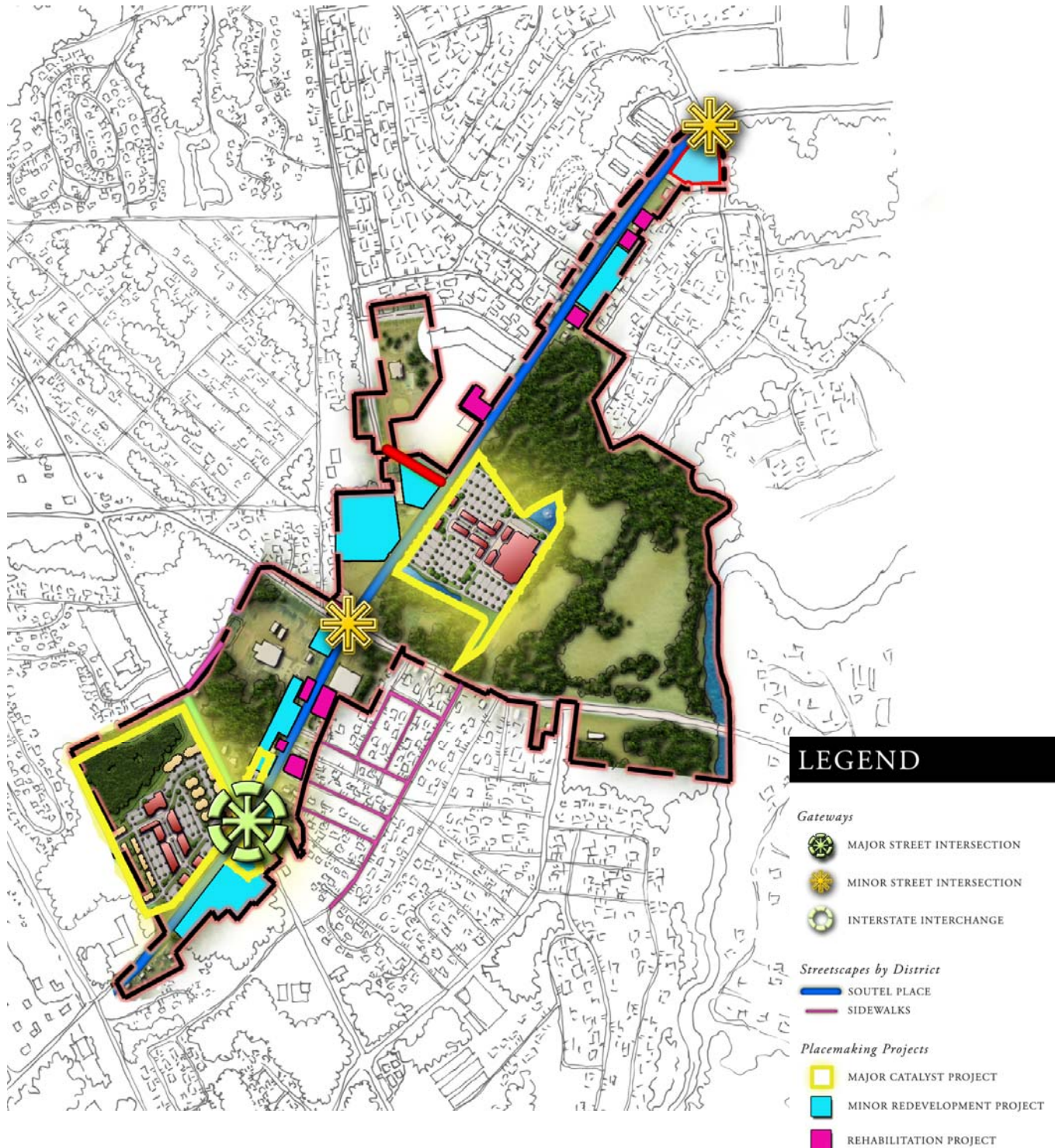
\*\* One side of the street

\*\*\* Includes estimated cost of construction, piping, and fill.

## Soutel Place District

The Soutel Place District once served as a more vibrant commercial area for the surrounding neighborhoods. It is the only district along the corridor that has potential to become a more pedestrian oriented shopping street providing a mix of shops, entertainment, civic and recreational uses, and housing. The development of “placemaking” projects should be pursued to serve as catalyst for other investments.

Map 7. – Soutel Place District Map



**Private Projects**

Major Catalyst Projects

The Soutel Place District contains two (2) sites that have strong potential for “placemaking” projects, but they appear to be competitors for a share at the community and neighborhood market. If either project is developed, the other project should be carefully evaluated to insure that there is market support for the proposed development program.

Based on expenditure patterns for community sized mixed-use centers, by 2012 (to 2015) the new households could support approximately 175,000 square feet of retail and entertainment space, and an additional 194,000 square feet would be required to serve the new households by 2020 (to 2023). Either of the two (2) sites could be expected to attract in the range of 60-70% of this demand in its primary market area, or between 100,000 to 120,000 square feet by 2012 (to 2015), and another 115,000 to 135,000 square feet by 2020 (to 2023), for a total ranging from 215,000 to 255,000 square feet by 2020 (to 2023).

For local serving office space, an extremely conservative approach was taken for these initial catalyst projects. The recommended projection for office space was reduced because of the impact of current negative projections. There will be substantially more demand when perception begins to improve. As redevelopment occurs and perceptions of blight diminish, office development should expand dramatically.

Soutel Place Center: The 28-acre project is located just east of Soutel Drive and Moncrief Road in the south east quadrant on a portion of Lonnie Miller Park and is owned by the City. The market appears to support a development program for a mix of uses that will allow creation of a mixed-use project as follows:

Large Retail Box	100,000 sf
Retail	46,000 sf
Out parcels	13,000 sf
Office	7,500 sf

Nearby residential neighborhoods provide a residential component to the mix within walking distance. Adding an on-site residential component to the project would be appropriate. Adding civic or government uses as well as recreation uses will strengthen the project. A public space for special events and informal events should be required.

Figure 4. – “Soutel Place” Major Catalyst Project Concept





Kings Crossing Center

The 48-acre project is located in the northwest quadrant of New Kings Road and Soutel Drive. The market appears to support a development program for a mix of uses as follows:

Retail and Entertainment	151,500 sf
Office	57,000 sf
Residential (Town homes)	76 units
Residential Apartments	46 units

The project is proposed for a residential component, but the site could rely on nearby neighborhoods for the residential component. Adding civic or government uses as well as recreation uses will strengthen the project. A public space for special events and informal events should be required. This site is located to serve both the Old Kings Road District and the New Kings Road District and could attract some regional uses. An economic feasibility analysis by a developer may prove to support a more aggressive retail/entertainment and office development than this proposal.

Figure 5. – Site #4 Catalyst Project Concept



## Minor Redevelopment Projects

Several vacant sites along the District have been identified on the Corridor Vision Redevelopment Master Plan Map for smaller redevelopment projects. It is likely that the long term sustainability of these projects will be greatly improved by implementation of one of the Major Catalyst Projects above.

## Rehabilitation Projects

Several existing buildings along the District have been identified in the Corridor Vision and Redevelopment Master Plan Map for rehabilitation projects.

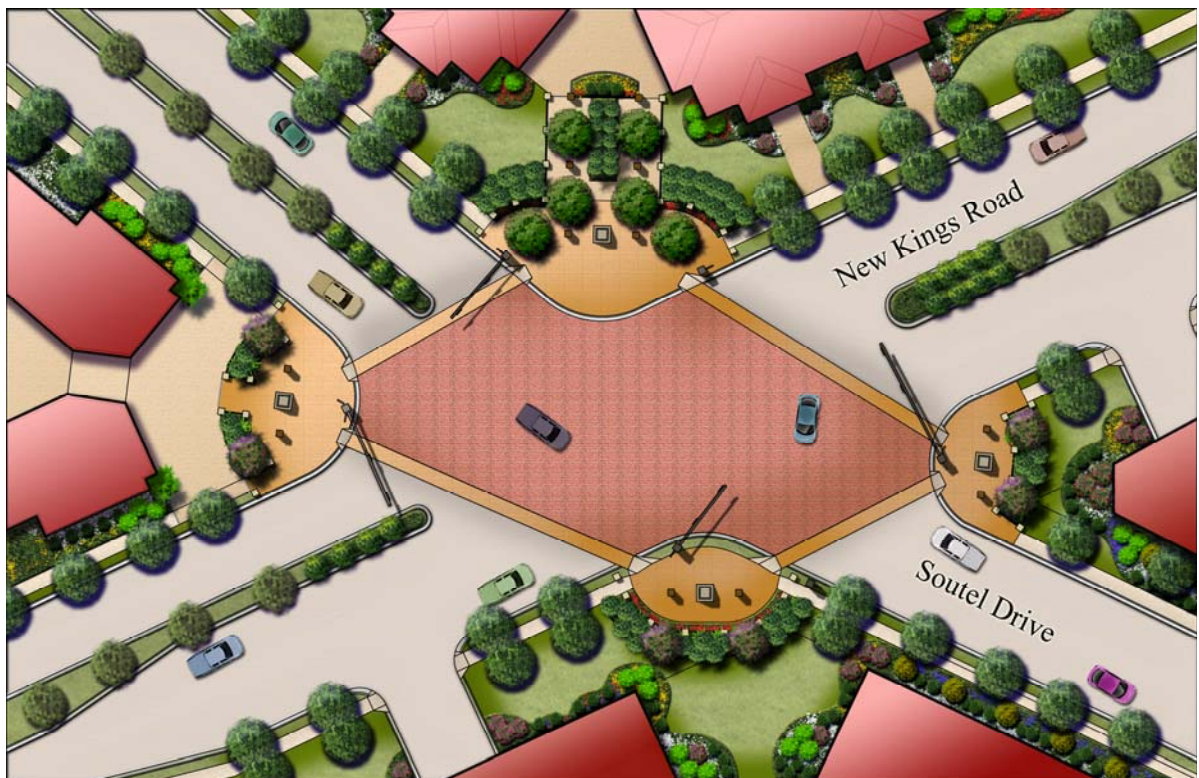
## **Public Projects**

To become a pedestrian shopping street Soutel Drive needs urban level streetscape improvements.

## Gateway Major

The intersection of Soutel Drive and New Kings Road will introduce the Soutel Place District and the Soutel Place Center (described above, pg. 27). It is the heart of the Corridor Redevelopment Area and the logical location to inform the general public that the City is focused on improving the area. Major intersection improvements are a proposed including hardscaping including a textured (stamped concrete intersection and crosswalks), landscaping, wayfinding, lighting, and architectural features.

Figure 6. – Gateway Major Project



# KingSoutel Crossing (Corridor) Community Redevelopment Plan

**Table 5. – Gateway Major Cost Estimate**

ITEM	Size	Qty.	Cost	Total
<b>Roadway Paving and Hardscape Demolition</b>				
Ex Pavement Demolition	SY	2110	8.00	16,880.00
Ex. Curb Removal	SY	105	8.00	840.00
Ex. Sidewalk Removal	SY	255	8.00	2,040.00
<b>SUB-TOTAL</b>				<b>\$19,760.00</b>

ITEM	Size	Qty.	Cost	Total
<b>Roadway Paving, Striping and Hardscape</b>				
Type "F" Curbing	LF	473	15.00	7,095.00
4" Colored Sidewalk w/ Patterning	SF	9217	7.50	69,127.50
Decorative Stamped Colored Conc.	SF	16662	6.50	108,303.00
Sub-base for Stamped Colored Conc.	SF	16662	3.50	58,317.00
Decorative Stamped Conc. Crosswalk	SF	2328	6.50	15,132.00
Sub-base for Stamped Conc. Crosswalk	SF	2328	3.50	8,148.00
Icon Element	Ea	4	25,000.00	100,000.00
Decorative Masonry Column	Ea	32	1,800.00	57,600.00
Decorative Metal Fence	LF	270	150.00	40,500.00
ADA Accessible Warning Pavement	Ea	8	250.00	2,000.00
<b>SUB-TOTAL</b>				<b>\$466,222.50</b>

ITEM	Size	Qty.	Cost	Total
<b>Landscape and Irrigation</b>				
Canopy Tree	Ea.	10	1,200.00	12,000.00
Understory Tree	Ea.	47	450.00	21,150.00
Shrub Area	SF	3399	2.50	8,497.50
Sod Area	SF	177	0.35	61.95
Irrigation	SF	3576	1.25	4,470.00
<b>SUB-TOTAL</b>				<b>\$46,179.45</b>

ITEM	Size	Qty.	Cost	Total
<b>Site Furniture Wayfinding, and Lighting</b>				
District Directional Signs	LS	1	10,000.00	10,000.00
Street Light Poles/Fixtures	Ea.	8	3,500.00	28,000.00
Mast Arm Signalization	Ea.	4	80,000.00	320,000.00
Illuminated Street Signage	Ea.	4	3,500.00	14,000.00
Mast Arm Structural Base/Footer	Ea.	4	8,000.00	32,000.00
Signalization	LS.	1	200,000.00	200,000.00
Ash/Waste Receptacle	Ea.	8	950.00	7,600.00
Benches	Ea.	16	1,600.00	25,600.00
<b>SUB-TOTAL</b>				<b>\$637,200.00</b>

**Major Gateway - Intersection New Kings Road & Soutel Drive \$1,169,361.95**

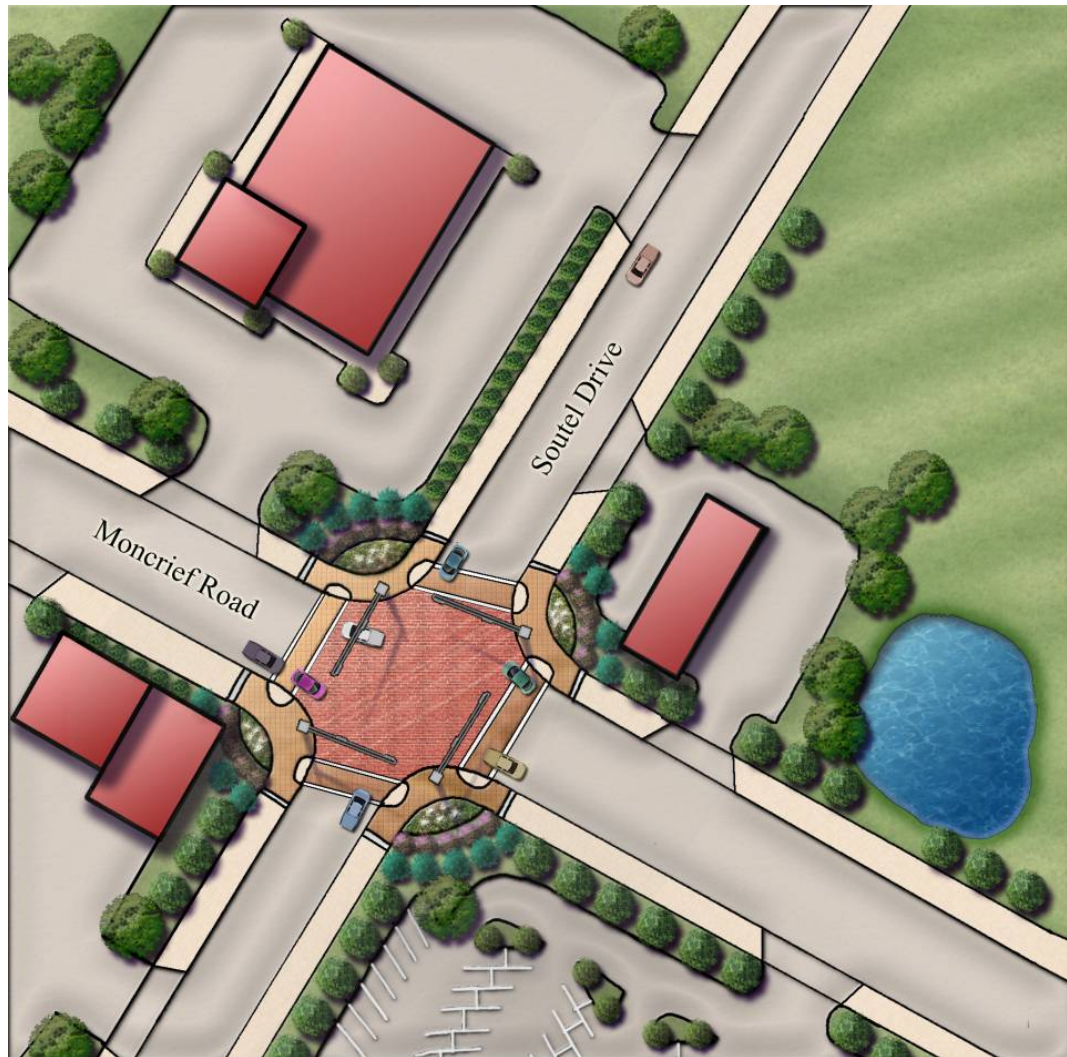
ITEM	Size	Qty.	Cost	Total
<b>General Conditions</b>				
Mobilization (5%)				58,468.10
Contingency (10%)				116,936.20
Maintenance of Traffic (4%)				46,774.48
<b>SUB-TOTAL</b>				<b>\$222,178.77</b>

**Major Gateway - Intersection New Kings Road & Soutel Drive \$1,391,540.72**

Gateway Minor

The Soutel and Moncrief intersection is an opportunity to create a gateway from the neighborhoods.

Figure 7. – Gateway Minor Project, Soutel and Moncrief



# KingSoutel Crossing (Corridor) Community Redevelopment Plan

**Table 6. –Gateway Minor Soutel & Moncrief Cost Estimate**

ITEM	Size	Qty.	Cost	Total
<b>Roadway Paving and Hardscape Demolition</b>				
Ex Pavement Demolition	SY	691	8.00	5,528.00
Ex. Curb Removal	SY	55	8.00	440.00
Ex. Sidewalk Removal	SY	146	8.00	1,168.00
<b>SUB-TOTAL</b>				<b>\$7,136.00</b>

ITEM	Size	Qty.	Cost	Total
<b>Roadway Paving, Striping and Hardscape</b>				
Type "F" Curbing	LF	250	15.00	3,750.00
4" Colored Sidewalk w/ Patterning	SF	2400	7.50	18,000.00
Decorative Stamped Colored Conc.	SF	4900	6.50	31,850.00
Sub-base for Stamped Colored Conc.	SF	4900	3.50	17,150.00
Decorative Stamped Conc. Crosswalk	SF	1320	6.50	8,580.00
Sub-base for Stamped Conc. Crosswalk	SF	1320	3.50	4,620.00
ADA Accessible Warning Pavement	Ea	8	250.00	2,000.00
<b>SUB-TOTAL</b>				<b>\$85,950.00</b>

ITEM	Size	Qty.	Cost	Total
<b>Landscape and Irrigation</b>				
Canopy Tree	Ea.	16	1,200.00	19,200.00
Understory Tree	Ea.	20	450.00	9,000.00
Signature Palm	Ea.	20	4,500.00	90,000.00
Shrub Area	SF	4400	2.50	11,000.00
Irrigation	SF	4400	1.25	5,500.00
<b>SUB-TOTAL</b>				<b>\$134,700.00</b>

ITEM	Size	Qty.	Cost	Total
<b>Site Furniture Wayfinding, and Lighting</b>				
District Directional Signs	LS	1	10,000.00	10,000.00
Street Light Poles/Fixtures	Ea.	8	3,500.00	28,000.00
Mast Arm Signalization	Ea.	4	80,000.00	320,000.00
Illuminated Street Signage	Ea.	4	3,500.00	14,000.00
Mast Arm Structural Base/Footer	Ea.	4	8,000.00	32,000.00
Signalization	Ls.	1	200,000.00	200,000.00
Ash/Waste Receptacle	Ea.	4	950.00	3,800.00
Benches	Ea.	8	1,600.00	12,800.00
<b>SUB-TOTAL</b>				<b>\$620,600.00</b>

**Minor Gateway - Intersection Soutel Drive & Moncrief Road** **\$848,386.00**

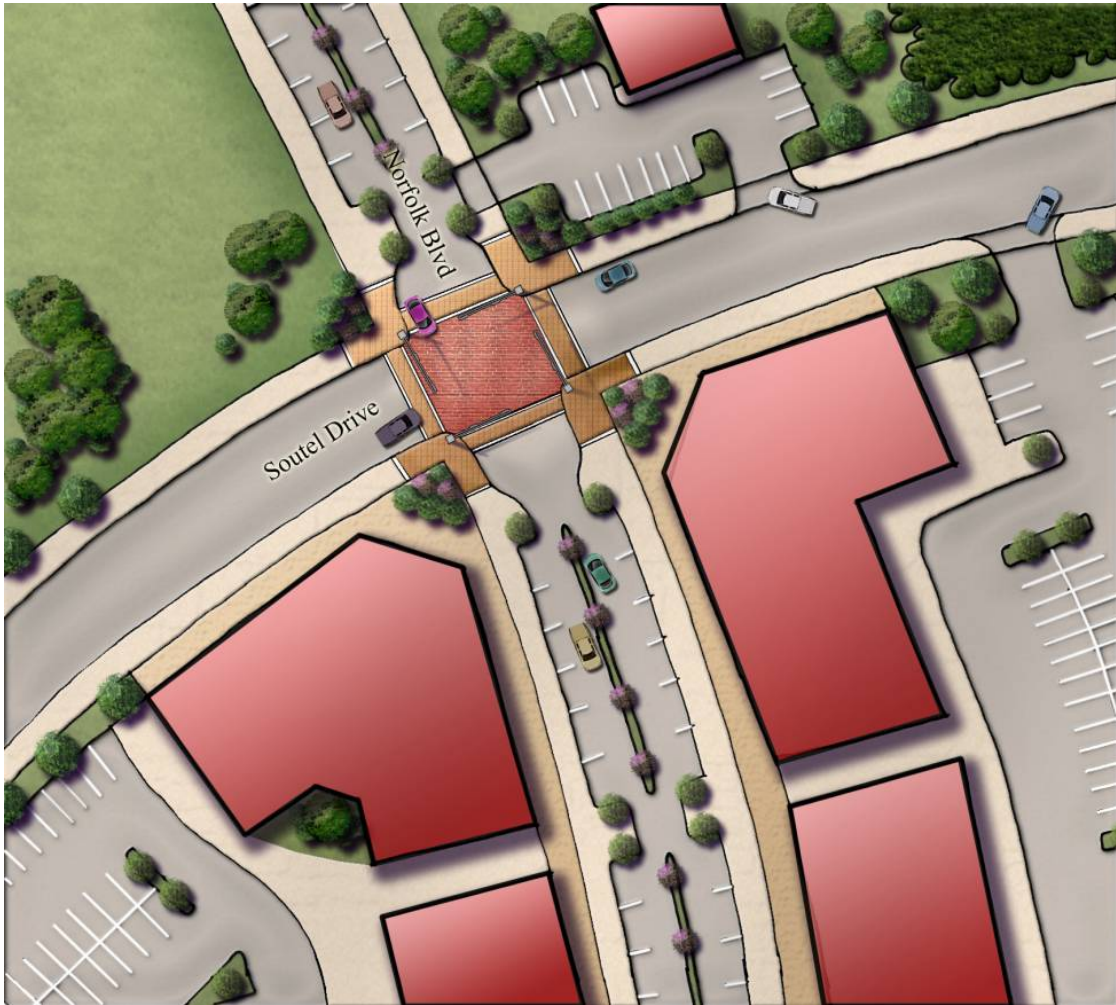
ITEM	Size	Qty.	Cost	Total
<b>General Conditions</b>				
Mobilization (5%)				42,419.30
Contingency (10%)				84,838.60
Maintenance of Traffic (4%)				33,935.44
<b>SUB-TOTAL</b>				<b>\$161,193.34</b>

**Minor Gateway - Intersection Soutel Drive & Moncrief Road** **\$1,009,579.34**

Gateway Minor

The Soutel and Norfolk intersection is another opportunity to create a gateway from the neighborhoods.

Figure 8. – Gateway Minor Project, Soutel and Norfolk



# KingSoutel Crossing (Corridor) Community Redevelopment Plan

**Table 7. –Gateway Minor Soutel & Norfolk Cost Estimate**

ITEM	Size	Qty.	Cost	Total
<b>Roadway Paving and Hardscape Demolition</b>				
Ex Pavement Demolition	SY	691	8.00	5,528.00
Ex. Curb Removal	SY	55	8.00	440.00
Ex. Sidewalk Removal	SY	146	8.00	1,168.00
<b>SUB-TOTAL</b>				<b>\$7,136.00</b>

ITEM	Size	Qty.	Cost	Total
<b>Roadway Paving, Striping and Hardscape</b>				
Type "F" Curbing	LF	250	15.00	3,750.00
4" Colored Sidewalk w/ Patterning	SF	2400	7.50	18,000.00
Decorative Stamped Colored Conc.	SF	4900	6.50	31,850.00
Sub-base for Stamped Colored Conc.	SF	4900	3.50	17,150.00
Decorative Stamped Conc. Crosswalk	SF	1320	6.50	8,580.00
Sub-base for Stamped Conc. Crosswalk	SF	1320	3.50	4,620.00
ADA Accessible Warning Pavement	Ea	8	250.00	2,000.00
<b>SUB-TOTAL</b>				<b>\$85,950.00</b>

ITEM	Size	Qty.	Cost	Total
<b>Landscape and Irrigation</b>				
Canopy Tree	Ea.	16	1,200.00	19,200.00
Understory Tree	Ea.	20	450.00	9,000.00
Signature Palm	Ea.	20	4,500.00	90,000.00
Shrub Area	SF	4400	2.50	11,000.00
Irrigation	SF	4400	1.25	5,500.00
<b>SUB-TOTAL</b>				<b>\$134,700.00</b>

ITEM	Size	Qty.	Cost	Total
<b>Site Furniture Wayfinding, and Lighting</b>				
District Directional Signs	LS	1	10,000.00	10,000.00
Street Light Poles/Fixtures	Ea.	8	3,500.00	28,000.00
Mast Arm Signalization	Ea.	4	80,000.00	320,000.00
Illuminated Street Signage	Ea.	4	3,500.00	14,000.00
Mast Arm Structural Base/Footer	Ea.	4	8,000.00	32,000.00
Signalization	Ls.	1	200,000.00	200,000.00
Ash/Waste Receptacle	Ea.	4	950.00	3,800.00
Benches	Ea.	8	1,600.00	12,800.00
<b>SUB-TOTAL</b>				<b>\$620,600.00</b>

**Minor Gateway - Intersection Soutel Drive & Moncrief Road** **\$848,386.00**

ITEM	Size	Qty.	Cost	Total
<b>General Conditions</b>				
Mobilization (5%)				42,419.30
Contingency (10%)				84,838.60
Maintenance of Traffic (4%)				33,935.44
<b>SUB-TOTAL</b>				<b>\$161,193.34</b>

**Minor Gateway - Intersection Soutel Drive & Moncrief Road** **\$1,009,579.34**

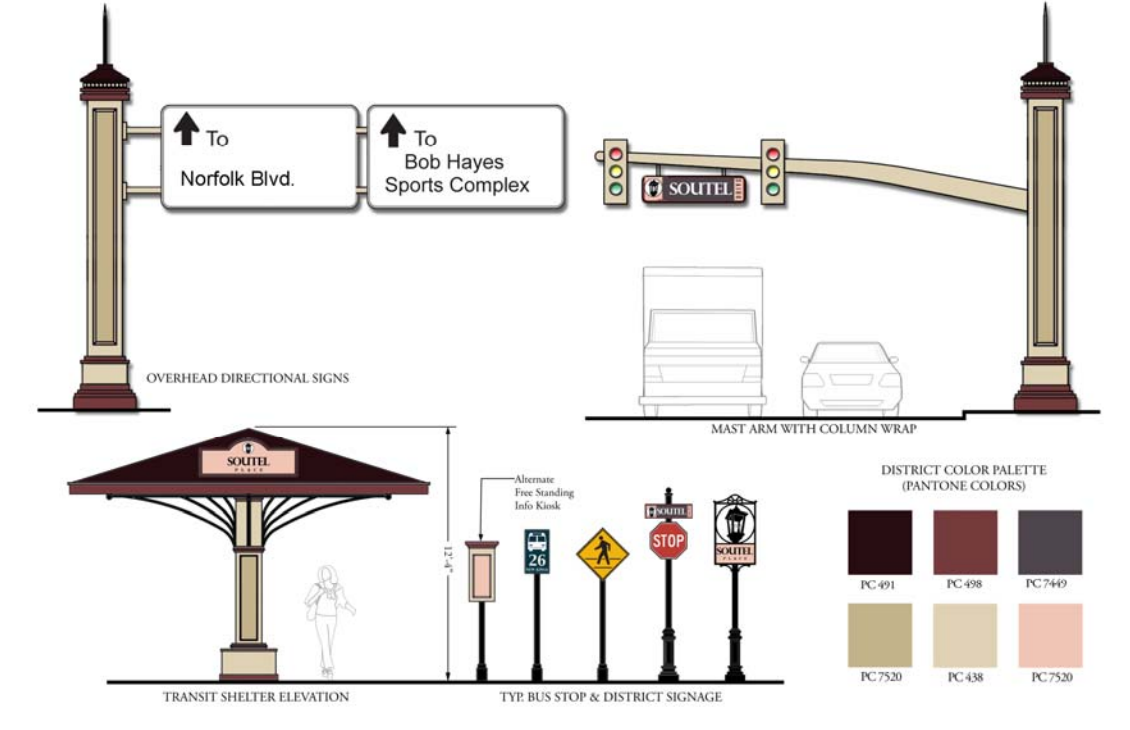


# KingSoutel Crossing (Corridor) Community Redevelopment Plan

## Wayfinding

The logo for the Soutel Place District wayfinding system is designed to reinforce a strong pedestrian oriented brand.

Figure 9. – Soutel Place District Wayfinding

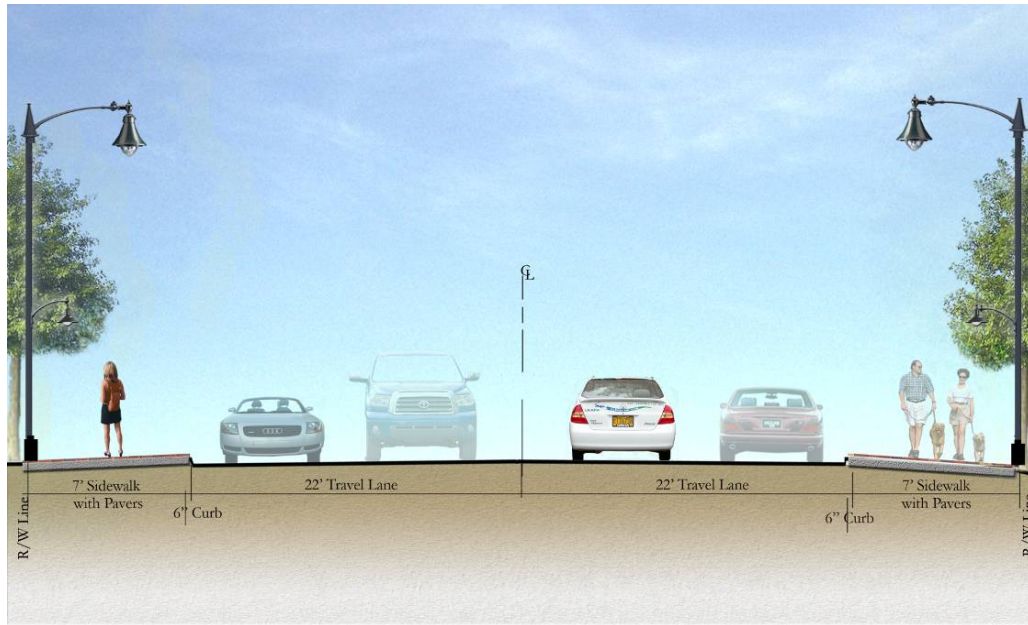


# KingSoutel Crossing (Corridor) Community Redevelopment Plan

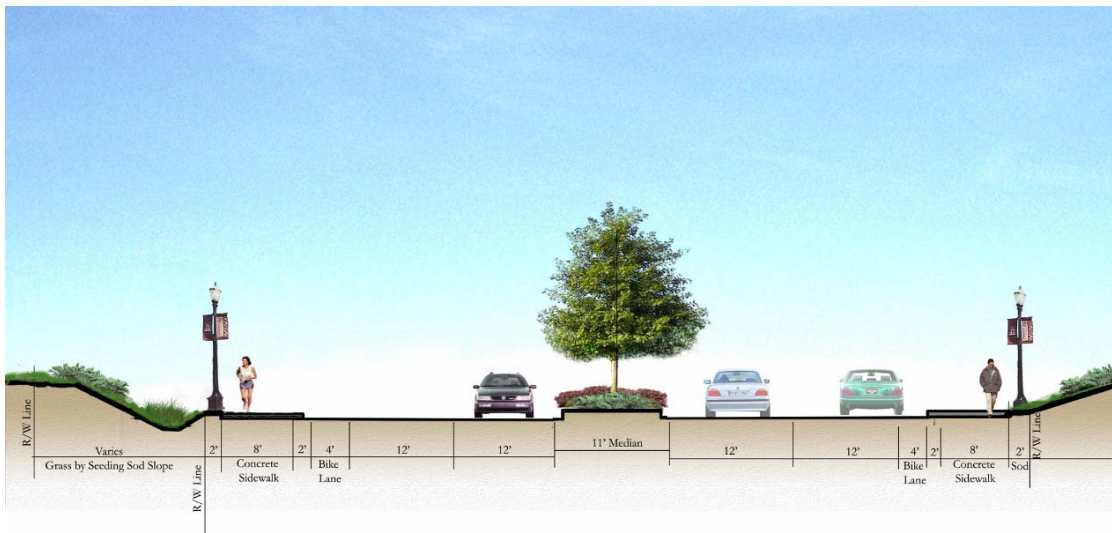
## Streetscape

The District includes two different streetscape concepts, one for the stretch to the east of New Kings Road and the other for the West. To the west, the new improvements provide curbed medians that should be beautified. To the east the four lane roadway is difficult to beautify because of the tight right of way that prohibits introduction of a median. This means that the edges of the street are even more important.

**Figure 10. – Soutel Place (East) Streetscape**



**Figure 11. – Soutel Place (West) Streetscape**



Soutel Suburban Drive  
Typical Section

## KingSoutel Crossing (Corridor) Community Redevelopment Plan

**Table 8. – Soutel Place (East) Wayfinding & Streetscape Cost Estimate**

ITEM	Size	Qty.	Cost	Total
<b>Roadway Paving, Striping and Hardscape</b>				
4" Gray Sidewalk (7' Wide)	SF	134624	4.25	572,152.00
<b>SUB-TOTAL</b>				<b>\$572,152.00</b>

ITEM	Size	Qty.	Cost	Total
<b>Site Furniture Wayfinding, and Lighting</b>				
Bus Stop Signs and District ID Signs	Ea.	22	2,500.00	55,000.00
Transit Shelters (@ Max 1 Mile)	Ea.	6	45,000.00	270,000.00
Street Light Poles/Fixtures (@150' O.C.)	Ea.	128	3,500.00	448,000.00
Ash/Waste Receptacle (Every 300')	Ea.	64	950.00	60,800.00
Benches	Ea.	64	1,600.00	102,400.00
<b>SUB-TOTAL</b>				<b>\$936,200.00</b>

**Soutel Place - Soutel Drive Streetscape (East of Major Gateway) \$1,508,352.00**

ITEM	Size	Qty.	Cost	Total
<b>General Conditions</b>				
Mobilization (5%)				75,417.60
Contingency (10%)				150,835.20
Maintenance of Traffic (4%)				60,334.08
<b>SUB-TOTAL</b>				<b>\$286,586.88</b>

**Soutel Place - Soutel Drive Streetscape (East of Major Gateway) \$1,794,938.88**

## KingSoutel Crossing (Corridor) Community Redevelopment Plan

Table 9. – Soutel Place (West) Wayfinding & Streetscape Cost Estimate

ITEM	Size	Qty.	Cost	Total
<b>Roadway Paving, Striping and Hardscape</b>				
4" Gray Sidewalk (10' Wide)	SF	27,040	4.25	114,920.00
<b>SUB-TOTAL</b>				<b>\$114,920.00</b>

ITEM	Size	Qty.	Cost	Total
<b>Landscape and Irrigation-Median</b>				
Landscape Soil Improvements	SF	13,675	0.75	10,256.25
Canopy Tree	Ea.	26	1,200.00	31,200.00
Understory Tree	Ea.	12	450.00	5,400.00
Shrub Area	SF	2475	2.50	6,187.50
Sod Area	SF	11200	0.35	3,920.00
Irrigation	SF	13,675	1.25	17,093.75
<b>SUB-TOTAL</b>				<b>\$74,057.50</b>

ITEM	Size	Qty.	Cost	Total
<b>Landscape and Irrigation-Street</b>				
Sod Area	SF	5408	0.35	1,892.80
Irrigation	SF	5408	1.25	6,760.00
<b>SUB-TOTAL</b>				<b>\$8,652.80</b>

ITEM	Size	Qty.	Cost	Total
<b>Site Furniture Wayfinding, and Lighting</b>				
Bus Stop Signs and District ID Signs	Ea.	8	2,500.00	20,000.00
Street Light Poles/Fixtures (@ 80' O.C.)	Ea.	33	3,500.00	115,500.00
Ash/Waste Receptacle (Every 300')	Ea.	10	950.00	9,500.00
Benches	Ea.	10	1,600.00	16,000.00
<b>SUB-TOTAL</b>				<b>\$161,000.00</b>

**Soutel Place - Soutel Drive Streetscape (West of Major Gateway) \$358,630.30**

ITEM	Size	Qty.	Cost	Total
<b>General Conditions</b>				
Mobilization (5%)				17,931.52
Contingency (10%)				35,863.03
Maintenance of Traffic (4%)				14,345.21
<b>SUB-TOTAL</b>				<b>\$68,139.76</b>

**Soutel Place - Soutel Drive Streetscape (West of Major Gateway) \$426,770.06**

# KingSoutel Crossing (Corridor) Community Redevelopment Plan

## Sidewalks

**Table 10. – Soutel Place Sidewalks**

	ROAD NAME	PROJECT LIMIT	IMPROVEMENT	Lineal Feet*	Cost/LF *** \$	COST
1	Washington Estates Drive	Soutel Drive - Dostie Drive	Construct 5-Foot Sidewalk	2,112	\$60	126,720
2	Dostie Drive South	Soutel Drive - Dostie Drive	Construct 5-Foot Sidewalk	2,112	\$60	126,720
3	Devron Drive	Soutel Drive - Dostie Drive	Construct 5-Foot Sidewalk	2,112	\$60	126,720
4	Kylan Drive North & West	Devron Drive- Devron Drive	Construct 5-Foot Sidewalk	2,112	\$60	126,720
5	Sims Drive	Devron Drive- Kylan Drive	Construct 5-Foot Sidewalk	1056	\$60	63,360
6	Dostie Drive	Moncrief Road.-Welland Road	Construct 5-Foot Sidewalk	4,224	\$60	253,440
	<b>TOTAL</b>			<b>13,728</b>		<b>*823,680</b>
						<b>**411,840</b>

**NOTES:**

\* Both sides of street.

\* One side of the street.

\*\*\* Construction cost only

## Old Kings District




The Old Kings District was bypassed for many years and the area’s mix of small houses and larger lots is reminiscent of rural Florida. The two (2) rail lines transecting the area provide local jobs and has created an emerging industrial area. Now that Soutel/Pritchard is aligned and reconstructed, the area will come under more intensive development pressure and is extremely well situated to provide workforce housing to the surrounding industrial areas.

Map 8. – Old Kings Road District Map




**LEGEND**




*Gateways*

-  MAJOR STREET INTERSECTION
-  MINOR STREET INTERSECTION
-  INTERSTATE INTERCHANGE

*Streetscapes by District*

-  OLD KINGS DISTRICT
-  SIDEWALKS

*Placemaking Projects*

-  MAJOR CATALYST PROJECT
-  MINOR REDEVELOPMENT PROJECT
-  REHABILITATION PROJECT

**Private Projects**

Major Catalyst Projects

Although the market for retail and big box uses is strong, a major catalyst project was not recommended for the New Kings District. This is because available parcels and the potential for assemblages did not appear to offer realistic opportunities to package a parcel of the necessary size to support a “placemaking” project. If assemblage of a twenty-five (25) +/- acre site occurs, a “placemaking” project meeting the corridor design standards should be supported at the intersection of Old Kings Road and Pritchard Road. The proposed Placemaking Project located in the Soutel Place District at the New Kings Road and Soutel Drive intersection is well located to serve the Old Kings District.

Minor Redevelopment Projects

There is market is support for retail, office and housing in the District and a variety of development and projects are appropriate.

**Public Projects**

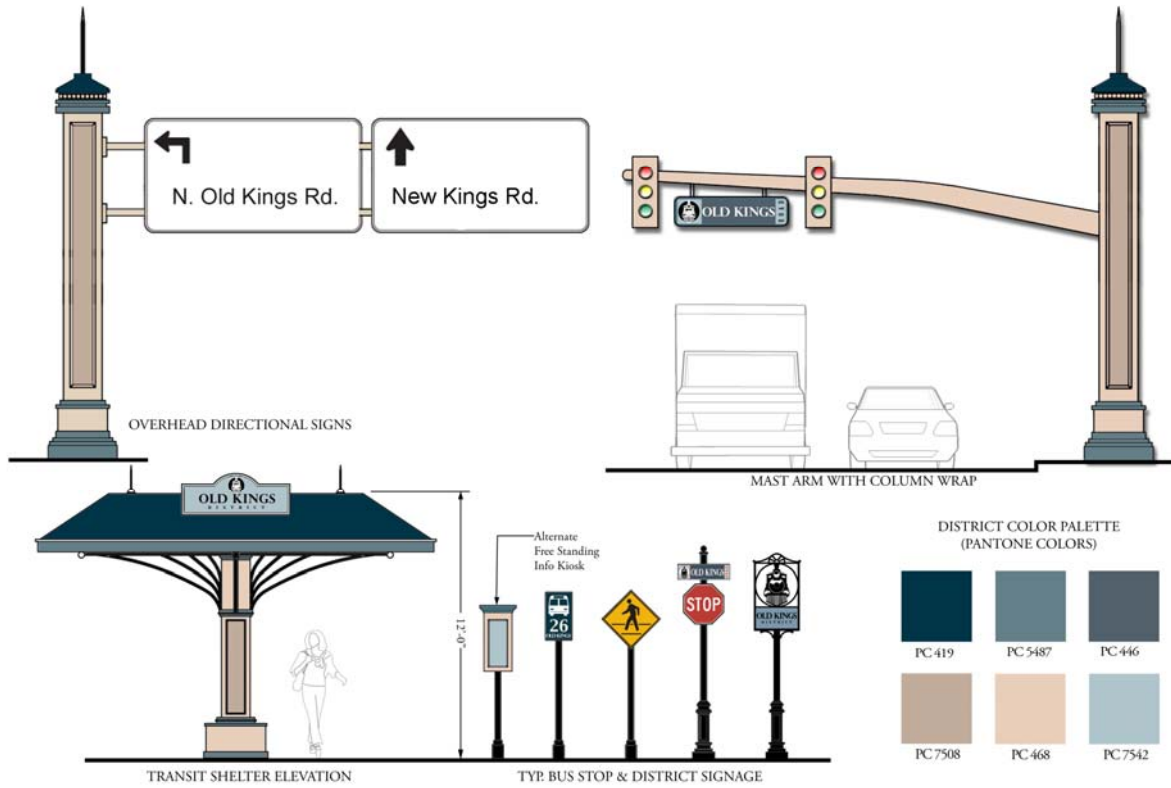
Wayfinding

The wayfinding program will establish a higher quality brand for the District.

Figure 12. – Old Kings Road Wayfinding



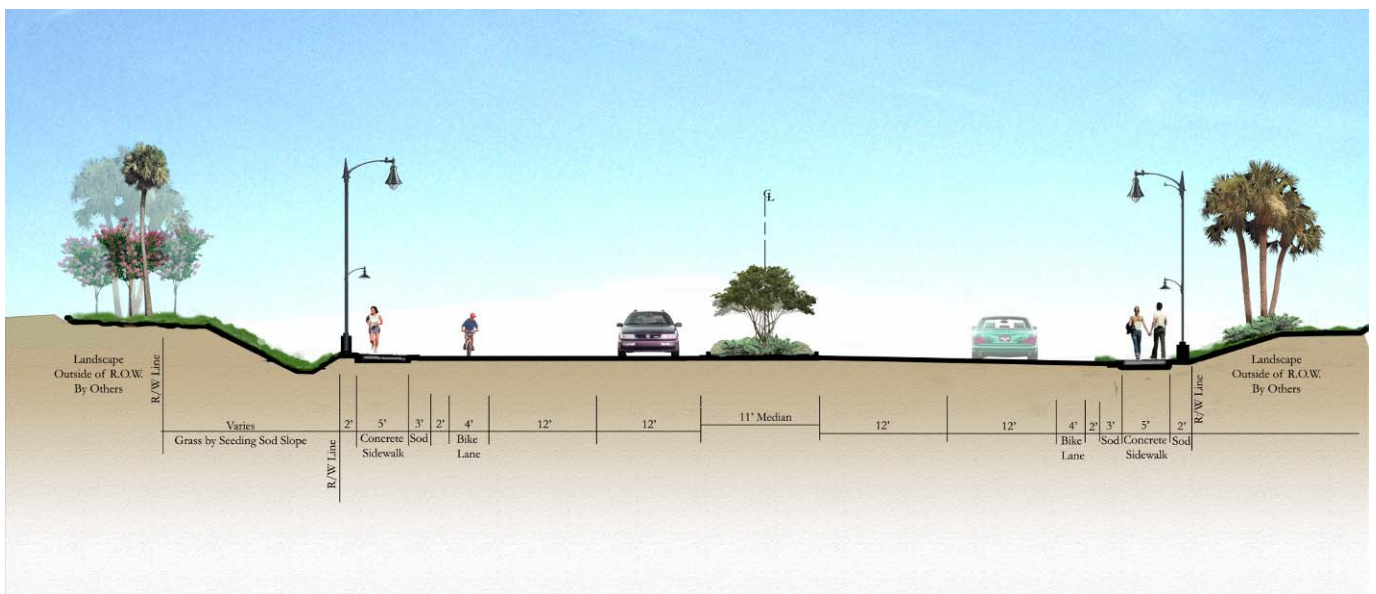
# KingSoutel Crossing (Corridor) Community Redevelopment Plan



## Streetscape

The new improvements that straighten the Soutel/Pritchard connection and providing curbed medians should be streetscaped.

**Figure 13. – Soutel/Pritchard Streetscape Improvements**





## KingSoutel Crossing (Corridor) Community Redevelopment Plan

**Table 11. – Old Kings Road Wayfinding & Streetscape Cost Estimate**

ITEM	Size	Qty.	Cost	Total
<b>Landscape and Irrigation-Median</b>				
Landscape Soil Improvements	SF	64,691	0.75	48,518.25
Canopy Tree	Ea.	135	1,200.00	162,000.00
Understory Tree	Ea.	48	450.00	21,600.00
Shrub Area	SF	4950	2.50	12,375.00
Sod Area	SF	59741	0.35	20,909.35
Irrigation	SF	64,691	1.25	80,863.75
<b>SUB-TOTAL</b>				<b>\$346,266.35</b>

ITEM	Size	Qty.	Cost	Total
<b>Landscape and Irrigation-Street</b>				
Sod Area	SF	62810	0.35	21,983.50
Irrigation	SF	62810	1.25	78,512.50
<b>SUB-TOTAL</b>				<b>\$100,496.00</b>

ITEM	Size	Qty.	Cost	Total
<b>Site Furniture Wayfinding, and Lighting</b>				
Bus Stop Signs and District ID Signs	Ea.	12	2,500.00	30,000.00
Transit Shelters (@ Max 1 Mile)	Ea.	2	45,000.00	90,000.00
Street Light Poles/Fixtures (150' O.C.)	Ea.	83	3,500.00	290,500.00
Ash/Waste Receptacle (@ 300')	Ea.	40	950.00	38,000.00
Benches	Ea.	40	1,600.00	64,000.00
<b>SUB-TOTAL</b>				<b>\$512,500.00</b>

**Old Kings District - Soutel Drive Streetscape \$959,262.35**

ITEM	Size	Qty.	Cost	Total
<b>General Conditions</b>				
Mobilization (5%)				47,963.12
Contingency (10%)				95,926.24
Maintenance of Traffic (4%)				38,370.49
<b>SUB-TOTAL</b>				<b>\$182,259.85</b>

**Old Kings District - Soutel Drive Streetscape \$1,141,522.20**

**Pritchard District**

The Pritchard Road interchange has created tremendous industrial development opportunities in surrounding areas because of highway access and large land holdings. The District has great potential for new development of all types and application of good design standards and quality public facilities can have a positive economic benefit to the surrounding areas.

**Map 9. – Pritchard Road District Map**



**Private Projects**

Major Catalyst Project

The District has a suitable parcel and strong market for a major catalyst project as follows:

*Pritchard Center Project:* A 52-acre site located in the southeast quadrant of Pritchard Road and Imeson Road is a portion of an approved PUD (Ordinance No. 2005-541-E). The approved PUD allows for a conventional shopping center with several out-parcels, a large 280,000 sf big box anchor, and a total of 343,500 sf of retail/commercial and 1780 parking spaces located in front of the building. The project currently has no “placemaking” characteristics.

The site has regional and interstate market. There are over 6,000 industrial jobs currently in the Westside Industrial submarket, and there is planned 1.2 million square feet of industrial distribution space and ten (10) acres dedicated to supporting retail/commercial uses in the Perimeter West Industrial Park. Along with the West Side Industrial Park, these two nearby industrial parks are continuing to develop and generate jobs. Thus, the PUD site is strategically located to provide housing for the industrial employment. Additionally, retail that services the day time populace and growing residential base offers a major development opportunity. The market appears to support a development program for a mix of uses that will allow the creation of a mixed-use project as follows:

Big Box Retail	200,000 sf
Attached Storefront Retail	100,000 sf
Out Parcels - Retail	33,000 sf
Office	44,000 sf

The approved PUD provides the residential component for the proposed project just south of the 52-acre site. Adding civic or government uses and recreation uses will strengthen the project. A public space for special events and informal events should be required.

A conceptual “placemaking” development concept is provided in Figure 6. The redesign includes a main street and locates the big box to serve an anchor for the storefronts. It must be noted that this site also provides an excellent opportunity for an industrial project.

Figure 14. – “Pritchard” Major Catalyst Project Concept

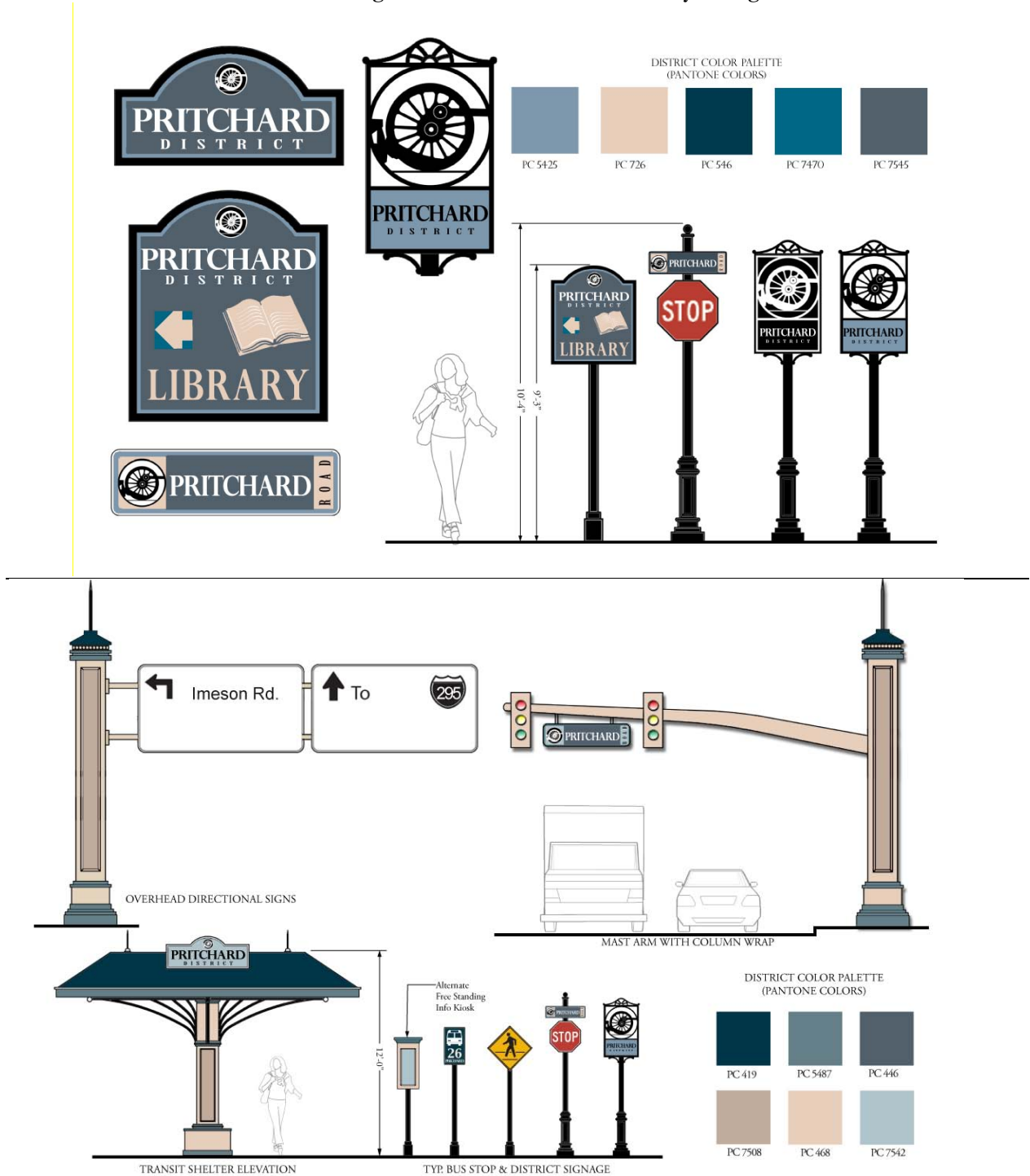


## Public Projects

### Wayfinding

The wayfinding program will establish a higher quality brand for the District.

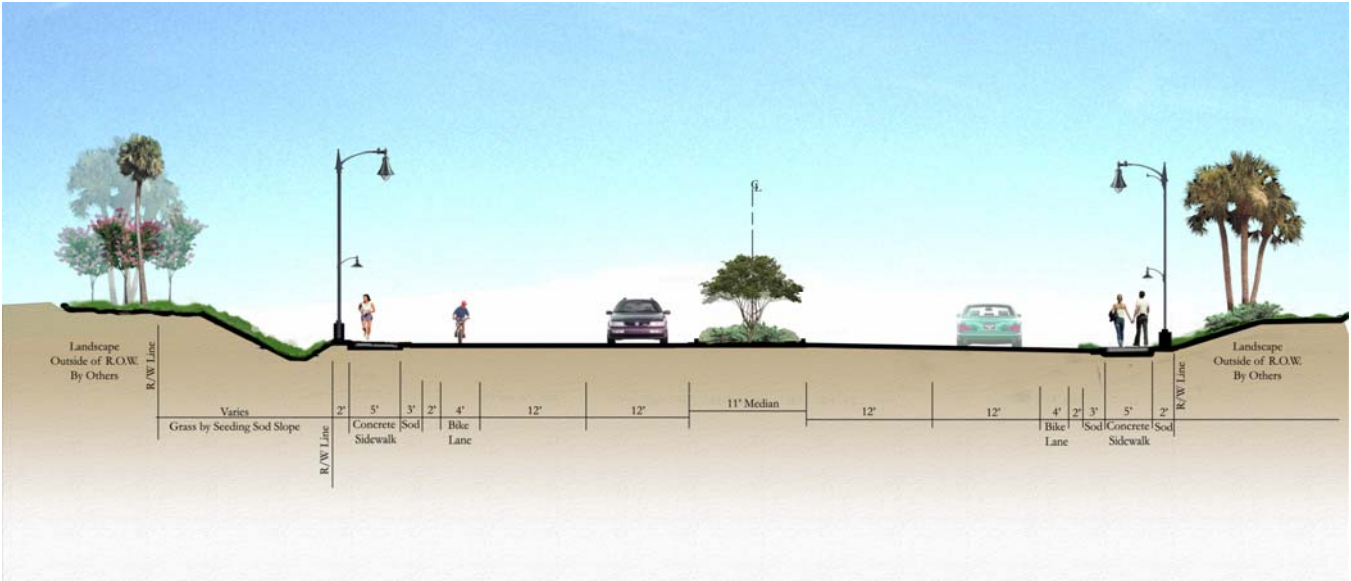
Figure 15. – “Pritchard” District Wayfinding



Streetscape

The new improvements providing curbed medians should be streetscaped.

Figure 16. – “Pritchard” District Streetscape



## Goal and Objectives

### Goal

To provide a blueprint for revitalizing the corridor and attracting modern private development through targeted public investment to create catalyst projects and establish a “sense of place” within the community.

### Objective 1

To focus the Redevelopment Program upon creating Major Catalyst Projects identified in the Corridor Vision and Master Plan that will help to spur investment in surrounding smaller properties including:

- Soutel Place Center: A mixed-use project containing a retail core of a nationally-recognized retail business on City owned property in Lonnie Miller Park and require that all impacted recreation facilities be replaced (Community Center, Gym Facility and the planned Bob Hayes Sports Complex).
- Kings Crossing Center: A mixed-use project located in the northwest quadrant of New Kings Road and Soutel Drive.
- Pritchard Center: A large mixed-use lifestyle center located in the southeast quadrant of Pritchard Road and Imeson Road or an alternate site.

### Objective 2

To establish conformity of all future corridor redevelopment to the City’s Commercial Design Standards for urban development, and ensure all Corridor Redevelopment Plan recommendations are pedestrian-oriented, “placemaking” design principles and standards that are incorporated into the following:

- Major Catalyst Projects
- Minor Redevelopment Projects
- Rehabilitation Projects

### Objective 3

To establish mandatory maintenance and appearance review requirements for improvements to existing buildings, and a façade grant program to match private investment into improvements that conform to the City’s Development Design Guidelines in Best Practices Handbook for urban buildings and “placemaking” standards, herein.

### **Objective 4**

Designate all parcels within the Soutel Place District as a Brownfield Area and establish a program to:

- Assist site remediation or clean-up (if needed)
- Mitigate any environmental hazard (if present)
- Obtain regulatory sign-off of the site (if needed)
- Identify funding to assist in the redevelopment of the site
- Promote redevelopment/reuse as an asset to the community
- Encourage economic development opportunities and job creation

### **Objective 5**

Reassemble and replat property for non-conforming, inaccessible and vacant lots using the powers conveyed to the Community Redevelopment Agency by Florida Statutes.

### **Objective 6**

Prioritize and phase the recommended public projects listed below and support infrastructure cost (long-term program) to maximize private investment in the major catalyst projects recommended in the “placemaking” plan:

- Gateways
- Wayfinding
- Streetscapes
- Bus Transit
- Sidewalk Connectivity

### **Objective 7**

Implement the recommended projects utilizing public and private partnerships and support their long-term success by improving public safety and basic infrastructure.



## Implementation Strategies and Actions

### Responsibility

To establish the JEDC as the agency responsible for managing the KingSoutel Crossing Corridor Redevelopment Program as follows:

- Provide staff support for a Community Redevelopment Agency Advisory Committee made up of local property owners.
- Coordinate the implementation actions with other City agencies that are responsible for specific duties.
- Coordinate with the City of Jacksonville Chamber of Commerce to promote the economic development of the area.

### Strategies

#### Major Catalyst Projects

To create a coordinated public investment strategy, the following steps should be pursued:

- **Step 1 - Adopt the Redevelopment Plan**  
City Council takes action to adopt the Redevelopment Plan, freeze the tax base, and establish tax increment financing.
- **Step 2 - Establish Public Agency Responsibilities**  
The JEDC will manage the Redevelopment Program and coordinate with other City agencies to implement public investments and other recommendations of the Plan.
- **Step 3 - Determine the Priorities for Project Implementation**  
The private realm projects that are to be pursued should be determined and prioritized. The recommended priorities are:
  - Soutel Place Center
  - Kings Crossing Center
  - Pritchard Center

- **Step 4 - Determine the Incentive Package**

Determine the public realm cost of improvements that support the private realm project including:

- Gateways
- Wayfinding
- Streetscapes
- Bus Transit
- Sidewalk connectivity
- Utilities
- Any other eligible cost such as:
  - Infrastructure cost that are not already included in the City’s Capital Improvement Plan (“CIP”) and as prescribed by the Community Redevelopment Act that may be required to implement the Corridor Redevelopment Plan.
  - Site consolidation, preparation, and disposal.

- **Step 5 - Issue Request for Proposal(s) (“RFP”)**

As a prerequisite for making for public improvements, a RFP should be issued to establish a process for selecting a developer, negotiating a Disposition, and negotiating a Development Agreement that identifies the following:

- The developer requirements for creating the “placemaking” project.
- The public investment commitments to support the project.
- Describes the development program characteristics and schedule for financing and construction management.

### Rehabilitation and Minor Redevelopment Projects

To encourage rehabilitation and minor redevelopment projects the City will consider:

- **Step 1**

Requiring that all future development within the Soutel Place District and all major catalyst projects meet the City’s Development Design Guidelines in Best Practices Handbook for urban buildings, and meet the design principles and standards for “placemaking” in this plan.

- **Step 2**

Adopting a mandatory maintenance and appearance code as well as review requirements for improvements to existing buildings.

- **Step 3**  
Establishing an architectural review board, or designate a staff to perform reviews of building façade improvements.
- **Step 4**  
Establishing a façade matching grant program to match private investment into improvements that conform to the City’s Development Design Guidelines in Best Practices Handbook for existing urban buildings and “placemaking” standards.

### Public Projects

The JEDC shall provide management oversight of other City Agencies in the implementation of the Corridor Redevelopment Program including the following processes:

- Capital Improvements Program (“CIP”)
- Capital Budget Process and Funding
- Design/Bid Specifications
- Permitting
- Construction

### Actions

The JEDC will focus redevelopment implementation actions on four (4) areas as follows:

- **Action 1: Future Land Use and Zoning**  
Any proposed changes in this Plan or requested changes will be reviewed for approval by a Community Redevelopment Agency (“CRA”) Advisory Committee and will be processed as required by City and State Statutes.
  - Parcels fronting the Corridor that currently have a low density residential future land use should be considered for application of an Overlay Zone that would allow non-residential uses to be developed under the condition that parcels be assemble to a size that will support all modern regulatory standards and requirements and be consistent with the Sherwood Forest/Lincoln Villas NAP.
  - Major catalyst projects shall be required to utilize PUD Zoning.
  - Mandatory Maintenance and Appearance Requirements shall be adopted for application to existing buildings.
  - Design principles and standards recommended in the “placemaking” section in this plan shall be incorporated into the city’s review process.

- **Action 2: Private Project Recommendations Involving Site Consolidation, Preparation and Disposal**

Consistent with the authority provided by the Community Redevelopment Act, the CRA will prepare real property through acquisition, clearance, demolition, and other improvements that may be necessary to induce private investment into the major catalyst projects or projects meeting the design principles and standards for major catalyst projects. All property acquired by the City/CRA shall be sold or leased at fair value in accordance with the KingSoutel Crossing Corridor Redevelopment Plan and as required by the disposition procedures of the Community Redevelopment Act. In disposal of property, the City/CRA may impose the conditions necessary to ensuring that the development occurs in accordance with the redevelopment plan and a reasonable development schedule.

- **Action 3: Public Project Recommendations**

The proposed improvements to carry out the KingSoutel Corridor Redevelopment Plan and any supporting infrastructure improvements (refer to Action 4, below) may be programmed and financed as provided under the community Redevelopment Act. The JEDC will manage the long-term implementation program that includes the recommended projects and cost as described in the section titled District “Placemaking” Plan and other eligible projects necessary to implement the Plan. The Soutel Place District public project is recommended to be the first priority project. Depending on funding, the CRA may elect to select all or significant components of this list of projects including:

- Major Gateway treatment at the New Kings Road and Soutel Drive intersection - \$1,391,000.00.
- Streetscape and Wayfinding through the District - \$936,000.00.
- Two Minor Gateways - \$1,900,000.00.

- **Action 4. Public Infrastructure Improvements**

The maintenance of existing infrastructure and any planned improvements in the City’s CIP or Neighborhood Action Plans shall continue and occur without any financial support from the Corridor Redevelopment Program. The projects identified in the Public Project Recommendations section above, must be supported by improvements to basic infrastructure including such utilities as potable water, sewer, stormwater gas, and electric.

## Financial Plan

### Sources of Redevelopment Funding

The City provided the initial funding for the Finding of Necessity and the KingSoutel Crossing Corridor Community Redevelopment Plan. In order to implement the Plan, the City shall use multiple funding sources from City funding programs, private funding, and State and Federal sources. General funding methods and sources that may be used to implement the Plan are as follows:

#### Local Funds

If available, general fund revenues and enterprise fund revenues can be used to finance redevelopment activities. General fund revenues may be the source of funding for staffing assistance to the redevelopment program. Enterprises of local government may be used to fund the recommended projects and system improvements in the redevelopment area.

#### Tax Increment Financing (“TIF”)

TIF is a power delegated by the Community Redevelopment Act to provide the Community Redevelopment Agency with local funding. It provides a principle source of revenue for use by the redevelopment agency for public expenditure associated with public or private projects. A Redevelopment Trust Fund has previously been established by as part of the City’s redevelopment program. For each taxing year following adoption of the Redevelopment Plan, ad valorem taxes generated by the assessed taxable real property value in excess of the area’s base year total will be deposited into the trust fund, which will be in an account specific to the KingSoutel Crossing Corridor - Community Redevelopment Area. The methodology for determining the amount of tax increment to be contributed each year shall be as provided in F.S.163.387 and may be amended.

All funds deposited into the Trust Fund shall be available to the Redevelopment Agency for any lawful purpose as defined in Chapter 163 F.S. in fulfillment of the provisions of the Redevelopment Plan and any amendments thereto, except as may be provided below.

It shall be the policy of the City that these funds may be spent toward projects that reduce or eliminate blight and enhance or cause the addition of taxable value of the property in the Redevelopment Area.

Such uses of funds are summarized below and may include the following:

- All cost associated with the design, permitting, financing, installation, and construction of blight reducing projects listed in the section below.
- Acquisition of properties within the blighted area for the purpose of constructing the listed projects and initiation of private development projects to support tax increment bonds.
- Disposition of property acquired in the redevelopment area at its fair value for uses in accordance with the Redevelopment Plan.

### **Advances and Loans**

The Redevelopment Agency shall have the power to borrow money and accept advances from any source, public or private, including the City, for any lawful purpose in connection with the redevelopment program. Tax increment revenues or any other funds deposited in the Redevelopment Trust Fund, which are not otherwise obligated, shall be available for repayment of such loans or advances. Such loans will be underwritten subject to the same criteria and limitations as described above.

Further more, the Agency may enter into a contract with the City or other public corporation under which the Agency agrees to reimburse the City or other public corporation for all or part of the cost of any eligible improvement by periodic payments over a period of years.

The obligation of the Agency under such contract shall constitute an indebtedness of the Agency as a result of a community redevelopment project, as per Section 163.387 (3) F.S.

### **Tax Increment Revenue Bonds**

When authorized by an ordinance of the City Commission, the City shall have the power to issue negotiable redevelopment revenue bonds to finance the undertaking of any community redevelopment eligible project activity, including the payment of principal and interest upon any loans and advances previously incurred and for the refunding and retirement of bonds or other obligations previously issued. The security of such bonds may be based upon anticipated tax increment revenues of the private components of redevelopment projects, general tax increment improvements in the Redevelopment Area, and other such revenues as may be available.

### **Bond Anticipation Notes**

The City Commission may also issue bond anticipation notes and may renew the same from time to time, but the maximum maturity of any such note, including renewals thereof shall not exceed five (5) years from the date issued of the original note. Such notes may be paid from tax increment revenues, the proceeds of sale of revenue bonds in anticipation of which they were issued, or any other funds in the Redevelopment Trust Fund not otherwise obligated.

### **Redevelopment Trust Fund Earned Interest**

Any interest earned from savings institutions from deposits of Trust Fund moneys shall become part of the redevelopment moneys available to the Redevelopment Agency for use in financing eligible redevelopment projects.

### **Sale or lease of Acquired Property**

All monies received from the sale of real property acquired by the Redevelopment Agency shall be deposited into the Trust Fund.

### **Grants**

The Redevelopment Agency and the City will seek to utilize Federal or State grant programs as are applicable and available for any eligible project to reduce blight or any other approved use within the redevelopment project area. Locally funded sources are available through the Northwest Jacksonville Economic Development Fund including:

- Business Infrastructure Grant/Loan (“BIG”)
- Large Scale Economic Development Fund
- Small Business Development Initiative
- Commercial Development Area
- Façade Renovation grant

## Tax Increment Projections

Trends in assessed valuation were analyzed and the potential for new development was assessed based on the evaluation of market projections and the potential for new development projects along the corridor. (Refer to Appendix "Tax Increment Projections.")

Basic assumptions were made in order to project potential tax increment revenue, some. These assumptions are listed as follows:

- The current assessed value is set as the base year for calculation of tax increment.
- The current applicable mileage rate of 9.65 per 1,000 dollars is held constant throughout our analysis.
- Base year assessed value is projected to increase annually by two and one-half (2.5) percent.
- The concepts of project site development that were considered feasible as described in the Phase II of the study leading to preparation of this Plan and titled "***Conceptual Alternatives Analysis***" were phased into the TIF projections according to their estimated time horizons for development.
- The increase in assessed values and tax revenues from potential development projects is based on the cost of new buildings and not land, which, as previously stated, is projected to increase a two and one-half (2.5) percent.
- The cost or sales values of the buildings are projected to grow three (3) percent annually.
  - Retail building cost is initially estimated at \$115 per square foot.
  - Office building costs are initially estimated at \$125 per square foot.
  - Town home building initially valued at \$130,000 per unit; single-family at \$200,000 per unit; and condos at \$180,000 per unit.
  - Industrial space is initially valued at \$50 per square foot.
- Taxable values are estimated as a percent of market value: Residential 70 percent, all others at ninety (90) percent.
- New ad valorem values are projected to increase two and one-half (2.5) percent annually after initial construction.

Based upon the application of assumptions, the detailed annual TIF revenue projections are reflected in Table 5. Estimated TIF revenue generation over twenty (20) years is conservatively projected to be approximately \$42 million.



## Proposed CRA Project Cost and Sequence

The proposed project cost of the long term redevelopment program and generalized sequence of priorities is as follows:

- Priority 1: Soutel Place District from \$400,000 to \$6,400,000
- Priority 2: Pritchard District \$574,000
- Priority 3: New Kings District \$370,000 to \$7,400,000
- Priority 4: Old Kings District \$400,000 to \$1,141,000

The CRA may elect to prioritize the major components of the Districts instead of completing all of the District components in one (1) phase.

The recommended sequence of project implementation is as follows:

- Step 1: City Council adopts Community Redevelopment Plan
- Step 2: City issues RFP for A Major Catalyst Project
- Step 3: City adopts Development Agreement for Major Catalyst Project
- Step 4: City established funding mechanism then Programs and Budgets Public Project Implementation
- Step 5: City and Developer construct projects consistent with the Development Agreement

The City may elect to construct other gateway, wayfinding, streetscape, and sidewalk projects through a long-term program investment that is independent from implementation of major catalyst projects.

## Management Plan

### Private Sector Involvement

The CRA is authorized to enter into Development Agreements with owners of property in the redevelopment area. Owners shall have the opportunity to submit proposals to the Redevelopment Agency to carry out development activities on property they own or control. The Agency shall consider all proposals submitted including plans specifications, financial and legal ability, time's schedules, terms and conditions, and any other information required by the Agency. The Agency may accept any such proposals deemed to be in the public's interest, in furthering of the purpose of the Redevelopment Plan (i.e. to reduce or eliminate transportation blight) and the Comprehensive Plan, and conforming with all applicable development regulations.

### CRA Development Agreement Procedures

The CRA is authorized to issue Request for Proposals for Redevelopment Projects and/or enter into Development Agreements with a developers to specify the terms, conditions, and schedules controlling private development that will provide the tax increment necessary to fund or assist in funding the CRA Improvements as well as the funding and scheduling of such public improvements. If acquisition of ownership or any other interest in real property is contemplated, the CRA shall advertise and issue a RFP for a Redevelopment Project for the area to be redeveloped/developed meeting all the public notice requirements. Upon adequate investigation of all proposals, the agency may negotiate with any and all or no parties involved and may accept such proposal as the agency deemed to be in the public's interest and in furthering of the purpose of the Redevelopment Plan.

Once a Disposition and Development Agreement is approved by the CRA, the CRA is authorized to transfer ownership or any other interest in any real property by sale, lease, exchange or any other legal means. The transfer of real property or interest therein may be to any private or public entity.

### Disposition and Development Documents

The CRA shall reserve such powers and controls through disposition and development documents with purchasers and lessees as may be necessary to prevent transfer, retention, or use of property for speculative purposes, and to insure that development begins within a reasonable period of time, as defined by the Agency.

To provide adequate safeguards that the provisions of this Plan or its Amendments will be carried out, all real property sold, leased, or conveyed by the Agency as well as all property subject to owner participation agreements, shall be made subject to the provisions of this plan by lease, deeds, contracts, agreements restrictions, or other means.

The leases, deeds, contracts, or other forms of agreement may contain restrictions, covenants running with the land, rights or reverter, conditions subsequent, equitable servitude, or any other provisions necessary to carry out this Plan.

### **Community Redevelopment Agency Powers and Responsibilities**

The Redevelopment Act sets forth the powers, responsibilities and duties of a CRA. The CRA shall have all powers available to it under the Redevelopment Act, as may be amended, to carry out this plan to the extent not limited by this Plan.

Additionally, the Redevelopment Agency and its designated staff or the JEDC will be responsible for the following functions:

- Provide continuous planning services to the redevelopment program.
- Administer the Redevelopment Program.
- Supervise/monitor the design, permitting, financing, and development of CRA Improvements to reduce or eliminate blight.
- Prepare budgets for operating and capital expenses.
- Negotiate and recommend Development Agreements.
- Reviewing and approving private development proposals in accordance with the Redevelopment Plan.
- Coordinating redevelopment activities with all public agencies and departments of the City.
- Developing and supervising promotional and marketing programs.

### **Consistency with other Plans and Regulations**

All development activities carried out within the Redevelopment area shall be developed in conformance with the Sherwood Forest/Lincoln Villas NAP and the City's Comprehensive Plan as exist now or is amended from time to time. Project located in the Redevelopment Area shall be developed in conformance with the City's Land Development Regulations as may be amended from time to time.

### **Modifications to the Redevelopment Plan**

The Redevelopment Plan is to be adopted by ordinance by the City Council. From time to time, modifications may be made to the Plan in the form of an Amendment(s) shall be approved by the City Council following the required prior advertised public hearing in accordance with Florida's Community Redevelopment Act (Section 163.361.F.S.).

### **Severability**

If any provisions, sections, subsections, section, clause, or phase of the redevelopment plan is, for any reason, held to be invalid or unconstitutional, such decision shall not affect the validity of the remaining portion or portions of this Plan.

**Project Close Out**

Following the completion of the CRA Improvements and the maturity, payment or defeasance of all financial obligations for the CRA Improvements, the CRA's responsibility for the Redevelopment Area shall cease.

## KingSoutel Crossing (Corridor) Community Redevelopment Plan

**Table 12. – Pritchard Road Wayfinding and Streetscape Cost Estimate**

ITEM	Size	Qty.	Cost	Total
<b>Landscape and Irrigation-Median</b>				
Ex. Landscape Grubbing and Removal	SY	1,165	5.00	5,825.00
Landscape Soil Improvements	SF	25,432	0.75	19,074.00
Canopy Tree (@ 40' O.C.)	Ea.	50	1,200.00	60,000.00
Understory Tree	Ea.	32	450.00	14,400.00
Shrub Area (75' L. beds at median tip)	SF	3300	2.50	8,250.00
Sod Area	SF	22132	0.35	7,746.20
Irrigation	SF	25,432	1.25	31,790.00
<b>SUB-TOTAL</b>				<b>\$147,085.20</b>
<b>Landscape and Irrigation-Street</b>				
Landscape Soil Improvements	SF	25,120	0.75	18,840.00
Sod Area	SF	25120	0.35	8,792.00
Irrigation	SF	25120	1.25	31,400.00
<b>SUB-TOTAL</b>				<b>\$59,032.00</b>
<b>Site Furniture Wayfinding, and Lighting</b>				
Bus Stop Signs and District ID Signs	Ea.	12	2,500.00	30,000.00
Transit Shelters (@ Max 1 Mile)	Ea.	2	45,000.00	90,000.00
Street Light Poles/Fixtures (@ 150' int)	Ea.	33	3,500.00	115,500.00
Ash/Waste receptacle (@ 300' int.)	Ea.	16	950.00	15,200.00
Benches	Ea.	16	1,600.00	25,600.00
<b>SUB-TOTAL</b>				<b>\$276,300.00</b>
<b>Pritchard District - Soutel Drive Streetscape</b>				<b>\$482,417.20</b>
<b>General Conditions</b>				
Mobilization (5%)				24,120.86
Contingency (10%)				48,241.72
Maintenance of Traffic (4%)				19,296.69
<b>SUB-TOTAL</b>				<b>\$91,659.27</b>
<b>Pritchard District - Soutel Drive Streetscape</b>				<b>\$574,076.47</b>

## Neighborhood Impact Element

### Traffic Circulation

The Plan will have a positive impact upon the current traffic circulation by:

- Implementing traffic calming mechanism and safety improvements through the streetscape projects.
- Providing new bus transit stations with a focus on integrating this public use into the design of major catalyst projects.
- Providing needed sidewalk connectivity to neighborhoods.
- Creating pedestrian-oriented, mixed-use projects that will reduce trip lengths for shopping and business needs.

### Environmental Quality

Environment quality will improve as a result of redevelopment efforts that reduce and eliminate blight including brownfields. In order to redevelop a site in the Soutel Place District, an environmental assessment needs to be performed. If environmental regulations require remediation, the Brownfield designation will provide the tools necessary to complete the project through resources and incentives. Streetscapes will improve the quality and safety of the environment, and the associated water, sewer, and stormwater improvements will enhance the environmental quality of the redevelopment area. Enforcement of building codes and mandatory maintenance codes will eliminate deteriorated building conditions.

### Availability of Community Facilities and Services

The corridor redevelopment area is underserved by retail, entertainment, and local serving office uses. The implementation of the redevelopment program to create major catalyst projects and spur investment in existing buildings will provide better community facilities and services in a much more attractive environment. New jobs will be created and local residents will not have to travel as much for needed services. Public facility improvements will provide improved vehicular, bike, and pedestrian access to improved public and private community facilities.

### Effect on School Population

The implementation of the redevelopment plan will have only a very minor impact upon the City's population or population density pattern and, therefore, not adversely impact the public school system.

### Consistency with Neighborhood Plans

The implementation of the redevelopment program is supportive of the Sherwood Forest/Lincoln Villas Neighborhood Action Plan for surrounding residential areas.

## Compliance with Florida Statues

### 1. Legal description of the Boundaries and the Location Rational

The Redevelopment Area contains approximately 1941 acres, including the rights of way. The legal description for the Redevelopment Area is set forth in Appendix 3.

The two (2) Finding of Necessity “Blight” Analyses, referenced in the City’s Blight Resolutions, attached as Appendices 1 and 2, provided the basis for determining the boundary of the Redevelopment Area. The analysis supported The Finding of Necessity conclusions that the area contains blighting conditions and conditions of economic disuse. The location rational is provided in the blight studies and addressed in the section titled “Introduction”, herein.

### 2. Show by diagram and in general terms the approximate amount of open space to be provided and the street layout.

The Corridor Vision and Redevelopment Master Plan Map, herein, on page 20, illustrates the Corridor Community Redevelopment Plan and proposed street layout. The Redevelopment Area is currently comprised of a mix of public and private developed and vacant land, which are identified and mapped by land use type in the Blight Studies referenced above. Future development within the Redevelopment area will comply with the open space requirements of the City’s Comprehensive Plan which will cause the creation of significantly more open space.

- **Limitations on the type, size, height, number of proposed buildings.**

The type, size, height, number, and proposed use of buildings must conform to the Land Development Regulations of the City of Jacksonville. The Community Redevelopment Plan recommends additional design principles and standards.

- **The approximate number of dwelling units.**

The corridor redevelopment area is primarily devoted to commercial uses. Future residential units may occur as components of vertically or horizontally integrated mixed-use projects or independent subdivisions or multi-family complexes. There is a limited market and land available for new units within the redevelopment area. The residential growth is expected to occur predominantly within the surrounding neighborhoods.

- **Such property as intended for use as public parks, recreation areas, streets, public utilities, and public improvements of any nature.**

The “Corridor Vision and Redevelopment Master Plan Map” illustrates the Redevelopment Plan indicating proposed redevelopment use and street improvements. The City’s Land Development Regulations and this Community Redevelopment Plan include design standards that will require pedestrian-friendly streetscapes and pedestrian plazas that are implemented through the development permitting process. The Community Redevelopment Plan recommends the construction of street and sidewalk improvements and related utility and other public improvements

3. **If the redevelopment area contains low or moderate income housing, a neighborhood impact element which describes, in detail, the impact of the redevelopment upon a) the residents of the redevelopment area and the surrounding areas in terms of relocation, b) traffic circulation, c) environmental quality, d) availability of community facilities, e) effect on school population, and other matters affecting the physical and social quality of the neighborhood.**

Please refer to the Section titled “Neighborhood Impact Element”.

4. **Identify specifically any public funded capital projects to be undertaken within the redevelopment area.**

The major projects proposed with the redevelopment area, that may be all or partially publicly funded, are in the sections titled:

- “Corridor Vision and Master Plan”
- “District “Placemaking Plan”
- “Implementation Strategies and Actions”
- “Financial Plan”

5. **Contain adequate safeguards that the work of the redevelopment plan will be carried out pursuant to the plan.**

The CRA will carry out the redevelopment work. The redevelopment process has been established and is consistent with Chapter 163, Part III, Florida Statutes. It is the intent of the City Council to comply with those requirements as established in Chapter 163, Part III, of the Florida Statutes as it has since the CRA was established in 1979. The requirement is addressed in the Section titled “Management Plan”.



6. **Provide for the retention of controls and the establishment of any restrictions running with land sold or leased for private use for such periods of time and under such conditions as the governing body deems necessary to effectuate the purposes of this plan.**

This requirement shall be carried out as stipulated in the section titled “Management Plan”

7. **Provide assurances that there will be replacement housing for the relocation of persons temporarily or permanently displaced from housing facilities within the redevelopment area.**

There is no relocation proposed.

8. **Provide an element of residential use in the redevelopment area should such use exist in the area prior to the adoption of the plan, or if the plan is to remedy a shortage of housing affordable to low and moderate income residents, including the elderly, or if the plan is not intended to remedy such shortage, the reason therefore.**

Residential uses are especially recommended as components of mixed-use “placemaking” projects. The Redevelopment Plan is not intended to remedy a shortage of affordable housing because a shortage does not currently exist.

9. **Contain a detailed statement of the projected cost of redevelopment, including the amount to be expended on publicly funded capital projects in the redevelopment area and any indebtedness of the CRA, the County, or the Municipality proposed to be incurred for such redevelopment if such indebtedness is to be repaid with increment revenues.**

The detailed statement of projected cost of redevelopment is provided in the sections titled:

- “Corridor Vision and Master Plan”
- “District “Placemaking Plan”
- “Implementation Strategies and Actions”
- “Financial Plan”

The Section titled “Financial Plan” provides for the use tax increment and tax increment bonds to repay indebtedness incurred for redevelopment.

10. **Provide a timeline/schedule for completing all redevelopment if financed by increment revenues. Such time certain shall occur no later than thirty (30) years after the fiscal year in which the plan is approved or adopted.**

It is anticipated that the redevelopment projects will require no more than thirty (30) years to complete after adoption of or amendment to the Redevelopment Plan.

# **APPENDIX 1**

## **SOUTEL/MONCRIEF RETAIL REDEVELOPMENT AREA**

### **EXHIBIT A. “BLIGHT STUDY”**

### **EXHIBIT B. “FINDING OF NECESSITY RESOLUTION”**



# Proposed Soutel/Moncrief/Retail Redevelopment Area Community Redevelopment Area Redevelopment Plan

Lincoln/Sherwood  
in development

Riverside Scenic



New Kings Corr  
corr in developm

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**COMMUNITY REDEVELOPMENT ACT  
SMRRA Community Redevelopment Plan**

**I. PROJECT DESCRIPTION  
AND OVERVIEW**

The Community Redevelopment Act (CRA), Chapter 163, Part III, F.S., provides legislative authority for local governments to use bold new approaches to overcoming the burdens of slum and blighted areas within the state. The CRA outlines a comprehensive program that provides the legal framework and financing mechanisms with which local governments can undertake the unique and highly complex task of overcoming the deterioration so common to older areas of Florida's cities.

Through enactment of the CRA, the Legislature recognizes that slum and/or blighted areas impose a costly burden on local communities in terms of service requirements and the tax revenues loss. Slum and blighted conditions also menace the general health, safety, morals and welfare of area residents. Furthermore, slum and blighted areas impair and arrest sound growth – growth that is needed if the City is to appropriately accommodate the population increases and economic expansion of the community.

Before the CRA was enacted, local governments were limited in encouraging improvements in blighted areas through development management plans and selected local controls. Cities did not have the power to attract the needed private development, but only to prevent inappropriate development. Even the provision of public improvements was greatly restricted by fiscal deficiencies. The CRA provides increased powers to both initiate desired private development and the financial means to pay the costs of public improvements, while also assisting in funding key portions of the private effort.

To take advantage of these increased powers authorized by the CRA, a local government must follow statutory procedures for declaring areas in need of redevelopment

through a slum and blight study, establishing redevelopment powers, preparing a redevelopment plan, instituting a trust fund and designating, if necessary, a tax increment financing district.

The blight study or “Finding of Necessity” was established, presented to the public for response and subsequently, approved by City Council. The purpose of this document is to establish redevelopment powers, prepare a redevelopment plan, and institute a tax increment financing district.

**The Soutel / Moncrief / Retail  
Redevelopment Area Blight Study**

The Soutel / Moncrief / Retail Redevelopment Area (SMRRA) Blight Study accomplished and summarizes data and information assembled by the Jacksonville Economic Development Commission to identify conditions of blight within the SMRRA. The general location of the SMRRA is shown on Map 1-1. This study fulfilled the following necessary functions:

1. Established the existence of conditions specifically identified in the definition of "Blighted Area" (Ch. 163.340(8), F.S.); and
2. Identified specific problems which exist and must be addressed in the redevelopment plan if a successful program of comprehensive redevelopment is to be realized.

The Blight Study identified factors that have impaired development and investment in the project area and the following SMRRA Community Redevelopment Plan (CRA) provides the strategies to guide the redevelopment to accommodate modern future commercial uses. Redevelopment will provide lacking retail services for area residents, an increase in the community's tax base, improved area appearance and job opportunities.

## **Community Redevelopment Act SMRRA Community Redevelopment Plan**

### **The Project Area**

The SMRRA located at the intersection of Soutel Drive and Moncrief Road. The area includes Lonnie Miller Park and various properties located north of Soutel Drive between Ageson Road to Archery Avenue and various properties south of Soutel Drive approximately Ageson Road to Norfolk Boulevard and further defined in the attached.

The boundary includes approximately 211 acres on the northeast, southeast and southwest corners of the intersection and the primary retail area along both sides of Soutel Drive. The SMRRA contains public facilities, recreational, commercial and medium-density residential zoning. The project boundary of the SMRRA is shown on map 3-1 and a legal description is in **Appendix A.**

Much of the SMRRA was platted during the 1930s and 1940s, with the principal focus of development along Soutel Drive and Moncrief Road. The Soutel corridor business district was a hub of the African-American community in Jacksonville and historically served as one of its retail districts. The area began to change dramatically with a development boom on the Southside of Jacksonville particularly with the opening of the Regency Square Mall in the early 1960s. African-Americans were now able to shop and work in other parts of the City and many moved out of the neighborhood as a result.

Since the early 1980s, the City of Jacksonville has made concerted efforts to revitalize, redevelop and improve the quality of life in SMRRA as well as surrounding areas which collectively comprise the northwest quadrant as a whole. The study area marks a focal point for strategic redevelopment that would kick start a new era of developer-generated interest.

In 2001, by Executive Order, the City established a Northwest Jacksonville Economic Development Advisory Committee to assist in the development, establishment and administration of projects in the Northwest area. The Northwest Jacksonville Economic Development Fund was created appropriating \$25 million for the Small Business Initiative Program, and the Northwest Area Façade Program. In the immediate vicinity of the SMRRA, revitalization has concentrated on two areas, the Soutel Drive Corridor and the Moncrief Road Corridor, which straddles Lonnie Miller Park, a key parcel in the revitalization and redevelopment effort with a vision to revitalize the area by enhancing the retail and commercial district, with shops, restaurants and services.

### **Boundary Justification**

The City of Jacksonville through the Jacksonville Economic Development Commission (JEDC) has encouraged growth and development in the commercial and retail sector of the local economy. The JEDC outlines four (4) of its primary objectives used throughout the city that will highlight growth in the SMRRA. They include: recruit and expand higher-wage jobs; increase the growth and expansion of small business; promote and encourage private investment; and promote and leverage investment in economically distressed areas. The JEDC objective further identifies four (4) additional actions that may guide redevelopment efforts in the SMRRA and are as follows:

1. Identify sites for large-scale development opportunities.
2. Utilize tools made available to local governments via Chapter 163 F.S. where appropriate.
3. Implement land banking and cooperative property assembly strategies for future development.



**Community Redevelopment Act  
SMRRA Community Redevelopment Plan**

4. Seek creative financing opportunities for new development in conjunction with the private sector.

At the Citywide level, the planning policy process will likely result in comprehensive plan and land development code changes that will positively impact the shape and form of redevelopment opportunities in SMRRA. Most importantly, by facilitating and creating development sites within this corridor the City can promote the near term development of neighborhood commercial/retail projects. The redevelopment of the Soutel Drive-Moncrief Road corridors is necessary to enhance the interest of quality retailers and to complete a corridor improvement program.

## **II. PLANNING EFFORTS AFFECTING SMRRA**

Inside the immediate planning area, the planning efforts have been extensive with the development of two (2) Neighborhood Action Plans (NAP), a Corridor Study, the Social Compact Jacksonville Neighborhood Market Drilldown Profile and the Buxton Retail Site Determination. These studies guided the City with strategic planning documents to coordinate the many revitalization activities occurring in the northwest quadrant area. A team was selected to work with the City on this Soutel / Moncrief / Retail Redevelopment Area project, to inventory information related to the existing redevelopment area plans, neighborhood plans and special projects and programs in the City and then develop an economic positioning strategy.

After a series of meetings with neighborhood focus groups, community leaders and stakeholders to gather input, final recommendations were developed and taken back to the community for input and endorsement. The City Council of the City of Jacksonville will rely on the NAPs to guide future policy and devise strategies that integrate planning, neighborhood and economic development principles to attain the expressed goals of the community which must rely on services provided only the establishments in the SMRRA.

One of the core findings of the NAPs and Social Compact Study was the desire of the citizens to strengthen opportunities for neighborhood commercial development which would provide basic needs such as groceries and sundries. Providing neighborhood retail would allow residents of the SMRRA (as well as all residents of the northwest quadrant) to shop near their homes helping to reduce the need for extended travel to obtain basic services, as

well as, fostering community through day-to-day and informal contact with neighbors.

While promoting neighborhood retailing opportunities, the citizenry sought to harmonize this development with its surroundings by promoting design which brought buildings closer to the street edge to provide a framework for the street which would help slow traffic and offer pedestrian connections to residential areas. These ideals for neighborhood and commercial development will continue and reinforce the traditional development pattern in the SMRRA.

The City of Jacksonville has identified the proposed SMRRA as an appropriate site for a neighborhood commercial redevelopment opportunity in the northwest quadrant of Jacksonville.

### **Neighborhood Planning and Revitalization Activities**

The SMRRA falls within the boundaries of several strong neighborhood associations such as the Moncrief Improvement Association, Fairway Oaks Homeowner's Association, and the Royal Terrace Community Association as well as the Northwest Citizens Planning and Advisory Committee (CPAC). The following summarizes the Neighborhood Action Plans and other planning initiatives as they relate to the corridor.

#### Lem Turner/ Ribault Scenic Drive Neighborhood Action Plan

The Lem Turner/ Ribault Scenic Neighborhood Action Plan provides a blueprint for development opportunities for this community. The plan identifies opportunities such as residential development on vacant property, a neighborhood commercial district in the interior of the neighborhood and a regional commercial shopping center along

## **Community Redevelopment Act SMRRA Community Redevelopment Plan**

Edgewood Avenue. While the community enjoys a stable neighborhood, there are areas of lower income housing that the plan states is in need of a rehabilitation program.

The Lem Turner/ Ribault Citizens Planning Team identified the following concerns regarding their neighborhood through a community workshop and survey questionnaire which took place at Ribault Senior High School. The recorded concerns include: housing needs; lack of social services; economic development programs; need for parks and recreation amenities and infrastructure.

### Sherwood Forest/ Lincoln Villas Neighborhood Action Plan

The Sherwood Forest neighborhood is defined by Trout River to the north, Soutel Drive to the south, Spottswood Road to the east and Sibbald Road to the west. The purpose of this plan is to promote growth and development in the Sherwood Forest/ Lincoln Villas areas. The plan identified key planning issues in both the neighborhoods and developed recommendations to address these issues with significant input from the residents. The plan provided the framework for local planning officials and other concerned parties in order to guide them in allocating resources for the plans' implementation.

It should be noted that the plan highlights the need for retail development, recognizing the corridor improvement of Route 1 (New Kings Road), as well as, redevelopment of existing retail centers. The challenge is that there is not the proper investment climate either actual or perceived to stimulate private sector development. Recommendations for redevelopment of this area include: future land uses, housing, commercial area revitalization, infrastructure improvements, code violations and crime.

### New Kings Road Corridor Study

The study area is generally located along and around the New Kings Road and Soutel/Pritchard Road Corridor. Final determination for parcels to be included in the proposed boundary was based upon the following criteria: statutory criteria pertaining to site conditions; consideration of future corridor development or redevelopment potential that would either assist in elimination or reduction of blight; consideration of sound planning principles for continuity of future land use based upon land use pattern and potentials, the transportation system and efficient provision of government services and facilities; consideration of irregular, small or surrounding parcels that might at some time be assembled for a larger, more efficient use of land; locations providing a logical terminus for the boundaries such as roads and property lines reflecting potential accessibility and marketability for quality redevelopment or development.

### Social Compact/ Jacksonville Neighborhood Market Drilldown Profile

The City of Jacksonville, supported by a business leadership team and in partnership with a team of industry leaders, contracted with Social Compact to conduct the Jacksonville Neighborhood Market Drilldown Profile. The intent was to provide the City of Jacksonville, the local business community and the related Jacksonville neighborhoods with a unique set of dependable business-oriented data and market insights that cannot be assessed through traditional market sources.

The study's objective is to help fuel the flow of private capital and support informed business decision-making processes relative to future investment strategies for Jacksonville's inner-city and undervalued neighborhoods.

The study reports the market opportunities. The Retail Market Opportunity: Adjusted aggregate market income is \$2.387 billion. Market retail purchasing power (based on the CES model of disposable income available after housing, housing costs, taxes and utilities) totals \$1.669 billion. Aggregate retail sales in the market amount to \$1.534 billion dollars.

There is a \$135 million gap between retail purchasing power and retail sales, or, \$135 million in retail purchasing potential is being spent outside the market, due in part to the absence of quality retailers serving the market. This represents an average of \$1,900 per household.

#### Buxton Retail Site Determination

In the Retail Site Determination, Buxton examined the potential for Jacksonville, FL to attract new retailers and restaurants. Buxton compared three (3) retail locations in Jacksonville and determined the relative viability of each to recruit the interest of retailers and restaurants. The Final Report is intended to identify special retailers and restaurants that are mostly likely to consider Jacksonville as a potential location.

**Community Redevelopment Act  
SMRRA Community Redevelopment Plan**

**III. LAND USES**

Existing Land Use

The current land uses are consistent with existing zoning. The current zoning is also compatible with the future land use pattern proposed for the corridor in the 2010 Comprehensive Plan.

Table 3-1 and 3-2 provide a detailed breakdown of the land uses and zoning with the SMRRA.

Table 3-1 Existing Land Use in SMRRA

Land Use	Acres
CGC	56.07380356
LDR	1.34253114
MDR	39.1206532
NC	4.21028189
PBF	10.06939105
ROS	99.09643989
	210.304827

Table 3-2 Existing Zoning Use in SMRRA

Zoning	Acres
CCG-1	27.89612201
CCG-2	20.78393678
CN	10.89384207
CO	1.366528147
PBF-1	99.40657072
RLD-G	1.077909955
RMD-A	25.04111369
RMD-D	14.10617319
ROS	9.732626616
	210.3048232

**Future Land Use**

As part of the Future Land Use Element of Jacksonville’s Comprehensive Plan, the City assigned to every parcel within its boundaries a future land use category which corresponds with 21 land use categories described in the Future Land Use Element. This land use category system provides for the location, type, density and intensity of development and redevelopment and

prescribes areas of the city acceptable for commercial, residential, office, industrial, open space and other uses.

Five (5) land use categories govern property within the Soutel / Moncrief / Retail Redevelopment Area: Commercial General (CGC), Neighborhood Commercial (NC), Medium Residential Density (MDR) Recreation and Open Space (ROS) and Public Building Facility (PBF). For a full description of each land use category, see **Appendix B.**

Zoning

Future land use categories are broad descriptions of generally appropriate land uses within a given area. The details and implementation of land development is left to one or several zoning districts which are required by Florida law to be consistent with the underlying land use category. While these several zoning districts may implement the same land use category, each call for different development intensities or densities. For instance, the Community/General Commercial future land use category is implemented through five different zoning districts tailored to unique commercial conditions within the city. But each district focuses on different types of commercial development, such as personal/professional development, sales/service activities, retail trade, hotels, motels, entertainment, light manufacturing, light industrial, or small scale retail development, and has different standards for minimum lot size and setbacks, pervious surface coverage, building height and other standards to be met by proposed commercial development.

The zoning districts allowed in the land use categories are attached in detail in **Appendix C.**

The CCG-1 and CCG-2 are zoning districts, which are intended to provide for areas

**Community Redevelopment Act  
SMRRA Community Redevelopment Plan**

where a variety of retail and commercial service activities can be conducted which are compatible with surrounding uses and residential districts. The stakeholders in the study area have asked the City of Jacksonville for help in restoring commercial/retail amenities to the neighborhood.

#### **IV. REDEVELOPMENT ISSUES IN SMRRA**

The approved Blight Study provided documentation that the proposed SMRRA is, in fact, subject to the blight conditions outlined in the definition of "Blighted Area" contained in the Community Redevelopment Act. The Blight Study established a need and this Community Redevelopment Plan will identify the solutions and recommendations to remedy the current conditions.

The finding of blight is supported by data and analysis and as such staff inventoried socioeconomic, land use and property value data and identified four (4) conditions of blight in SMRRA. The elements of blight include:

- Governmentally owned property with adverse environmental conditions caused by a public or private entity
- Assessed real property values of real estate in the area have failed to show any appreciable increase
- Residential and commercial vacancy rates higher than in the remainder of the City
- Incidence of crime in the area higher than in the remainder of the City

##### Governmentally Owned Property

Large parcels of environmentally sensitive property targeted for commercial redevelopment within the proposed CRA is owned by the City of Jacksonville. The Lonnie C. Miller Park is currently managed by the City of Jacksonville's Parks & Recreation Department and encompasses approximately 100 acres. The area was previously used by the City of Jacksonville for disposal of incinerator ash from the 5th & Cleveland Incinerator site. Public facilities at the site presently include a playground, public restrooms, picnic shelters and a small fishpond. Presently, the park is

underutilized because of perceptions of potential health hazards.

Remediation and development under the Brownfields Redevelopment Program would facilitate a favorable re-use of this environmentally sensitive area to stimulate economic development in the vicinity, create needed jobs and provide commercial stability in this low to moderate income area.

##### Structure and Site Conditions

An indicator of blight within the SMRRA is the structural condition of buildings and site improvements. A substantial number of deteriorated buildings or substandard sites in an area indicate a relative lack of private investment in the development, redevelopment and maintenance of building assets. The presence of deteriorated buildings and site improvements also impairs future growth by contributing to a poor environment for investment.

In January 2006, the City of Jacksonville Planning and Development Department developed a Neighborhood Action Plan and conducted a survey of the SMRRA corridor. Their findings determined that the commercial corridor (1 percent of total parcels) consists of professional offices and retail uses. Most of these commercial uses are currently vacant, causing commercial area deterioration. The analysis of current field data of the neighborhoods indicated that there is high vacancy and under-utilization of existing shopping center space with some plazas nearly 100 percent vacant. This presents an obstacle for attracting new tenants and retail establishments to the corridor.

A pictorial of building and site conditions is attached in **Appendix D** and all parcel listings are included in **Appendix E**.

## Community Redevelopment Act SMRRA Community Redevelopment Plan

Much of the retail space within the competitive market area is located along the Interstate 95 and Lem Turner corridors and portions of Edgewood Avenue. The study of this market area identified two (2) retail comparables in the northwest quadrant. They are as follows:

- Gateway Shopping Center. A large shopping center with multiple anchor tenants including Publix groceries. 50,000 square feet of space has been identified for office use.
- K-Mart Plaza. A shopping plaza anchored by K-Mart and Aaron Rents.

The substantial number of deteriorated or dilapidated buildings in the SMRRA substantiates blighted structural conditions. It should be noted that several individual properties demonstrate aspects of maintenance and are free from blighting conditions. However, it is the overall appearance of the SMRRA that has impaired new development, not just the appearance of any one building.

### Vacancy Rates

An inventory analysis of all retail and service development in the comparable area was part of the scope of analysis. The challenge for attracting new retail businesses to the area is finding the right mix of retailers that would be supported by the local consumer while also generating customer sales from the non-local consumer. Additionally, the challenge for this neighborhood has been their inability to attract a “pioneer” retail development. The Redevelopment Plan addresses this topic in detail.

The following table 4-1 is a summary of the vacancy rates of comparable retail outlets in the northwest quadrant.

Table 4-1 Vacancy Rates SMRRA City of Jacksonville

Retail Detail	Retail Space Available	Vacancy	%
SMRRA	137,716 sq ft	59,217 sq ft	43%
Gateway Shopping Ctr.	518,265 sq ft	51,826 sq ft	10%
K-Mart Plaza	105,930 sq ft	0	0

### Crime Statistics

Crime in the neighborhoods is analyzed based on Crime Analysis Zones (CAZ.) Crime Data by CAZs indicates that zones 231 and 277 have had increases of 3.4 percent, and 6.4 percent respectively between 2001 and 2004 while other zones have experienced a decline (see table 4-2.)

Overall crime in the Study Area has decreased by 11.6% from 2001 to 2004 (see table 4-3.) Homicide (33.3 percent increase) and robbery (22.7 percent increase) have increased from 2001 to 2004. All other crimes also showed an increase of 29.7 percent.

The “calls for serve” in the study area shows that high call volumes have been experienced. It can be observed that the centers of these neighborhoods require high calls of service compared to the periphery. The Jacksonville Sheriff’s Office is aware of these issues and is working diligently to improve them.

Table 4-2 – Crime Analysis Zones

CAZ	2001	2002	2003	2004	% Change 2001-2004
225	31	34	28	31	0.0%
239	79	65	53	64	-19.0%
230	241	238	167	195	-19.1%
231	116	120	87	120	3.4%
277	110	131	97	117	6.4%
278	183	189	157	136	-25.7%
279	193	204	178	179	-7.3%
<b>Total</b>	<b>953</b>	<b>981</b>	<b>767</b>	<b>842</b>	<b>-11.6%</b>

Source: Jacksonville Sheriff’s Office, 2004



## Community Redevelopment Act SMRRA Community Redevelopment Plan

Table 4-3 Overall Crime Study

	2001	2002	2003	2004	% Change 2001- 2004
All Offenses	953	981	767	842	-11.6%
Homicide	3	3	5	4	33.3%
Forced Rape	5	4	1	2	-60.0%
Other Sex	2	2	3	2	0.0%
Robbery	22	23	17	27	22.7%
Aggravated Assault	67	60	49	54	-19.4%
Residential Burglary	95	119	121	87	-8.4%
Other Burglary	21	30	18	18	-14.3%
Thefts Less \$3000	169	168	107	108	36.1%
Thefts Greater \$3000	73	90	88	69	-5.5%
Motor Vehicle Theft	81	103	48	65	-19.8%
Arson	4	7	6	4	-0.0%
Kidnap	4	1	6	2	-50.0%
Simple Assault	149	124	112	128	-14.1%
Crime Misc.	129	112	93	106	-17.8%

Source: Jacksonville Sheriff's Office, 2004

### Recommendation

The area known as the Soutel / Moncrief / Retail Redevelopment Area (SMRRA) is intended to accommodate modern neighborhood and commercial uses. However, blighted conditions have impaired the ability of the City and the private sector to make this a reality.

Despite its location on a major corridor, central location within the northwest quadrant and access to a large underserved consumer market, redevelopment of the SMRRA has not occurred. In fact, very little investment has occurred in the large portions of the City's northwest areas during the

economic boom in the United States over the latter half of the 1990s.

The initial public role in creating a redevelopment area focuses on removing slum and blight conditions to foster opportunities for future job creation and commercial development, as well as having positive impacts on existing businesses and the surrounding residential areas throughout the Soutel / Moncrief / Retail Redevelopment Area.

### Declining Tax Base

The loss of potential tax revenues in blighted areas has a broad effect on a community. Property taxes are the largest single source of revenue available to a city for funding general government services. As inflation increases the cost of providing services and the demand for these services expands, the taxable value of property must increase. If it does not, the city must either cut services or raise the tax rate. The current total annual tax revenues in the SMRRA are approximately \$157,273. The county collects approximately \$81,703 annually and the other taxing authorities collect the remaining \$75,570. Using conservative increases of two (2), five (5) and seven (7) percent, the tables in **Appendix F** demonstrate the steady increases in taxes for existing parcels and a newly created retail center on a portion of Lonnie Miller Park. Successful implementation of the CRP will not only increase the tax base, but will generate other tax revenues related to increased business activity such as increased sales taxes, business license fees, utility fees and other charges.

## **V. REDEVELOPMENT PLAN OBJECTIVES**

The Community Redevelopment Plan is intended to promote growth and development along the Soutel Drive, Moncrief Road corridors. To understand the need for commercial revitalization in the area it is important to evaluate the patterns of retail activity throughout the community.

The adjacent neighborhoods were developed according to modern planning principles of the 1950s. The curved streets were designed to present a less “urban” and more tranquil environment than traditional grid systems. The neighborhoods were designed to be somewhat self-sufficient with its own elementary school and retail center. The Shops of Sherwood, which was originally developed in 1957, is still in existence today.

Housing and local school segregation occurred in Jacksonville between the 1960s and 1970s. The desire of African-American residents to live in newer houses in modern neighborhoods, and the subsequent “white flight” led to the racial shift in the neighborhoods. By the end of the 1970s, the area had become a working class African-American neighborhood.

By the 1980s, the area had an abundance of vacant parcels and unoccupied buildings. However, the housing conditions remained well maintained with only five (5) percent deteriorating.

By the 1990s, the neighborhood faced serious issues with crime, including drug sales and use and vehicular and residential burglary. The community stakeholders are working in earnest with the Jacksonville Sheriff’s Office to take back the neighborhood.

In 1995, the Lonnie Miller Park, a regional park, was opened in the neighborhood. The

park is located adjacent to the intersection of Soutel Drive and Moncrief Road. It totals 126 acres. A portion of the park was closed to the public in December 2004 due to soil contamination. To date, the active park consists of picnic tables, picnic shelters, grills, playground equipment and walking trails.

The primary goal of the SMRRA is to provide a blueprint for revitalization of the area by preparing the corridor for modern commercial and retail development.

**Objective 1: Designate all parcels within the SMRRA boundary as a Brownfield area. A local government shall designate a Brownfield area under the provisions of the 2001 Florida Statutes 376.80 as stated in Appendix G.**

1. The state of Florida and the City of Jacksonville entered into a cooperative agreement to encourage the redevelopment of the City’s Brownfield properties.
2. Identify parcels/sites that may have an actual or perceived environmental concern that may prohibit redevelopment or reuse.
3. The program will provide proven redevelopment strategies to:
  - site remediation or clean-up (if needed);
  - mitigation of any environmental hazard (if present);
  - obtain regulatory sign-off of the site (if needed);
  - identify funding to assist in the redevelopment of the site;
  - promote redevelopment/reuse as an asset to the community;
  - encourage economic development opportunities and job creation.

**Objective 2: Create a new major retail center with a nationally-recognized**

## **Community Redevelopment Act SMRRA Community Redevelopment Plan**

### **commercial retail establishment on existing city owned property located on a portion of Lonnie Miller Park.**

1. Provide an adequate land area to accommodate a new commercial center and out parcels for other modern commercial uses.
2. The new development should recognize the character and context of the surrounding neighborhoods. Utilize appropriately scaled architecture, site layout, and amenities to create a strong and vibrant activity center serving the needs of the surrounding neighborhoods.
3. Reproduce all existing park amenities on adjacent parcel including a newly developed Community Center/Shelter and a gym facility, **Appendix H**, and the planned Bob Hayes Sports Complex, **Appendix I**.
4. The new development should have a balanced approach to pedestrian, vehicular and public transportation.
5. The new development should provide appropriately scaled sidewalks, and strategically locate public and semi public spaces to allow social interaction.
6. Roadway designs and crosswalks should be utilized to create pedestrian connections between all phases of the redevelopment planning area.

### **Objective 3: Provide private developers with flexible site requirements responsive to the future needs of commercial uses that promote quality building and site design.**

1. All phases of the development plan should have a flexible design adhering to the current and future needs of retail, office and other uses associated with this type of retail center.

2. Process a Semi-Annual Land Use Amendment to the Future Land Use Map Series to facilitate private development proposals which are appropriate to and compatible with the surrounding neighborhood.
3. To ensure the long-term sustainability and aesthetic appearance of all projects, building materials and finishes should be durable and maintainable with a minimum amount of effort.
4. Architectural detailing should be carried around all sides of retail buildings.
5. Buildings should address the intersection of Soutel Drive and Moncrief Road creating appropriate connections for all modes of transportation.

### **Objective 4: Buffer commercial development projects from surrounding residential uses to foster neighborhood revitalization.**

1. Retail buildings should utilize design, fenestration patterns, landscaping, fencing and other means to provide adequate buffers and protections from surrounding residential properties.
2. Residential uses should be adequately protected from light and sound intrusion.
3. Dumpsters, mechanical equipment and delivery areas should be located appropriately and buffered from surrounding residential uses.
4. Parking, landscaping and retention areas should be utilized to buffer commercial and residential uses.
5. The fronts of commercial buildings should address the connections to the street with sidewalks, landscaping and

## Community Redevelopment Act SMRRA Community Redevelopment Plan

pedestrian amenities. Good site planning which provides access control.

### **Objective 5: Ensure multi-modal transportation opportunities in the Commercial Node by integrating pedestrian and mass transit amenities into the commercial redevelopment site design.**

1. Provide well designed entry and exit system utilizing Soutel Drive and Moncrief Road to access all off street parking areas.
2. Vehicular access to residential streets should be limited to avoid cut-through traffic within the neighborhood.
3. In addition to vehicular flow, address pedestrian connections. This includes all parcels within the boundary with emphasis on the new commercial center on Soutel Drive and Moncrief Road to ensure proper connections with the surrounding residential neighborhoods.
4. All phases of the redevelopment plan should provide bicycle racks consistent with required municipal standards.
5. Public transportation is a critical component of this plan. The CRP recognizes the needs of public transportation and site layout should consider features to allow easy public transportation access which is compatible with vehicular and pedestrian traffic.

### **Objective 6: Implement proposed improvements to include approximately 0.7 miles of the Moncrief Road project beginning at the Soutel Drive intersection and ending at U.S.-1.**

1. Proposed improvements include construction of a three-lane section with center turn lane, curb and gutter,

sidewalks, bike lanes and an underground storm drainage system.

2. A stormwater management pond will be constructed as well as streetlighting and streetscaping.
3. A left turn lane will be added on east bound Soutel Drive onto northbound Moncrief Road with minor modifications to traffic signals at Soutel Drive and Moncrief Road.
4. Utility improvements should include the relocation of several overhead electric poles and the extension of a 16-inch water main.

Illustrations can be found in **Appendix J**.

### **Objective 7: Reassemble and replat property for nonconforming, inaccessible and vacant lots.**

To the extent required, and as a last resort, the City of Jacksonville has the power of eminent domain, and through condemnation proceedings may acquire necessary land for public purpose. The City of Jacksonville shall follow all state and local law regarding the sale and disposition and property should land transactions be required.

1. As required to facilitate the plan objectives, the City may assemble properties. The CRA can negotiate with individual property owners to reach a cash settlement of his or her property without using eminent domain.
2. The assembly of land is accomplished in a standard and orderly manner.
3. Use an existing non-profit or create a new non-profit developer to be used in the acquisition and rehabilitation and resell of vacant and abandoned buildings along the SMRRA.

## **Community Redevelopment Act SMRRA Community Redevelopment Plan**

### **Objective 8: Ensure the success of the SMRRA program by improving the neighborhood's public safety, infrastructure and streetscaping.**

1. Neighborhood associations should coordinate with the Jacksonville Sheriff's Office (JSO) to increase police patrol and surveillance, establish environmental design techniques such as street lighting, cleaning vacant lots and graffiti on walls and organize community newsletters and festivals to create a sense of belonging and awareness among residents.
2. Increase collaboration among neighborhood associations and residents at a specific geographic area as a permanent office and employ qualified staff to carry on development plans and oversee the stability of the neighborhood.
3. Provide adequate streetlights and sidewalks throughout the SMRRA. Work with the City of Jacksonville Pedestrian Safety Improvements Program (PSI) to develop a Pedestrian Safety Improvements Program as well as a Street Lighting Program.
4. Existing and newly proposed commercial developments should be subject to a mandatory design review to maintain the design character of the neighborhood.
  - Signage, lighting and streetscaping should include welcome signs at entrance points to the neighborhoods.
  - New and bright streetlights should be placed at pedestrian and vehicular intersections to ensure safety and reduce the incidence of crime.
  - Bricked paved pedestrian crossings should be provided at major

junctions on Soutel Drive and Moncrief Road.

- Sidewalks shall be provided on Soutel Drive and Moncrief Road to assist residents to conveniently access the commercial area.

For review of recommended streetscapes and infrastructure improvements see **Appendix K.**

### **Objective 9: Create a utility infrastructure to support the level of development proposed for the SMRRA.**

1. In conjunction with JEA and the City Public Works Department, develop basic guidelines for the repair and replacement of different utility systems.
2. Propose phasing and location for project installation sanitary system, sanitary sewer pump station, and force main.
3. Develop a stormwater management plan to include a review of existing drainage and needed improvements, flow attenuation and water quality treatment of stormwater.

## **VI. PLAN STRATEGIES, IMPLEMENTATION AND ACTIONS**

The first phase of the project implementation process consists of the issuance of a Request for Proposals (RFP) for a 28-acre site located on the City of Jacksonville Lonnie Miller Park property. The RFP will address the imminent and specific need to develop a neighborhood commercial center with focus on the attraction of a national retailer. Other phases, including the remaining areas of the SMRRA, will address future opportunities for augmenting the commercial center with additional commercial/retail development along the Soutel/Moncrief corridor.

### **Proposed Redevelopment Actions**

Redevelopment actions in SMRRA will focus on four (4) areas: modification of future land use and rezoning classifications; site consolidation, preparation and disposition; and public infrastructure improvements.

#### Future Land Use and Zoning

A future land use plan amendment and rezoning application was submitted to the Jacksonville Planning and Development Department (PDD) on March 29, 2006. In accordance with **Section 650.407(c) of the Jacksonville Ordinance Code** ten (10) yellow land use signs were posted on the property advertising and inviting the public to attend the Workshop providing an overview of the application on April 10, 2006. There were no objections to the land use or rezoning changes at the workshop.

The City of Jacksonville does not anticipate any additional changes to the current land uses or zoning. However, in the event a change is petitioned, the CRA Advisory Committee will follow process as required by municipal code and state statute.

#### Site Consolidation, Preparation and Disposition

Consistent with the authority provided in the Community Redevelopment Act, the City intends to prepare the real property through acquisition, clearance and demolition and other improvements that may be necessary to induce a quality national retailer and other commercial/retail developers to the SMRRA. All property acquired by the City shall be sold or leased for development for fair market value in accordance with the uses permitted in the CRP and as required by the disposition procedures of the Community Redevelopment Act. The City may reserve such powers and controls through disposition and development documents with purchasers or lessees of real property as may be necessary to ensure that development begins within a reasonable period of time and that such development is carried out pursuant to the purposes of the CRP.

The City may acquire real property as needed for site consolidation for desired development of commercial uses along the Soutel/Moncrief corridor. **At no time will site consolidation for commercial or retail oriented development intrude outside of the boundary or include any residential component.**

#### Public Infrastructure Improvements

Infrastructure improvements are in two (2) categories in the SMRRA. Existing improvements are those associated with remedial maintenance, scheduled capital improvements or improvements designed to implement existing Neighborhood Action Plans. Proposed improvements are those associated with SMRRA or involve infrastructure upgrades to be provided in conjunction with private development proposals as part of the implementation of the CRP.

## **Community Redevelopment Act SMRRA Community Redevelopment Plan**

As part of the SMRRA, the City will continue to maintain its facilities in the area and implement the capital improvements proposed in the existing neighborhood plans, including sidewalk repair, traffic calming measures, installation of landscaping, street lighting and identity signs.

Proposed infrastructure improvements expected as part of the SMRRA redevelopment program include streetscaping, lighting upgrades, surveillance enhancements and transportation upgrades. The City shall determine the appropriate time and need to install and construct or to cause to be installed or constructed the public improvements and infrastructure within or outside the redevelopment area as are necessary to carry out the CRP. The City will retain the responsibility of insuring that public infrastructure systems are adequately installed and maintained. In addition, new commercial/retail construction in SMRRA will be required to meet the City's drainage requirements, which will help improve drainage conditions in the area. Through development agreements, elements of this responsibility may be assigned to private entities; however, adequate safeguards will be instituted to insure that the necessary and appropriate levels of public services are provided.

A streetscape program will be established to support the Commercial Node redevelopment goal. The program should include sidewalk/curb installation and repair, landscaping, street furniture and lighting. The streetscaping will benefit the aesthetics and the function of the redevelopment area. Some of the streetscaping investment should focus on areas adjacent to but not within the SMRRA boundary to establish a consistent design scheme throughout the Soutel/Moncrief corridor.

A review of the traffic circulation element will be conducted at the time the development projects are identified, to ensure proper flow of traffic. The review would provide an opportunity to explore options addressing pedestrian connections at the intersection of Soutel Drive and Moncrief Road as well as opportunities relating to mass transit. The timing of any proposed improvements should not precede private sector development to ensure optimal coordination and cost sharing between the public and private sectors.

### **Sources for Redevelopment Funding**

The City of Jacksonville provided the initial funding for the preparation of the Finding of Necessity and the Community Redevelopment Plan. In order to carry out redevelopment, the City may use multiple funding sources, including private sector as well as local, state and federal government sources. General funding methods and sources that will be examined to finance redevelopment activities to implement the plan include the following:

#### Grant Funds and Loans

Various federal, state and private sources will be considered to implement the CRP by both the public and private sector. Locally funded sources are available through the Northwest Jacksonville Economic Development Fund. They include:

- Business Infrastructure Grant/Loan (BIG)
- Large Scale Economic Development Fund
- Small Business Development Initiative
- Commercial Development Area
- Façade Renovation Grant

#### Brownfield Redevelopment Program

As a designated Brownfield area the following tools are available to all property owners within the SMRRA boundary.

## **Community Redevelopment Act SMRRA Community Redevelopment Plan**

- Environmental Site Assessment Reimbursement Funding
- Brownfield Cleanup Revolving Loan Fund
- Brownfield Redevelopment Bonus Refund
- Voluntary Cleanup Tax Credit
- Brownfield Property Ownership Clearance Revolving Loan Trust Fund (OTTED)
- Qualified Targeted Industry Tax Refund (OTTED/COJ)
- Qualified Defense Contractor Tax Refund (OTTED/COJ)
- Environmental Cleanup Cost Deduction (EPA/TRS)
- Community Development Block Grants (HUD/COJ)
- Brownfields Tax Incentive (EPA)

### Tax Increment Financing

Tax increment financing is a power delegated by statute to the Community Redevelopment Committee.

### Local Funds

If available, general fund revenues can be used to finance redevelopment activities. General fund revenues are the source of staffing assistance to the redevelopment program. Enterprises of local government may be used to fund system improvements in the redevelopment area.

### **Timing of Redevelopment**

As per Chapter 163.362(5) and (6), F.S., all redevelopment plans must contain adequate safeguards that the work of redevelopment will be carried out pursuant to the plan and provide for controls and restrictions or covenants to ensure development in accordance with the plan.

All new construction shall comply with all applicable local laws and ordinances and shall be consistent with the CRP. Proposals for new development shall be reviewed by the Advisory Committee and the

Jacksonville Economic Development Commission (JEDC) sitting as the regional Community Redevelopment Agency.

No limitations are anticipated on building use as long as development conforms to approved plans and ordinances. Every effort will be made to provide existing property owners, business owners and business tenants within the redevelopment area preference for business growth within the redevelopment area provided the business is compatible with the uses, theme and quality of development in the redevelopment area and that the business owner or tenant has the financial ability to operate the business consistent with the overall integrity of the redevelopment project.

Should any real property be owned, leased or otherwise come under the control of the City, the City will conduct supervision and management. The City shall enter into contracts, leases or management agreements as necessary to insure the preservation and maintenance of any such real property, and shall insure the greatest return feasible to the project and the community.



## **VII. NEIGHBORHOOD IMPACT ELEMENT**

### **Traffic Circulation**

The CRP will have a positive impact on the current traffic circulation in the SMRRA. Proper utilization of the site, redesign of the Soutel/Moncrief intersection and lane improvements is important to allow the proper flow of traffic. This will also provide an opportunity to explore options addressing pedestrian and bicycle connections at the intersection, opportunities to explore options addressing pedestrian and bicycle connections as well as opportunities relating to mass transit. The solutions to these problems will be addressed with a balanced approach allowing all forms of transit, from pedestrians and bicyclists to the automobile to access shopping areas in a safe and convenient manner. Site layout should recognize the needs of public transportation and provide needed site features to allow easy public transportation access which is compatible with and complements vehicular of pedestrian traffic.

### **Environmental Quality**

Environmental quality will improve as a result of redevelopment. In order to redevelop a site, an environmental assessment is performed. If environmental regulations require remediation, the Brownfield designation will provide the tools necessary through resources and incentives to complete the project. Streetscape improvements will also enhance the health and appearance of the area. Finally, the SMRRA redevelopment program aims to eliminate the deteriorated building conditions which are a concern for environmental quality.

### **Availability of Community Facilities and Services**

The present state of the commercial corridor is not an asset to SMRRA or the surrounding neighborhoods. The buildings do not contribute aesthetically and many of the commercial uses are marginal. The impact of redevelopment of SMRRA is expected to be positive as additional retail services, improved commercial structures, upgraded landscaping and new jobs generated by the private sector will result from redevelopment of this area. Retail tenants will be solicited to the target market for private sector redevelopment of retail activities. Local business owners will have the opportunity for new commercial space if so desired. Area residents who presently drive or must take mass transit to access retail, amenities and services will have comparable alternatives within a short walk or car ride from their home.

### **Effect on School Population**

Implementation of the SMRRA CRP will have little or no impact on the City's population or the population density pattern and therefore, not impact on the public school system.

### **Consistency with Neighborhood Plans**

SMRRA's Commercial Node program is consistent with the NAPs for the surrounding areas. The objectives outlined in the CRP echo the objectives of the two (2) NAPs referenced in this document.

**Community Redevelopment Act  
SMRRA Community Redevelopment Plan**

**VIII. COMPLIANCE WITH  
FLORIDA STATUTES**

redevelopment agency for the City according to Part III of Chapter 163, F.S. is on file if needed.

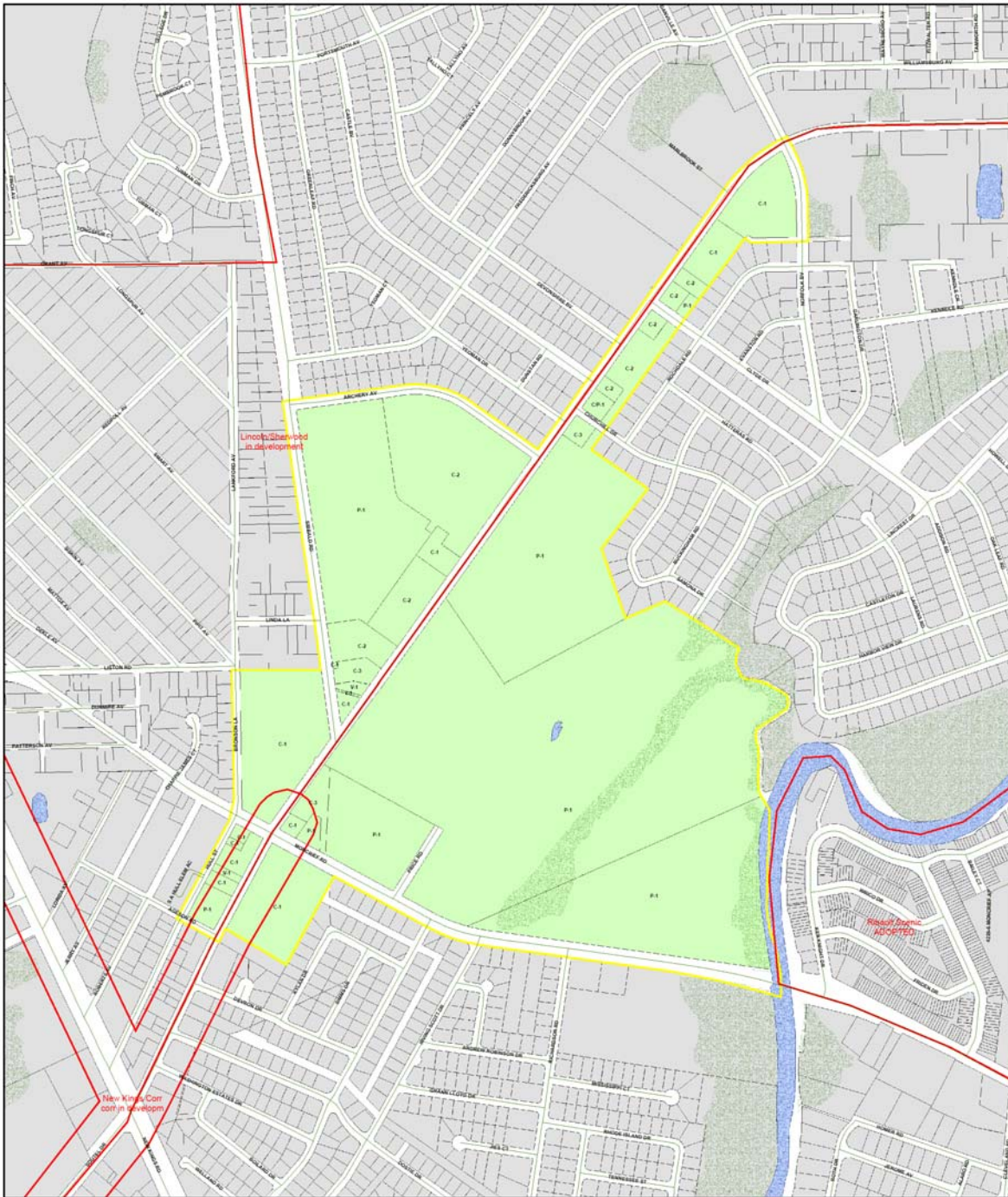
The Florida Community Redevelopment Act (Chapter 163, Article III, F.S.) specifies requirements for communities embarking on a redevelopment program and involve such activities as declaring a blighted area, delegation of redevelopment authority, establishing a community redevelopment agency, preparing a redevelopment plan and implementation strategies, identifying relocation strategies for business is affected by the redevelopment program and conformance of the CRP with locality's comprehensive plan.

If the Jacksonville City Council approves the Soutel Moncrief Retail Redevelopment Area Plan, it first must make the following findings per Chapter 163.360, F.S.

- The SMRRA CRP conforms to the general plan of the municipality as a whole;
- The SMRRA CRP considers utilizing community policing innovations, and providing adequate park and recreational areas and facilities that may be desirable for neighborhood improvement, with special consideration for the health, safety, and welfare of children residing in the general vicinity of the site covered by the plans, and;
- The SMRRA CRP will afford maximum opportunity, consistent with the sound needs of the county or municipality as a whole, for the rehabilitation or redevelopment of the community redevelopment area by private enterprise.

For each statutory requirement, the Soutel Moncrief Retail Redevelopment Area Plan is compliant with Florida's Community Redevelopment Act. Enabling legislation designating the JEDC as the sole community

Community Redevelopment Act  
SMRRA Community Redevelopment Plan



# Soutel Drive and Moncrief Road

Map Scale: 1:2,204  
 Created: January 29th, 2008  
 Developed by: Sabrina Sauer  
 C:\Development\GIS\MapDocs\Current\Workbooks\_Moncrief.mxd  
 This document is intended for illustrative purposes only.

**Legend**

- Streets
- Study Area
- Water
- Wetlands
- Soutel\_Parcel
- All Parcels
- NAP's

**Uses**

- GF - Single Family
- MF - Multi-Family
- MH - Mobile Home
- C - Commercial
- I - Industrial
- V - Vacant
- P - Public/Church

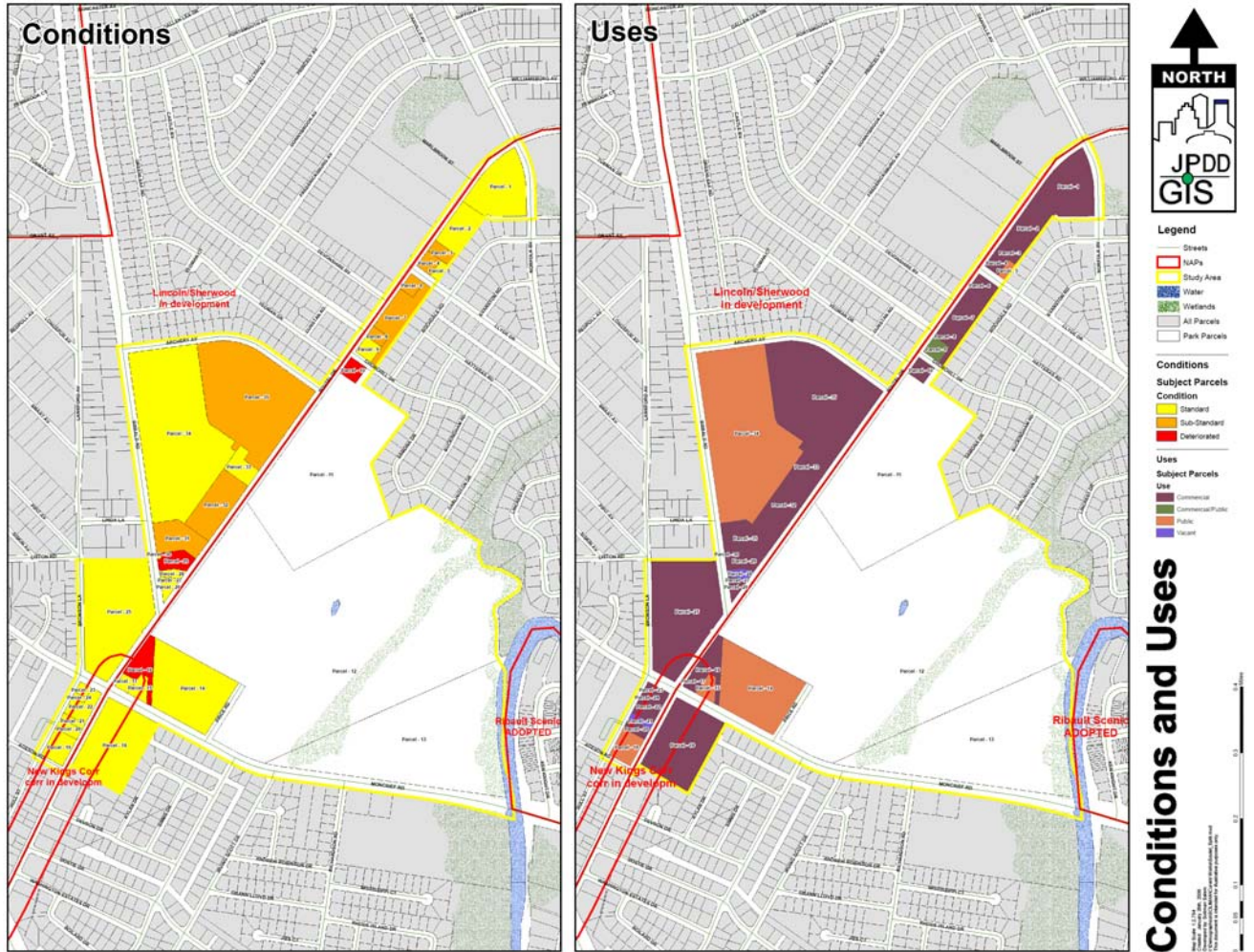
**Conditions**

- 1 - Standard
- 2 - Substandard
- 3 - Deteriorated
- 4 - Dilapidated
- 5 - Abandoned/Vacant

**North Arrow**  
 JPD GIS

Map 3-1  
Project Boundary

**Community Redevelopment Act  
SMRRA Community Redevelopment Plan**



**Map 3-2  
Existing Land Use and Conditions**

# APPENDIX A LEGAL DESCRIPTION

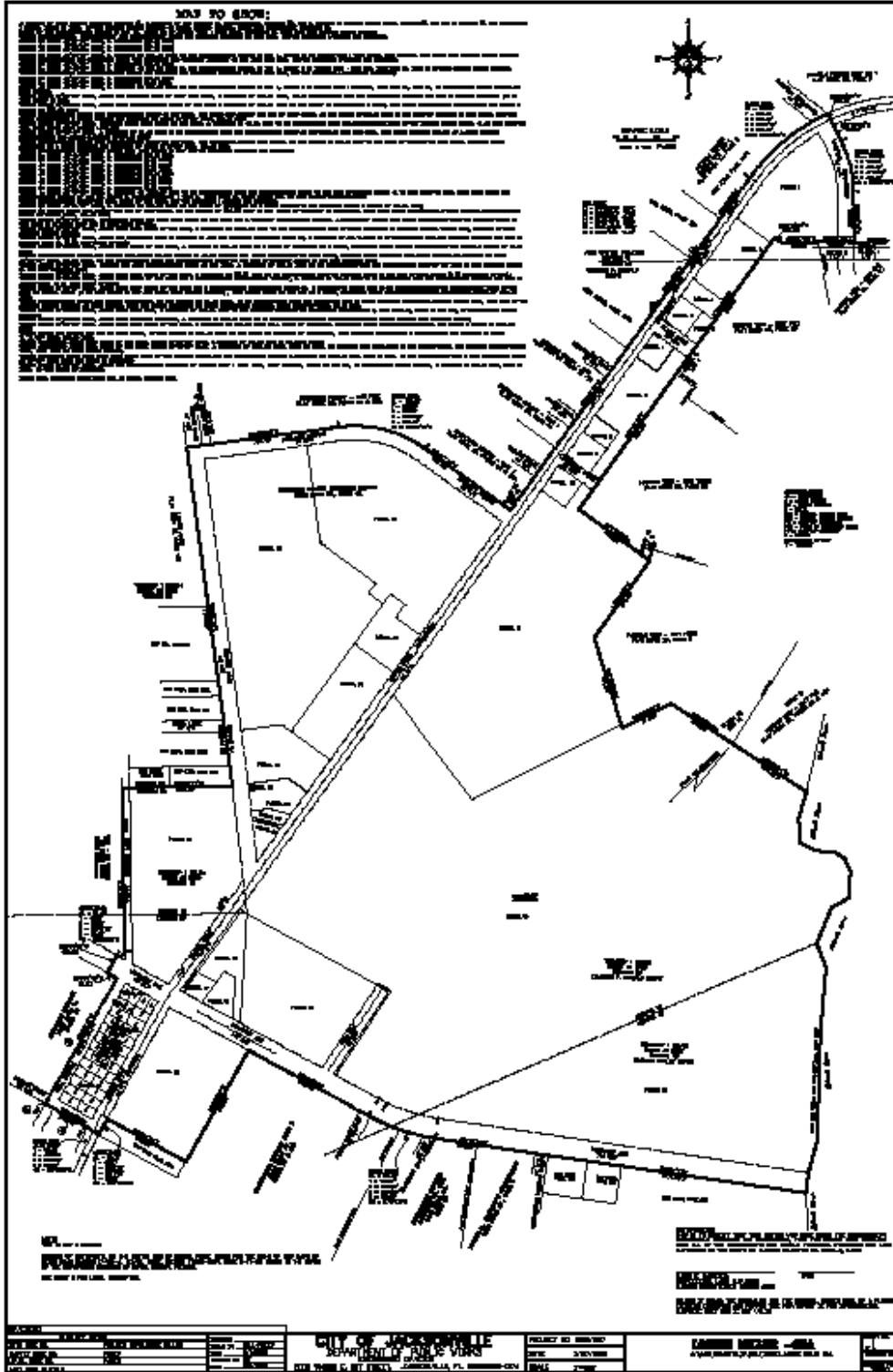
APPENDIX A  
Legal Description

Soutel / Moncrief / Retail Redevelopment Area

Prepared by the Jacksonville Economic Development Commission, January 2006

This legal description is subject to a survey and is for informational purposes only

The Soutel / Moncrief/ Retail Redevelopment Area (SMRRA) is generally described as:



**APPENDIX A**  
**Legal Description**

**Soutel / Moncrief / Retail Redevelopment Area**

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**MAP TO SHOW:**

A PORTION OF JOHN CARTER DONATION SECTION 41, A PORTION OF THE CHARLES F. SIBBALD GRANT, SECTION 39, AND A PORTION OF FRANCIS BAGLEY GRANT, SECTION 43, ALL THREE IN TOWNSHIP 1 SOUTH, RANGE 26 EAST, OF THE CURRENT PUBLIC RECORDS OF DUVAL COUNTY, FLORIDA, BEING MORE PARTICULARLY DESCRIBED AS:

BEGIN AT THE SOUTHEASTERLY CORNER OF LOT 15, BLOCK 15 OF HARBOR VIEW - UNIT FOUR, PLAT BOOK 29, PAGE 8 OF THE CURRENT PUBLIC RECORDS OF SAID COUNTY;

THENCE ALONG THE BOUNDARY LINE OF SAID PLAT THE FOLLOWING 4 COURSES:

COURSE 1: NORTH 56°57'00" WEST, A DISTANCE OF 370.00 FEET;

COURSE 2: SOUTH 63°33'00" WEST, A DISTANCE OF 273.00 FEET;

COURSE 3: NORTH 17°22'30" WEST, A DISTANCE OF 471.84 FEET;

COURSE 4: NORTH 35°48'30" EAST, A DISTANCE OF 476.80 FEET TO THE SOUTHEASTERLY CORNER OF LOT 5, BLOCK 12 OF HARBOR VIEW - UNIT THREE, PLAT BOOK 28, PAGE 92 OF THE CURRENT PUBLIC RECORDS OF SAID COUNTY;

THENCE ALONG THE BOUNDARY LINE OF THE LAST SAID PLAT THE FOLLOWING 2 COURSES:

COURSE 1: NORTH 54°11'30" WEST, A DISTANCE OF 438.65 FEET;

COURSE 2: NORTH 35°45'30" EAST, A DISTANCE OF 838.20 FEET TO THE SOUTHWESTERLY CORNER OF LOT 1, BLOCK 8 OF HARBOR VIEW - UNIT TWO, PLAT BOOK 28, PAGE 80 OF THE CURRENT PUBLIC RECORDS OF SAID COUNTY;

THENCE ALONG THE BOUNDARY LIEN OF THE LAST SAID PLAT THE FOLLOWING 3 COURSE;

COURSE 1: NORTH 35°48'30" EAST, A DISTANCE OF 840.18 FEET;

COURSE 2: SOUTH 54°14'30" EAST, A DISTANCE OF 45.60 FEET;

COURSE 3: NORTH 89°07'50" EAST, A DISTANCE OF 182.28 FEET TO THE NORTHWESTERLY CORNER OF LOT 1, BLOCK 2 OF HARBOR VIEW - UNIT NE, PLAT BOOK 28, PAGE 43, OF THE CURRENT PUBLIC RECORDS OF SAID COUNTY;

THENCE NORTH 89°07'50" EAST, ALONG THE BOUNDARY LINE OF LAST SAID PLAT, A DISTANCE OF 180.00 FEET, TO AN INTERSECTION WITH THE NORTHEASTERLY RIGHT OF WAY LINE OF NORFOLK BOULEVARD (AN 80 FOOT RIGHT OF WAY);

THENCE NORTH 00°52'10" WEST, ALONG SAID RIGHT OF WAY LINE A DISTANCE OF 250.85 FEET TO THE POINT OF CURVATURE OF A CURVE, SAID CURVE BEING CONCAVE SOUTHWESTERLY AND HAVING A RADIUS OF 540.00 FEET;

THENCE NORTHWESTERLY ALONG SAID NORTHEASTERLY RIGHT OF WAY LINE, ALONG AND AROUND SAID CURVE, AN ARC LENGTH OF 262.58 FEET TO THE POINT OF TANGENCY OF SAID CURVE, SAID ARC BEING SUBTENDED BY A CHORD BEARING AND DISTANCE OF NORTH 13°50'30" WEST, 260.00 FEET;

THENCE NORTH 26°48'50" WEST, CONTINUE ALONG SAID RIGHT OF WAY LINE, A DISTANCE OF 87.54 FEET TO AN INTERSECTION WITH THE SOUTHEASTERLY RIGHT OF WAY LINE OF SOUTEL DRIVE, (A 66 FOOT RIGHT OF WAY, ALSO STATE ROAD NUMBER S-115-A);

THENCE NORTH 24°49'47" WEST, A DISTANCE OF 65.95 FEET TO AN INTERSECTION WITH THE NORTHEASTERLY RIGHT OF WAY LINE OF LAST SAID ROAD, SAID POINT BEING IN THE ARC OF A CURVE CONCAVE SOUTHEASTERLY AND HAVING A RADIUS OF 987.93 FEET;

THENCE SOUTHWESTERLY ALONG SAID NORTHEASTERLY RIGHT OF WAY LINE, ALONG AND AROUND SAID CURVE, AND ARC LENGTH OF 509.26 FEET TO THE POINT OF

**APPENDIX A**  
**Legal Description**

**Soutel / Moncrief / Retail Redevelopment Area**

Prepared by the Jacksonville Economic Development Commission, January 2006

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TANGENCY OF SAID CURVE, SAID ARC BEING SUBTENDED BY A CHORD BEARING AND DISTANCE OF SOUTH 50°34'33" WEST, 503.64 FEET;

THENCE CONTINUE ALONG SAID RIGHT OF WAY LINE THE FOLLOWING 9 COURSES;

COURSE 1: SOUTH 35°48'30" WEST, A DISTANCE OF 455.79 FEET;

COURSE 2: NORTH 54°11'30" WEST, A DISTANCE OF 15.00 FEET;

COURSE 3: SOUTH 34°48'30" WEST, A DISTANCE OF 10.00 FEET;

COURSE 4: SOUTH 54°11'30" EAST, A DISTANCE OF 15.00 FEET;

COURSE 5: SOUTH 35°48'30" WEST, A DISTANCE OF 328.91 FEET;

COURSE 6: NORTH 54°11'30" WEST, A DISTANCE OF 15.00 FEET;

COURSE 7: SOUTH 34°48'30" WEST, A DISTANCE OF 15.00 FEET;

COURSE 8: SOUTH 54°11'30" EAST, A DISTANCE OF 15.00 FEET;

COURSE 9: SOUTH 35°48'30" WEST, A DISTANCE OF 1244.55 FEET TO AN INTERSECTION WITH THE NORTHEASTERLY RIGHT OF WAY LINE OF ARCHERY AVENUE (A 70 FOOT RIGHT OF WAY), SAID POINT BEING THE SOUTHEASTERLY CORNER OF LOT 46, BLOCK 2, SHERWOOD FOREST - UNIT 5, PLAT BOOK 28, PAGES 69 AND 69A, OF THE CURRENT PUBLIC RECORDS OF SAID COUNTY;

THENCE ALONG THE LAST SAID RIGHT OF WAY LINE THE FOLLOWING 2 COURSES;

COURSE 1: NORTH 54°11'30" WEST, A DISTANCE OF 527.23 FEET TO THE POINT OF CURVATURE OF A CURVE CONCAVE TO THE SOUTHWEST HAVING A RADIUS OF 601.90 FEET;

COURSE 2: THENCE AROUND THE ARC OF SAID CURVE, AN ARC LENGTH OF 468.54 FEET TO THE POINT OF TANGENCY OF SAID CURVE, SAID CURVE BEING SUBTENDED BY A CHORD BEARING AND DISTANCE OF NORTH 75°56'20" WEST, 445.66 FEET;

THENCE SOUTH 82°18'80" WEST, CONTINUE ALONG THE LAST SAID RIGHT OF WAY LINE AND A PROLONGATION THERE OF, A DISTANCE OF 769.49 FEET TO AN INTERSECTION WITH THE WESTERLY RIGHT OF WAY LINE OF SIBBALD ROAD (AN 80 FOOT RIGHT OF WAY);

THENCE SOUTH 07°41'10" EAST, ALONG LAST SAID RIGHT OF WAY LINE, A DISTANCE OF 1708.69 FEET TO THE SOUTHEASTERLY LINE OF LANDS DESCRIBED IN OFFICIAL RECORDS VOLUME 4784, PAGE 389 OF THE CURRENT PUBLIC RECORDS OF SAID COUNTY;

THENCE SOUTH 88°18'80" WEST, A DISTANCE OF 552.74 FEET TO AN INTERSECTION WITH THE WESTERLY RIGHT OF WAY LINE OF BRONSON LANE (A 37.50 FOOT RIGHT OF WAY);

THENCE SOUTH 00°50'36" EAST, ALONG SAID RIGHT OF WAY LINE, A DISTANCE OF 817.44 FEET TO A POINT OF CURVATURE, SAID CURVE BEING CONCAVE NORTHWESTERLY AND HAVING A RADIUS OF 25.00 FEET;

THENCE SOUTHWESTERLY ALONG AND AROUND SAID CURVE AN ARC LENGTH OF 52.91 FEET TO THE POINT OF TANGENCY, SAID ARC BEING SUBTENDED BY A CHORD BEARING AND DISTANCE OF SOUTH 59°30'17" WEST, 43.45 FEET, TO THE INTERSECTION WITH THE NORTHWESTERLY RIGHT OF WAY LINE OF MONCRIEF AVENUE (A 100 FOOT RIGHT OF WAY);

THENCE SOUTH 29°51'00" WEST, LEAVING THE SAID NORTHWESTERLY RIGHT OF WAY LINE, A DISTANCE OF SOUTH 29°51'00" WEST, A DISTANCE OF 100.00 FEET TO AN INTERSECTION WITH THE SOUTHWESTERLY RIGHT OF WAY LINE OF SAID MONCRIEF AVENUE (A 100 FOOT RIGHT OF WAY);

THENCE SOUTH 60°09'00" EAST, ALONG SAID RIGHT OF WAY LINE, A DISTANCE OF 56.55 FEET TO AND INTERSECTION WITH HULL AVENUE (A 50 FOOT RIGHT OF WAY);



**APPENDIX A**  
**Legal Description**

**Soutel / Moncrief / Retail Redevelopment Area**

Prepared by the Jacksonville Economic Development Commission, January 2006

This legal description is subject to a survey and is for informational purposes only

THENCE SOUTH 29°49'13" WEST, ALONG SAID RIGHT OF WAY LINE AND A PROLONGATION THERE OF, A DISTANCE OF 721.73 FEET TO AN INTERSECTION WITH THE NORTHWESTERLY RIGHT OF WAY LINE OF AGESON ROAD (A 50 FOOT RIGHT OF WAY);  
THENCE SOUTH 80°22'00" EAST, ALONG SAID RIGHT OF WAY LINE AND A PROLONGATION THERE OF, A DISTANCE OF 310.16 FEET TO AN INTERSECTION WITH THE SOUTHWESTERLY RIGHT OF WAY LINE OF SOUTEL DRIVE (A 66 FOOT RIGHT OF WAY AND STATE ROAD NUMBER S-115-A), SAID POINT BEING IN THE ARC OR A CURB, SAID CURVE BEING CONCAVE NORTHWESTERLY AND HAVING A RADIUS OF 3786.72 FEET;  
THENCE NORTHEASTERLY ALONG AND AROUND SAID CURVE, AN ARC LENGTH OF 221.12 FEET, TO AN INTERSECTION WITH THE PROPERTY LINE OF LANDS DESCRIBED IN OFFICIAL RECORDS VOLUME 6528, PAGE 1583 OF THE CURRENT PUBLIC RECORDS OF SAID COUNTY, SAID ARC BEING SUBTENDED BY CHORD BEARING AND DISTANCE, NORTH 28°32'38" EAST, 85.73 FEET;  
THENCE SOUTH 61°34'00" EAST, ALONG LAST SAID PROPERTY LINE, A DISTANCE OF 449.01 FEET TO THE BOUNDARY LINE OF WASHINGTON ESTATES UNIT 3, PLAT BOOK 32, PAGES 28 AND 28A, OF THE CURRENT PUBLIC RECORDS OF SAID COUNTY;  
THENCE NORTH 29°19'00" EAST, ALONG SAID BOUNDARY LINE, A DISTANCE OF 639.91 FEET TO AN INTERSECTION WITH THE SOUTHWESTERLY RIGHT OF WAY LINE OF SAID MONCRIEF AVENUE;  
THENCE SOUTH 61°26'38" EAST, ALONG SAID RIGHT OF WAY LINE, A DISTANCE OF 6871.16 FEET TO A POINT OF CURVATURE, SAID CURVE BEING CONCAVE NORTHEASTERLY, AND HAVING A RADIUS OF 1005.37 FEET;  
THENCE ALONG AND AROUND SAID CURVE AN ARC LENGTH OF

A PORTION OF JOHN CARTER DONATION SECTION 41, A PORTION OF THE CHARLES F. SIBBALD GRANT, SECTION 39, AND A PORTION OF FRANCIS BAGLEY GRANT, SECTION 43, AND ALL OF SECTION 29, ALL IN TOWNSHIP 1 SOUTH, RANGE 26 EAST, OF THE CURRENT PUBLIC RECORDS OF DUVAL COUNTY, FLORIDA, BEING MORE PARTICULARLY DESCRIBED AS:

BEGIN AT THE SOUTHEASTERLY CORNER OF LOT 15, BLOCK 15 OF HARBOR VIEW - UNIT FOUR, PLAT BOOK 29, PAGE 8 OF SAID CURRENT PUBLIC RECORDS;

THENCE NORTHWESTERLY, SOUTHWESTERLY AND NORTHEASTERLY ALONG THE WESTERLY BOUNDARY OF SAID PLAT, RUN THE FOLLOWING 4 COURSES AND DISTANCES:

COURSE 1: NORTH 56°57'00" WEST, A DISTANCE OF 370.00 FEET;

COURSE 2: SOUTH 63°33'00" WEST, A DISTANCE OF 273.00 FEET;

COURSE 3: NORTH 17°22'30" WEST, A DISTANCE OF 471.84 FEET;

COURSE 4: NORTH 35°48'30" EAST, A DISTANCE OF 476.80 FEET TO THE SOUTHEASTERLY CORNER OF LOT 5, BLOCK 12 OF HARBOR VIEW - UNIT THREE, PLAT BOOK 28, PAGE 92 OF SAID CURRENT PUBLIC RECORDS;

THENCE NORTHWESTERLY AND NORTHERLY ALONG THE SOUTHERLY AND WESTERLY BOUNDARY OF THE LAST SAID PLAT, RUN THE FOLLOWING 2 COURSES AND DISTANCES:

COURSE 1: NORTH 54°11'30" WEST, A DISTANCE OF 438.49 FEET;

COURSE 2: NORTH 35°45'30" EAST, A DISTANCE OF 838.20 FEET TO THE SOUTHWESTERLY CORNER OF LOT 1, BLOCK 8 OF HARBOR VIEW - UNIT TWO, PLAT BOOK 28, PAGE 80 OF SAID CURRENT PUBLIC RECORDS;

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Legal Description**

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THENCE NORTHEASTERLY AND SOUTHEASTERLY ALONG THE WESTERLY AND NORTHERLY BOUNDARY OF LAST SAID PLAT, RUN THE FOLLOWING 3 COURSE AND DISTANCES;

COURSE 1: NORTH 35°48'30" EAST, A DISTANCE OF 840.18 FEET;

COURSE 2: SOUTH 54°14'30" EAST, A DISTANCE OF 45.60 FEET;

COURSE 3: NORTH 89°07'50" EAST, A DISTANCE OF 182.28 FEET TO THE NORTHWESTERLY CORNER OF LOT 1, BLOCK 2 OF HARBOR VIEW - UNIT ONE, PLAT BOOK 28, PAGE 43, OF SAID CURRENT PUBLIC RECORDS OF SAID COUNTY;

THENCE NORTH 89°07'50" EAST, ALONG THE BOUNDARY LINE OF LAST SAID PLAT, A DISTANCE OF 180.00 FEET, TO AN INTERSECTION WITH THE NORTHEASTERLY RIGHT OF WAY LINE OF NORFOLK BOULEVARD (AN 80 FOOT RIGHT OF WAY);

THENCE NORTH 00°52'10" WEST, ALONG SAID NORTHEASTERLY RIGHT OF WAY LINE A DISTANCE OF 250.85 FEET TO THE POINT OF CURVATURE OF A CURVE, SAID CURVE BEING CONCAVE SOUTHWESTERLY AND HAVING A RADIUS OF 540.00 FEET;

THENCE NORTHWESTERLY ALONG SAID NORTHEASTERLY RIGHT OF WAY LINE, AND ALONG AND AROUND THE ARC OF SAID CURVE, AN ARC LENGTH OF 262.58 FEET TO THE POINT OF TANGENCY OF SAID CURVE, SAID ARC BEING SUBTENDED BY A CHORD BEARING AND DISTANCE OF NORTH 13°50'30" WEST, 260.00 FEET;

THENCE NORTH 26°48'50" WEST, CONTINUE ALONG SAID RIGHT OF WAY LINE, A DISTANCE OF 87.54 FEET TO AN INTERSECTION WITH THE SOUTHEASTERLY RIGHT OF WAY LINE OF SOUTEL DRIVE, (A 66 FOOT RIGHT OF WAY, ALSO STATE ROAD NUMBER S-115-A);

THENCE NORTH 24°49'47" WEST, A DISTANCE OF 65.95 FEET TO AN INTERSECTION WITH THE NORTHWESTERLY RIGHT OF WAY LINE OF LAST SAID ROAD, SAID POINT BEING IN THE ARC OF A CURVE CONCAVE SOUTHEASTERLY AND HAVING A RADIUS OF 987.93 FEET;

THENCE SOUTHWESTERLY ALONG SAID NORTHWESTERLY RIGHT OF WAY LINE, AND ALONG AND AROUND SAID CURVE, AN ARC LENGTH OF 509.26 FEET TO THE POINT OF TANGENCY OF SAID CURVE, SAID ARC BEING SUBTENDED BY A CHORD BEARING AND DISTANCE OF SOUTH 50°34'33" WEST, 503.64 FEET;

THENCE SOUTHWESTERLY CONTINUING ALONG LAST SAID RIGHT OF WAY RUN THE FOLLOWING 9 COURSES AND DISTANCES;

COURSE 1: SOUTH 35°48'30" WEST, A DISTANCE OF 455.79 FEET;

COURSE 2: NORTH 54°11'30" WEST, A DISTANCE OF 15.00 FEET;

COURSE 3: SOUTH 34°48'30" WEST, A DISTANCE OF 10.00 FEET;

COURSE 4: SOUTH 54°11'30" EAST, A DISTANCE OF 15.00 FEET;

COURSE 5: SOUTH 35°48'30" WEST, A DISTANCE OF 328.91 FEET;

COURSE 6: NORTH 54°11'30" WEST, A DISTANCE OF 15.00 FEET;

COURSE 7: SOUTH 34°48'30" WEST, A DISTANCE OF 15.00 FEET;

COURSE 8: SOUTH 54°11'30" EAST, A DISTANCE OF 15.00 FEET;

COURSE 9: SOUTH 35°48'30" WEST, A DISTANCE OF 1244.55 FEET TO AN INTERSECTION WITH THE NORTHEASTERLY RIGHT OF WAY LINE OF ARCHERY AVENUE (A 70 FOOT RIGHT OF WAY), SAID POINT BEING THE SOUTHEASTERLY CORNER OF LOT 46, BLOCK 2, SHERWOOD FOREST - UNIT 5, PLAT BOOK 28, PAGES 69 AND 69A, OF SAID CURRENT PUBLIC RECORDS;

THENCE NORTHWESTERLY ALONG THE SAID RIGHT OF WAY LINE, RUN THE FOLLOWING 2 COURSES AND DISTANCES;

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COURSE 1: NORTH 54°11'30" WEST, A DISTANCE OF 527.23 FEET TO THE POINT OF CURVATURE OF A CURVE CONCAVE TO THE SOUTHWEST HAVING A RADIUS OF 601.41 FEET;

COURSE 2: THENCE ALONG AND AROUND THE ARC OF SAID CURVE, AN ARC LENGTH OF 456.54 FEET TO THE POINT OF TANGENCY OF SAID CURVE, SAID CURVE BEING SUBTENDED BY A CHORD BEARING AND DISTANCE OF NORTH 75°56'20" WEST, 445.66 FEET;

THENCE SOUTH 82°18'50" WEST, CONTINUING ALONG THE LAST SAID RIGHT OF WAY LINE AND A WESTERLY PROLONGATION THEREOF, A DISTANCE OF 769.49 FEET TO AN INTERSECTION WITH THE WESTERLY RIGHT OF WAY LINE OF SIBBALD ROAD (AN 80 FOOT RIGHT OF WAY);

THENCE SOUTH 07°41'10" EAST, ALONG LAST SAID RIGHT OF WAY LINE, A DISTANCE OF 1708.69 FEET TO THE SOUTHEASTERLY LINE OF LANDS DESCRIBED IN OFFICIAL RECORDS VOLUME 4784, PAGE 389 OF SAID CURRENT PUBLIC RECORDS;

THENCE SOUTH 88°56'17" WEST, ALONG THE NORTHERLY LINE OF SECTION 29 AND THE WESTERLY EXTENSION THERE OF, A DISTANCE OF 552.74 FEET TO AN INTERSECTION WITH THE WESTERLY RIGHT OF WAY LINE OF BRONSON LANE (A 37.50 FOOT RIGHT OF WAY);

THENCE SOUTH 00°50'36" EAST, ALONG SAID RIGHT OF WAY LINE, A DISTANCE OF 817.44 FEET TO A POINT OF CURVATURE OF A CURVE, SAID CURVE BEING CONCAVE NORTHWESTERLY AND HAVING A RADIUS OF 25.00 FEET;

THENCE SOUTHWESTERLY ALONG AND AROUND SAID CURVE AN ARC LENGTH OF 52.66 FEET TO THE POINT OF TANGENCY OF SAID CURVE, SAID ARC BEING SUBTENDED BY A CHORD BEARING AND DISTANCE OF SOUTH 59°30'12" WEST, 43.45 FEET, TO THE INTERSECTION WITH THE NORTHEASTERLY RIGHT OF WAY LINE OF MONCRIEF AVENUE (A 100 FOOT RIGHT OF WAY);

THENCE SOUTH 29°51'00" WEST, LEAVING THE SAID NORTHEASTERLY RIGHT OF WAY LINE, A DISTANCE OF 100.00 FEET TO AN INTERSECTION WITH THE SOUTHWESTERLY RIGHT OF WAY LINE OF SAID MONCRIEF AVENUE (A 100 FOOT RIGHT OF WAY);

THENCE SOUTH 60°09'00" EAST, ALONG LAST SAID RIGHT OF WAY LINE, A DISTANCE OF 56.55 FEET TO AN INTERSECTION WITH THE WESTERLY RIGHT OF WAY LINE OF HULL AVENUE (A 50 FOOT RIGHT OF WAY);

THENCE SOUTH 29°49'13" WEST, ALONG SAID RIGHT OF WAY LINE AND A SOUTHWESTERLY PROLONGATION THEREOF, A DISTANCE OF 721.73 FEET TO AN INTERSECTION WITH THE SOUTHWESTERLY RIGHT OF WAY LINE OF AGESON ROAD (A 50 FOOT RIGHT OF WAY);

THENCE SOUTH 60°22'00" EAST, ALONG LAST SAID RIGHT OF WAY LINE AND A SOUTHEASTERLY PROLONGATION THERE OF, A DISTANCE OF 310.16 FEET TO AN INTERSECTION WITH THE SOUTHEASTERLY RIGHT OF WAY LINE OF SOUTEL DRIVE (A 66 FOOT RIGHT OF WAY AND STATE ROAD NUMBER S-115-A), SAID POINT BEING IN THE ARC OR A CURVE, SAID CURVE BEING CONCAVE SOUTHEASTERLY AND HAVING A RADIUS OF 3786.72 FEET;

THENCE NORTHEASTERLY ALONG AND AROUND SAID CURVE, AN ARC LENGTH OF 85.73 FEET, TO AN INTERSECTION WITH THE NORTHERLY LINE OF LANDS DESCRIBED IN OFFICIAL RECORDS VOLUME 6528, PAGE 1583 OF THE CURRENT PUBLIC RECORDS OF SAID COUNTY, SAID ARC BEING SUBTENDED BY CHORD BEARING AND DISTANCE, NORTH 28°32'38" EAST, 85.73 FEET;

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THENCE SOUTH 61°34'00" EAST, ALONG LAST SAID LINE, A DISTANCE OF 449.01 FEET TO THE WESTERLY BOUNDARY OF WASHINGTON ESTATES UNIT 3, PLAT BOOK 32, PAGES 28 AND 28A, OF SAID CURRENT PUBLIC RECORDS;

THENCE NORTH 29°19'00" EAST, ALONG SAID WESTERLY BOUNDARY, A DISTANCE OF 639.91 FEET TO AN INTERSECTION WITH THE SOUTHWESTERLY RIGHT OF WAY LINE OF SAID MONCRIEF AVENUE;

THENCE SOUTH 61°26'38" EAST, ALONG LAST SAID RIGHT OF WAY LINE, A DISTANCE OF 687.16 FEET TO A POINT OF CURVATURE, SAID CURVE BEING CONCAVE NORTHEASTERLY, AND HAVING A RADIUS OF 1005.37 FEET;

THENCE ALONG AND AROUND THE ARC OF SAID CURVE, AN ARC LENGTH OF 351.72 FEET TO THE POINT OF TANGENCY OF SAID CURVE, SAID ARC BEING SUBTENDED BY A CHORD BEARING AND DISTANCE OF SOUTH 71°11'31" EAST, 349.93 FEET;

THENCE CONTINUING ALONG SAID RIGHT OF WAY LINE, SOUTH 81°31'33" EAST, A DISTANCE OF 508.27 FEET TO AN ANGLE POINT;

THENCE CONTINUING ALONG SAID RIGHT OF WAY LINE, SOUTH 81°41'17" EAST, A DISTANCE OF 1371.00 FEET, MORE OR LESS, TO THE MEAN HIGH WATER LINE OF THE RIBAUT RIVER, PER OFFICIAL RECORDS VOLUME 7032, PAGE 2071 OF SAID PUBLIC RECORDS;

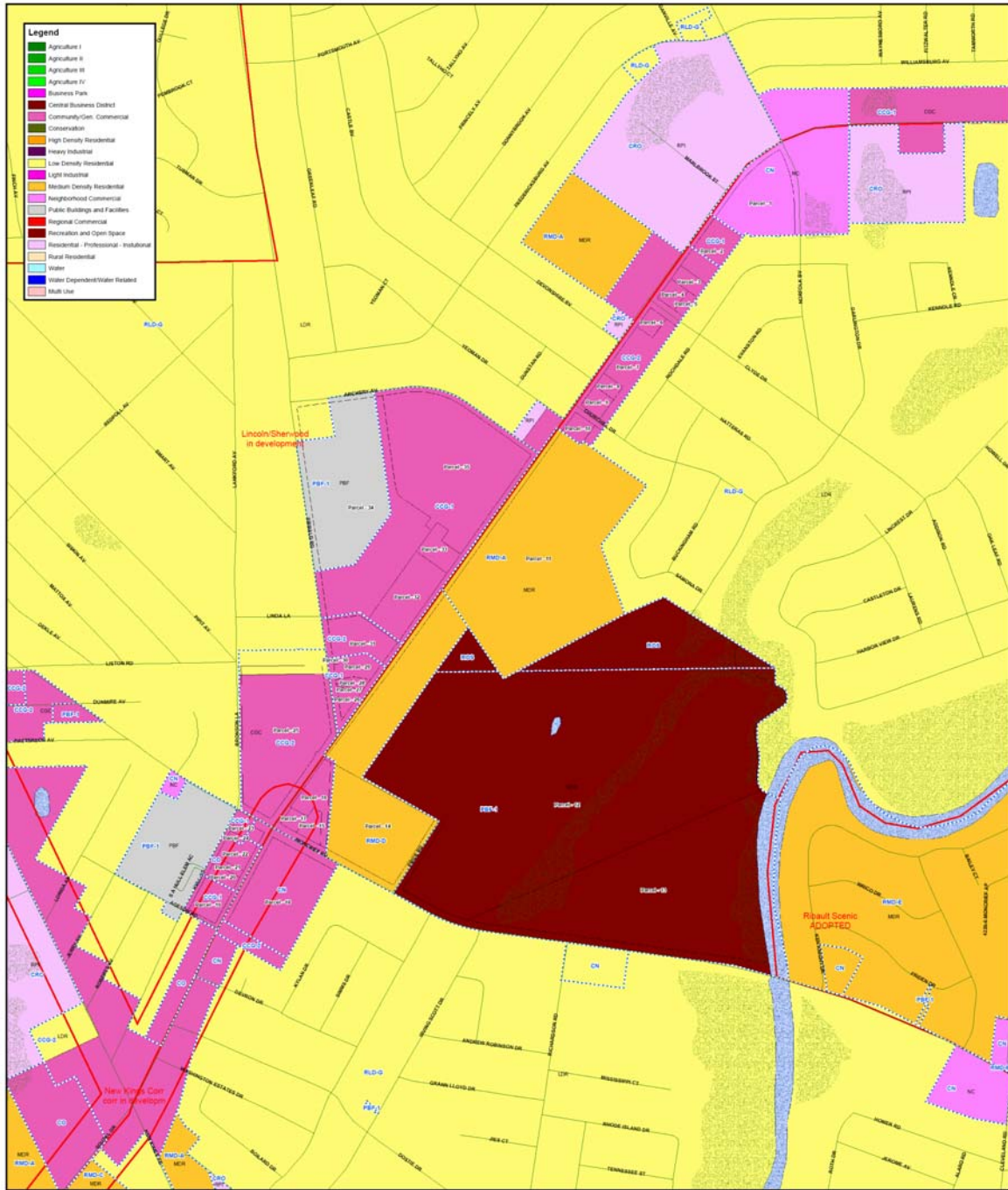
THENCE NORTHERLY ALONG SAID MEAN HIGH WATER LINE OF THE RIBAUT RIVER AND THE MEANDERINGS THEREOF A DISTANCE OF 2300.00 FEET, MORE OR LESS, TO ITS INTERSECTION WITH A LINE WHICH BEARS SOUTH 56°57'00" WEST FROM THE POINT OF BEGINNING;

THENCE NORTH 56°57'00" WEST, ALONG THE SOUTHEASTERLY BOUNDARY OF HARBOR VIEW - UNIT FIVE, PLAT BOOK 29, PAGES 92 AND 92A, OF SAID CURRENT PUBLIC RECORDS, A DISTANCE OF 440.00 FEET, MORE OR LESS, TO THE POINT OF BEGINNING.

LANDS THUS DESCRIBED CONTAINING 244.63 ACRES, MORE OR LESS.

**APPENDIX B  
FUTURE LAND USE  
CATEGORIES**

**APPENDIX B**  
**Future Land Use Categories**  
**Soutel / Moncrief / Retail Redevelopment Area**



**Soutel Drive and Moncrief Road - LUZ**

Map Scale: 1:2,250  
 Created: January 2008  
 Developed by: Johnnie Saxon  
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**Map B-1:**  
**Existing Zoning**

**COMMERCIAL**

This category is primarily intended to provide for all types of sales and service activities, such as retail trade, personal and professional services and storage, offices, hotels, motels, entertainment, and amusement facilities. Adult entertainment facilities are allowed by right in the heavy industrial land use category, but not in commercial. Commercial recreation and entertainment activities, such as amusement parks and marinas, are also allowed in this category. Single family dwellings and multi-family uses are also permitted consistent with the Medium Density Residential (MDR) and High Density Residential (HDR) plan category descriptions. A limited amount of warehousing, wholesaling, fabrication and light industrial uses may also be allowed subject to the standards and criteria in the Land Development Regulations. The plan includes five types of commercially dominated land use categories: Residential-Professional-Institutional, Neighborhood Commercial, Community/General Commercial, Regional Commercial, and Central Business District. The primary uses range from a small convenience store, laundry/dry cleaning shop to a large shopping center or a multi-story office building. Secondary uses allowed in the residential categories are also allowed in all commercial categories. In addition, secondary and supporting uses having external impacts similar to the primary uses described above are also included. Examples of these uses are: trade schools and colleges, hospitals, medical centers, and sanitariums, museums, art galleries, convention, exhibition and trade festival facilities, transit stations, and off-street parking lots and garages. In addition to the above, criminal justice facilities, transportation terminals and facilities, stadiums and arenas, yard waste composting, dude ranches, riding academies, shooting ranges, commercial fishing or hunting camps, fairgrounds etc.; may also be allowed in some commercial categories. Not all primary or secondary/supporting uses stated above would be permitted in all commercial land use categories. The intensity and range of uses permitted, in a specific commercial category, are subject to the provisions of this and other elements of the 2010 Comprehensive Plan, and all applicable Land Development Regulations.

***Residential-Professional-Institutional (RPI)***

This is a mixed use category primarily intended to accommodate office, limited commercial retail and service establishments, institutional and medium density residential uses. Large scale institutional uses, which require supporting residential and office components, are also permitted; as are office-professional uses as well as mixed use developments utilizing the "Traditional Neighborhood Design" (TND) concept, which are predominantly residential but also include a broad mixture of supporting recreational, commercial, public facilities and services, subject to the standards and criteria in the Land Development Regulations. In addition to the secondary and supporting uses for all commercial land use categories listed heretofore, veterinarians, filling stations, off street parking, nursing homes, residential treatment facilities, day care centers, and other institutional uses such as libraries, public/private schools, colleges and universities, cemeteries, mausoleums but not funeral homes or mortuaries, private clubs, art galleries, museums, theaters and related uses may also be permitted when sited in compliance with this and other elements of the 2010 Comprehensive Plan and all applicable Land Development regulations. The location, type, scale and density/intensity of the supporting and secondary uses shall be compatible with the overall character of the existing, as well as the proposed future development of the area. This category permits housing and mixed use developments in a gross density range of up to twenty (20) dwelling units per acre when full urban services are available to the site. Generally, low rise multi-family dwellings such as apartments condominiums, town homes and row houses will be the predominant land use in this category, although cluster and patio home mixed use projects, single family dwellings, Single Room Occupancies (SROs), and supporting neighborhood commercial retail, professional offices and institutional uses may also be developed in appropriate locations. The scale of individual commercial/residential buildings or developments will depend upon locational criteria established in the Land

**APPENDIX B**  
**Future Land Use Categories**  
**Soutel / Moncrief / Retail Redevelopment Area**

Development Regulations and on the Functional Highway Classification System Map. The mix of land uses included in this category is flexible and consists of up to 60 percent residential, up to 50 percent commercial and service establishments, up to 70 percent institutional and up to 80 percent of the land area as professional offices. RPI developments are frequently appropriate transitional uses between residential and non-residential areas.

***Neighborhood Commercial (NC)***

These uses serve the daily needs of contiguous neighborhoods. Neighborhood commercial nodes will generally be located within a ten minute drive time of the service population. The locational standards are in the Land Development Regulations to ensure that these uses do not penetrate into residential neighborhoods, but rather serve as they are intended, to provide for the daily needs of those areas. Neighborhood commercial uses may include convenience goods, personal services, veterinarians, filling stations and other low intensity retail and office-professional commercial uses developed in freestanding or shopping center configurations. Normally, such centers will be anchored by a food or drug store and will contain four to ten other supporting retail and office uses. New neighborhood commercial uses shall not be allowed as secondary uses in existing single-family residential neighborhoods when such uses would constitute intrusion.

Secondary and supporting uses included in all commercial categories as well as the additional uses allowed in the RPI land use category may also be permitted in this category. The standards to be prepared as Land Development Regulations and the criteria herein only designate locations that may be considered for neighborhood commercial uses. Consideration does not guarantee the approval of a particular retail or office commercial use in any given location. The location and expansion of secondary, supporting uses in and on the peripheries of neighborhoods should be consistent in location and design with the development standards, as well as all applicable Land Development Regulations. The location, type, scale and density/intensity of the supporting and secondary uses shall be compatible with the overall character of the existing, as well as the proposed future development of the area. Neighborhood commercial sites should abut a roadway classified as a collector or higher facility on the adopted highway functional classification system map, which is part of the 2010 Comprehensive Plan. Sites with two or more property boundaries on transportation rights-of-way will be considered preferred locations for neighborhood serving uses.

***Community/General Commercial (C/GC)***

Community/general commercial uses are generally developed in nodal patterns and serve large areas of the City, which include a diverse set of neighborhoods with a combined service population of at least 25,000 people or 10,000 dwelling units. Community/general commercial nodes will generally be located within a fifteen minute drive time of the service population. Such uses may only be developed within the commercial and commercially dominated mixed use plan categories of this element. This type of development includes outlets and establishments that offer a wide range of goods and services including general merchandise, apparel, food and related items. Neighborhood commercial scale uses and projects may be part of a community/general commercial node. Community/general commercial centers are generally developed with a variety or small department store(s) as its primary anchor. General commercial uses include business and professional offices, financial institutions highway commercial such as auto repair and sales, mobile home/motor home rental and sales, off street parking lots and garages, boat storage and sales, hotel, motel, fast food establishments, commercial, recreational and entertainment facilities



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such as carnivals or circuses, theaters, shooting galleries, skating rinks, athletic complexes, arenas, auditoriums, racetracks, and similar other types of commercial developments. In addition, warehousing, light manufacturing and fabricating could be permitted provided it is part of a retail sales or service establishment, and the use must be located on a road classified as collector or higher on the Functional Highway Classification Map. Commercial uses will comprise 70 to 90 percent of the land area of the category, while warehousing, light manufacturing and fabricating would constitute the remaining 10 to 30 percent. These uses may be developed as freestanding uses within the area of the Community/General Commercial node, or as separate general commercial projects.

In addition to secondary and supporting uses allowed in all commercial categories, multi-family uses, nursing homes, group care facilities, trade schools and colleges, hospitals, medical centers, sanitariums, museums, criminal justice facilities, art galleries, exhibition and trade facilities and similar other institutional uses, dude ranches, riding academies, private camps, camping grounds, shooting ranges, fishing and hunting camps, fairgrounds, race tracks, stadiums and arenas, transit stations, bus and other transportation terminals (but not freight or truck terminals), personal property storage establishments, crematoria, blood donation and plasma centers, building trade contractors, rescue missions, residences in conjunction with a permitted use, and day labor pools etc. may also be allowed within the Community/General Commercial land use category. Single family and multi-family residences as part of a mixed use development and those which were originally designed and legally built as single or multi-family residences prior to adoption of the Comprehensive Plan are allowed within this category, as long as there is not an adopted Neighborhood Action Plan recommending against them. No new residential use will be more than 90% of the land area of a parcel of greater than 50 acres. Residential uses shall generally not be the sole use within the CGC land use category but may be greater than the commercial uses. Land development regulations shall include standards for minimum setbacks and buffer zones between different types of uses allowed in this category, as well as locational criteria for such uses. Not all potential uses are routinely acceptable anywhere within this land use category. Each potential primary or secondary use must be evaluated for compliance with this and other elements of the 2010 Comprehensive Plan as well as applicable Land Development Regulations. The location, type, scale and density/intensity of the supporting and secondary uses shall be compatible with the overall character of the existing, as well as the proposed future development of the area.

Nodal development patterns at highway intersections are preferred and generally all new community/general commercial uses will be developed in this pattern. An exception to this standard may occur in areas where commercial infill can occur on commercially designated sites and where infill would create a more compact use pattern than development of new commercial nodes in the same area. The standards to be prepared as Land Development Regulations and the criteria herein only designate locations that may be considered for community commercial uses. Consideration does not guarantee the approval of a particular retail or office commercial use in any given location. Community commercial uses should abut a roadway classified as an arterial or higher facility on the adopted highway functional classification system map, which is part of the 2010 Comprehensive Plan. Sites with two or more boundaries on a transportation right-of-way will be considered preferred locations for these uses.

**RESIDENTIAL**

The residential categories primarily allow for single-family dwellings, multi-family dwellings, group homes, foster care facilities, community residential homes and other congregate living facilities in appropriate locations. Not included are hotels, motels, campgrounds, travel trailer parks for the transient population, and similar other commercial facilities. Also excluded are facilities such as jails, prisons,

**APPENDIX B**  
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hospitals, sanitariums and similar other facilities and dormitories, etc. Areas depicted for residential uses are shown under four residentially dominated plan categories; Rural Residential (RR), Low Density Residential (LDR), Medium Density Residential (MDR), and High Density Residential (HDR). Various housing types, ranging from detached single-family dwelling units at a density of one (1) unit per acre, to attached multi-family structures at densities of over 20 dwelling units per acre, are allowed in the various categories. In order to encourage infill and redevelopment on existing lots of record within the Urban Area, the net density may exceed the allowable gross density limitation specified herein, provided such density is consistent with the applicable Land Development Regulation for such area. Achievement of the density ranges of the various categories will only be permitted when full urban services are available to the development site, subject to the provisions of this and other applicable elements of the 2010 Comprehensive Plan. It is not the intent of this plan to permit maximum allowable densities throughout the area depicted for a particular land use category on the Future Land Use Map series. Land Development Regulations that will be developed to implement the plan shall ensure that several development districts are established within each residential category to allow a variety of lot sizes and development densities. Accordingly, the average residential density in each category will be much lower than the maximum allowable density. Notwithstanding the density limitation of the residential categories, one dwelling unit will be permitted on any nonconforming lot of record, which was existing on the effective date of the 2010 Comprehensive Plan. Development on such nonconforming lots of record shall be subject to all other plan provisions.

Certain secondary and supporting non-residential uses are permitted in all residential categories subject to the provisions of this and other elements of the 2010 Comprehensive Plan. These uses may include neighborhood supporting recreation facilities and public facilities, such as schools, churches, day care centers, fire stations, branch libraries, community centers, essential services, as well as supporting commercial and service establishments and home occupations. Golf, yacht, tennis and country clubs, driving ranges, cemeteries and mausoleums but not funeral homes or mortuaries may be allowed in residential categories subject to the applicable Land Development Regulations. Bed and breakfast establishments meeting the performance standards and criteria in the Land Development Regulations are also allowed in appropriate locations in the designated historic residential districts. In addition to the above, nursing homes, emergency shelter homes, rooming houses, residential treatment facilities, private clubs, animals other than household pets, borrow pits and drive through facilities associated with a permitted use may also be allowed as secondary or supporting uses in some of the residential land use categories. Not all primary or secondary/supporting uses stated above would be permitted in all residential land use categories. The intensity and range of uses permitted in a specific land use category are subject to the provisions of this and other elements of the 2010 Comprehensive Plan, and all applicable Land Development Regulations. New neighborhood commercial uses shall not be allowed, as secondary/supporting uses, where such uses would constitute an intrusion into an existing single-family neighborhood. In order to encourage more compact development pattern, mixed use, Traditional Neighborhood Design (TND) planned unit and cluster developments are allowed in all plan categories allowing predominantly residential uses. These types of conditionally controlled developments, whether at a development of regional impact (DRI) scale or below, are exempt from application of the nodal criteria of this element in their internal site designs.

***Low Density Residential (LDR)***

This category permits housing developments in a gross density range of up to seven (7) dwelling units per acre when full urban services are available to the site. Generally, single-family detached housing will be the predominant land use in this category, although mobile homes, patio homes and multi-family dwellings may also be permitted in appropriate locations. Mixed use developments utilizing "Traditional Neighborhood Design" (TND) concept, which are

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predominantly residential but include a broad mixture of supporting recreational, commercial, public facilities and services are also allowed subject to the standards and criteria in the Land Development Regulations. Minimum lot size shall be half acre per dwelling unit when both centralized potable water as well as wastewater is not available. The lot size shall be reduced to 1/4 acre per dwelling unit if either one of these services are not available. It is not intended to permit the maximum allowable density throughout the area depicted for this category on the Future Land Use Map series. Land Development Regulations shall include several development districts, each with different density range, which will be permitted in this category to permit a varied physical environment. In addition to the secondary and supporting uses for all residential land use categories listed heretofore, borrow pits, animals other than household pets, foster care homes, community residential homes and drive through facilities in conjunction with a permitted use may also be allowed in the LDR category subject to the standards and criteria in the Land Development Regulations.

***Medium Density Residential (MDR)***

This category permits housing developments in a gross density range of up to twenty (20) dwelling units per acre when full urban services are available to the site. Generally, low rise multi-family housing such as apartments, condominiums, townhomes and rowhouses will be the predominant land use in this category, although duplexes, mobile home parks and single family homes/mobile home subdivisions may also be developed in appropriate locations. Mixed use developments utilizing "Traditional Neighborhood Design" (TND) concept, which are predominantly residential but include a broad mixture of supporting recreational, commercial, public facilities and services are also allowed subject to the standards and criteria in the Land Development Regulations. MDR developments may be cited as transitional uses between single-family and commercial or public/semi-public use areas. In the absence of the availability of centralized water and sewer, the density of development permitted in this category shall be the same as allowed in LDR without such services. It is not intended to permit the maximum allowable density throughout the area depicted for this category on the Future Land Use Map series. Land Development Regulations shall include several development districts, each with different density range, which will be permitted in this category to permit a varied physical environment. In addition to the secondary and supporting uses indicated for all residential land use categories listed heretofore, nursing homes, emergency shelter homes, foster care homes, community residential homes, rooming houses, residential treatment facilities and private clubs are also allowed at appropriate locations in this category subject to the standards and criteria in the Land Development Regulations.

**RECREATION AND OPEN SPACE (ROS)**

This category includes lands used for activities that are associated with outdoor recreation, such as parks, playgrounds, golf courses, driving ranges, marinas, fairgrounds and spectator sports facilities in public and private ownership. Pastoral open space managed by the Recreation and Parks Department is also included. Secondary and supporting uses include dude ranches, riding academies, boarding stables, private camps, campgrounds, travel trailer parks, country clubs, private clubs, sale and service of alcoholic beverages for on-site consumption in conjunction with a permitted use, rifle or pistol shooting ranges, archery ranges, hunting and fishing camps and similar other recreational uses. The location, type, scale and density/intensity of the supporting and secondary uses shall be compatible with the overall character of the existing, as well as the proposed future development of the area. Areas designated as

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national or state forests, parks or preserves, although they offer excellent recreational opportunities, are included in the Conservation category. Private recreation and open space facilities that are part of residential, commercial or institutional land uses approved as integrated developments are also excluded since they are considered to be supporting uses to their projects. The FLUMs depicts major existing recreational facilities only since neighborhood scale recreational areas are allowed as secondary uses within the residential and commercial categories. The location of new recreational facilities will be guided by the provisions of this and other elements of the 2010 Comprehensive Plan. Recommendations in the more specific planning district, neighborhood or functional plans will also be considered in siting future recreational facilities.

**PUBLIC BUILDINGS AND FACILITIES (PBF)**

This is a broad land use category that is intended to identify major public use or community service activities. Uses include all lawful government activities, public buildings and grounds, schools, colleges and universities, criminal justice facilities, military installations, transportation facilities including airports, train stations, terminals etc.; along with ancillary and accessory uses such as warehouses, general aviation uses, hotels, motels, restaurants, car rental agencies, public/private institutions, churches, hospitals, including professional offices, medical clinics, pharmacies, and other uses normally associated therewith, private clubs, sale and service of alcoholic beverages in conjunction with a permissible use, major public utilities, and off street parking lots. Nursing homes, group care homes, homes for the aged or orphans and other uses normally associated therewith are also permitted within this land use category. All types of public facilities; i.e., institutional, communication and utilities, and transportation are combined into one category on the FLUMs. Only major Secondary uses in this category include recreation and open space such as ball parks, stadiums arenas and equestrian facilities, etc.; sale and service of alcoholic beverages in conjunction with a permitted activity, off street parking lots, silviculture activities, conservation areas, sanitary landfills, construction and demolition debris landfills, yard waste composting facilities including the mulching plant and similar other uses. Siting public/semi-public facilities that are allowed in commercial, light and heavy industrial, residential and institutional categories as supporting uses will not require plan amendment. Some major uses, however, because of their scale and potential community impacts, may only be sited in this plan category. Not all potential primary or secondary uses are permissible anywhere within this land use category. Each potential use must be evaluated for compliance with the provisions of this and other elements of the 2010 Comprehensive Plan, and all applicable Land Development Regulations. The location, type, scale and density/intensity of the supporting and secondary uses shall be compatible with the overall character of the existing, as well as the proposed future development of the area.

Activities that provide community service functions vary in character and locational need. A primary consideration in locating these uses is to ensure that each use will function as it is intended, as an important part of the urban service delivery system. The standards to be prepared as Land Development Regulations and the criteria herein only designate locations that may be considered for public/semi-public uses, and do not apply to military bases or other uses that do not directly serve the citizens of the City. Consideration does not guarantee approval of a particular use in any given location. With the exception of utility substations and other similar non-trip generating uses, community and regional serving public/semi-public sites should abut a roadway classified as a collector or higher facility on the adopted highway functional classification system map, which is part of the 2010 Comprehensive Plan.

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Sec. 656.311. Residential-Professional- Institutional Category.

This mixed use category primarily for office, institutional and medium density residential uses. Large scale institutional uses, which require supporting residential and office components, are permitted, as are office-professional uses, nursing homes, day care centers and related uses when sited in compliance with all applicable development regulations.

The category permits housing and mixed use developments in a gross density range of up to 20 dwelling units per acre when full urban services are available to the site. Generally, multi-family dwellings such as apartments, condominiums, townhomes and rowhouses will be the predominant land use in this category, although cluster and patio home developments, and supporting professional, office and institutional uses may also be developed in appropriate locations.

The mix of land uses included in this category is flexible and consists of up to 60 percent residential, up to 70 percent institutional and up to 80 percent of the land area as professional offices. Supporting retail commercial such as restaurants, barber/beauty shops, etc., not to exceed 25 percent of the building or development of which they are a part, are also allowed as part of the primary use structures. Developments in this category are frequently appropriate transitional uses between residential and nonresidential areas.

The following primary and secondary zoning districts may be considered in the residential professional and institutional category depicted on the Future Land Use Maps of the Comprehensive Plan.

A. *Primary zoning districts.* The primary zoning districts shall include the following:

- (1) Residential, Medium Density-E (RMD-E); Section 656.306.
- (2) Commercial Office (CO); Section 656.311.
- (3) Commercial Residential and Office (CRO); Section 656.311.

The commercial office and residential mixed use districts allow for business and professional offices and medium density residential uses of a density of 20 dwelling units per acre.

I. *Residential Medium Density-E (RMD-E).* Permitted uses and structures, accessory uses and structures, permissible uses by exception, minimum lot and yard requirements, and maximum lot coverage and height of buildings and structures shall be as provided in Section 656.306.

II. *Commercial Office (CO) District.*

(a) *Permitted uses and structures.*

- (1) Medical and dental or chiropractor offices (but not clinics or hospitals).
- (2) Professional offices.
- (3) Business offices.
- (4) Facilities for the production of eyeglasses, hearing aids, dentures, prosthetic appliances and similar products in conjunction with a professional service being rendered at the time provided the gross floor area shall not exceed 4,000 square feet.
- (5) Essential services, including water, sewer, gas, telephone, radio, television and electric, meeting the performance standards and development criteria set forth in Part 4.

(b) *Permitted accessory uses and structures.* See Section 656.403.

(c) *Permissible uses by exception.*

- (1) Day care centers meeting the performance standards and development criteria set forth in Part 4.

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- (2) Essential services, including water, sewer, gas, telephone, radio, television and electric, meeting the performance standards and development criteria set forth in Part 4.
- (3) Churches, including a rectory or similar use, meeting the performance standards and development criteria set forth in Part 4.
- (4) Off-Street parking lots meeting the performance standards and development criteria set forth in Part 4.
- (5) Facilities for the production of eyeglasses, hearing aids, dentures, prosthetic appliances and similar products, not in conjunction with a professional service being rendered at the time provided, if the gross floor area does not exceed 2,500 square feet.
- (d) *Minimum lot requirements (width and area).*
  - (1) Width--60 feet.
  - (2) Area--6,000 square feet (except as otherwise required for certain uses).
- (e) *Maximum lot coverage by all buildings and structures.* 35 percent.
- (f) *Minimum yard requirements.*
  - (1) Front--20 feet.
  - (2) Side--Ten feet.
  - (3) Rear--Ten feet.
- (g) *Maximum height of structures.* 35 feet; provided, height may be unlimited where all required yards are increased one foot for every one foot of building height in excess of 35 feet. Buildings in excess of 35 feet may not be located within three feet for every one foot of total building height or 200 feet, whichever is greater, from any property zoned either Residential, Low Density or Rural Residential.
- (h) *Limitations on permitted and permissible uses by exception.* All of the permitted and permissible uses by exception in the CO District are limited by the following conditions unless otherwise provided:
  - (i) No retail sales, display or storage of merchandise shall be permitted.
  - (ii) No vehicles other than passenger automobiles or trucks of not more than three-quarter-ton capacity shall be used.
  - (iii) No manufacture, repair or work of a mechanical nature of any kind shall be permitted and no machinery shall be used other than normal office equipment such as typewriters, calculators, computers and bookkeeping machines.

**III. Commercial, Residential and Office (CRO) District.**

- (a) *Permitted uses and structures.*
  - (1) Medical and dental or chiropractor offices (but not hospitals).
  - (2) Medical and dental or chiropractor clinics (but not hospitals).
  - (3) Professional offices.
  - (4) Business offices.
  - (5) Facilities for the production of eyeglasses, hearing aids, dentures, prosthetic appliances and similar products in conjunction with a professional service being rendered at the time provided and the gross floor area shall not exceed 4,000 square feet.
  - (6) Multiple-family dwellings.
  - (7) Private clubs.
  - (8) Schools meeting the performance standards and development criteria set forth in Part 4.

**Sec. 656.312. Neighborhood Commercial Category.**

This category permits retail uses, office commercial uses and services establishments which serve the daily needs of contiguous residential neighborhoods. Neighborhood commercial sites should abut a roadway classified as a collector or higher facility on the adopted Functional Highway Classification Map. Sites with two or more property boundaries on transportation rights-of-way will be considered

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preferred locations for neighborhood serving uses. The total area for neighborhood commercial centers should not exceed a total of 16 acres at any one intersection or node and not more than 50 percent of the total area should be located at one corner. New neighborhood commercial uses should not be allowed as secondary uses in existing single-family residential neighborhoods when such uses would constitute intrusion.

The following primary and secondary zoning districts may be considered in the Neighborhood Commercial Category depicted on the Future Land Use Maps of the Comprehensive Plan.

A. *Primary zoning districts.* The primary zoning districts shall include the following:

- (1) Commercial Office (CO); Section 656.311.
- (2) Commercial Neighborhood (CN); Section 656.312.

These districts allow neighborhood commercial uses which include convenience goods, personal services, and other low intensity retail and office uses developed in freestanding or shopping center configurations. Normally, such shopping centers will be anchored by a food and/or drug store and will contain other supporting retail and office uses.

I. *Commercial Office (CO) District.* The permitted uses and structures, accessory uses and structures, permissible uses by exception, minimum lot and yard requirements, maximum lot coverage and height of buildings and structures shall be as provided for in Section 656.311 herein.

II. *Commercial Neighborhood (CN) District.*

(a) *Permitted uses and structures.*

- (1) Medical and dental or chiropractor offices and clinics (but not hospitals).
- (2) Professional offices.
- (3) Business offices.
- (4) Facilities for the production of eyeglasses, hearing aids, dentures, prosthetic appliances and similar products in conjunction with a professional service being rendered at the time if the gross floor area shall not exceed 4,000 square feet.
- (5) Retail outlets for sale of food and drugs, wearing apparel, toys, sundries and notions, books and stationery, leather goods and luggage, jewelry (including watch repair but not pawnshops), art, cameras or photographic supplies (including camera repair), sporting goods, hobby shops and pet shop (but not animal boarding kennels), musical instruments, television and radio (including repair incidental to sales), florist or gift shops, delicatessens, bakeshops (but not wholesale bakeries), drugs and similar products.
- (6) Service establishments such as barber or beauty shops, shoe repair shops, restaurants (without drive-in or drive-thru facilities), interior decorators, self-service laundries or dry cleaners, tailors or dressmakers, laundry or dry cleaning pickup stations.
- (7) Banks (but not drive-thru tellers) and financial institutions, travel agencies and similar uses.
- (8) Libraries, museums and community centers.
- (9) An establishment or facility which includes the retail sale of beer or wine in sealed containers for off-premises consumption.
- (10) Veterinarians meeting the performance standards and development criteria set forth in Part 4.



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- (11) Essential services, including water, sewer, gas, telephone, radio, television and electric, meeting the performance standards and development criteria set forth in Part 4.
- (12) Employment office (but not a day labor pool).
- (13) Churches, including a rectory or similar use, meeting the performance standards and development criteria set forth in Part 4.
- (14) Art galleries, dance, art, gymnastics, karate and martial arts, music and photography studios, and theaters for stage performances (but not motion picture theaters).
  - (b) *Permitted accessory uses and structures.* See Section 656.403.
  - (c) *Permissible uses by exception.*
    - (1) Off-street parking lots for premises requiring off-street parking meeting the performance standards and development criteria set forth in Part 4.
    - (2) Filling stations for gasoline, diesel fuel or gasohol meeting the performance standards and development criteria set forth in Part 4.
    - (3) Retail outlets for sale of used wearing apparel, toys, books, luggage, jewelry, cameras and sporting goods.
    - (4) An establishment or facility which includes the retail sale of beer or wine for on-premises consumption.
    - (5) An establishment or facility which includes the retail sale of alcoholic beverages including liquor, beer or wine for on-premises consumption, in conjunction with the service of food which is ordered from a menu and prepared or served for pay for consumption on-premises.
    - (6) Essential services, including water, sewer, gas, telephone, radio, television and electric, meeting the performance standards and development criteria set forth in Part 4.
    - (7) Drive-thru facilities in conjunction with a permitted or permissible use or structure.
    - (8) Day care centers meeting the performance standards and development criteria set forth in Part 4.
  - (d) *Minimum lot requirements (width and area).*
    - (1) Minimum lot width--75 feet, except as otherwise required for certain uses.
    - (2) Minimum lot area--7,500 square feet, except as otherwise required for certain uses.
  - (e) *Maximum lot coverage by all buildings and structures.* 35 percent.
  - (f) *Minimum yard requirements.*
    - (1) All uses:
      - (i) Front--20 feet or, where the lot is adjacent to a residential district the required front yard setback of the district, whichever is greater.
      - (ii) Side--None if the building or the adjacent lot is built to the property line or if the adjacent lot is vacant. Unless no space is left between buildings on adjacent lots, a space where the lot is adjacent to residentially zoned property, a minimum side yard of 15 feet shall be provided.
      - (iii) Rear--Ten feet.
  - (g) *Maximum height of structures.* 35 feet.
  - (h) *Limitations on permitted or permissible uses by exception.* All of the permitted or permissible uses by exception are subject to the following provisions:

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- (1) Sale, display, preparation and storage shall be conducted within a completely enclosed building and no more than 20 percent of the floor space shall be devoted to storage.
- (2) Products shall be sold only at retail.

B. *Secondary zoning districts.* The following secondary zoning districts may be permitted in the Commercial Neighborhood Category as depicted on the Future Land Use Maps of the Comprehensive Plan, subject to the district regulations for same.

- (1) Agriculture (AGR); Section 656.331.
- (2) Public Buildings and Facilities-1 (PBF-1); Section 656.332.
- (3) Public Buildings and Facilities-2 (PBF-2); Section 656.332.
- (4) Conservation (CSV); Section 656.333.
- (5) Planned Unit Development (PUD); Section 656.340.

The aforementioned secondary zoning districts may be permitted provided that the supplemental criteria and standards for same specified in Subpart G, Part 3 are met.

(Ord. 91-59-148, § 1; Ord. 91-527-268, § 1; Ord. 91-761-410, § 1; Ord. 92-1760-1438, § 1; Ord. 93-672-1055, § 2; Ord. 94-195-171, § 2; Ord. 94-483-331, § 1; Ord. 98-528-E, § 1; Ord. 1999-1009-E, § 1)

Sec. 656.313. Community/General Commercial Category.

This category allows a wide range of retail sales and services including general merchandise, apparel, food and related items. Neighborhood commercial sale uses may be part of community shopping centers and are generally developed with a variety of small department store(s) as its primary anchor. General commercial uses include offices, highway commercial, entertainment and similar other types of commercial developments.

Community/general commercial uses should abut a roadway classified as an arterial or higher facility on the adopted Functional Highway Classification Map of the Comprehensive Plan. Nodal development patterns at highway intersections are preferred and generally all new community/general commercial uses will be developed in this pattern. An exception to this standard may occur in areas of commercial infill where infill would create a more compact use pattern than development of new commercial nodes in the same area. The total area for a community/general commercial use node should not exceed a total of 36 acres at an intersection node, not more than 50 percent of which may be located in any one corner of the intersection. The following primary and secondary zoning districts may be considered in the Community/General Commercial Category depicted on the Future Land Use Maps of the Comprehensive Plan.

A. *Primary zoning districts.* The primary zoning districts shall include the following:

- (1) Commercial Office (CO); Section 656.311.
- (2) Commercial Neighborhood (CN); Section 656.312.
- (3) Commercial Community/General-1 (CCG-1); Section 656.313.
- (4) Commercial Community/General-2 (CCG-2); Section 656.313.
- (5) Public Buildings and Facilities-2 (PBF-2); Section 656.332.

I. *Commercial Office (CO) District.* The permitted uses and structures, accessory uses and structures, permissible uses by exception, minimum lot and yard requirements and maximum lot coverage and height of buildings and structures shall be as provided in Section 656.311.

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II. *Commercial Neighborhood (CN) District.* The permitted uses and structures, accessory uses and structures, permissible uses by exception, minimum lot and yard requirements, and maximum lot coverage and height of buildings and structures shall be as provided in Section 656.312.

III. *Commercial Community/General-1 (CCG-1) District.*

(a) *Permitted uses and structures.*

- (1) Retail outlets for sale of food and drugs, wearing apparel, toys, sundries and notions, books and stationery, leather goods and luggage, jewelry (including watch repair) art, cameras or photographic supplies (including camera repair), sporting goods, hobby shops and pet shops (but not animal boarding kennels), musical instruments, florist or shops, delicatessens, bakeries (but not wholesale bakeries), home furnishings and appliances (including repair incidental to sales), office equipment or furniture, antiques, hardware, new automobile parts (including rebuilt parts not installation, repair or rebuilding of parts) and accessories and similar uses.
- (2) Service establishments such as barber or beauty shops, shoe repair shops, restaurants, interior decorators, reducing salons or gymnasiums, self-service laundries or dry cleaners, tailors or dressmakers, laundries or dry cleaning pickup stations, dry cleaning and laundry package plants in completely enclosed buildings using nonflammable liquids such as perchloroethylene and with no odor, fumes or steam detectable to normal senses from off the premises, radio and television broadcasting offices and studios, communication antennas, communication towers, funeral homes, marinas, blueprinting, job printing (but not newspaper), radio and television repair shops, travel agencies, employment offices, home equipment rental and similar uses.
- (3) Banks (including drive-thru tellers), loan companies, mortgage brokers, stockbrokers and similar financial institutions.
- (4) All types of professional and business offices, newspaper offices (but not printing), employment offices, union halls, buildings trades contractors (not requiring outside storage or the use of a vehicle in excess of one-ton capacity or equipment, machinery, ditching machines, tractors, bulldozers or other heavy construction equipment) and similar uses.
- (5) Hotels and motels.
- (6) Commercial indoor recreational or entertainment facilities such as bowling alleys, swimming pools, indoor skating rinks, theaters (including motion picture theaters but not open-air theaters), and similar uses (but not dance halls).
- (7) Art galleries, museums, community centers, dance, art or music studios, vocational, trade or business schools and similar uses.
- (8) Homes for aged and orphans.
- (9) Nursing homes and group care homes.
- (10) Day care centers or care centers meeting the performance standards and development criteria set forth in Part 4.
- (11) Off-street commercial parking lots meeting the performance standards and criteria set forth in Part 4.
- (12) Hospital, sanitariums and similar uses.
- (13) An establishment or facility which includes the retail sale and service of beer or wine for off-premises consumption or for on-premises conjunction with the service of food which is ordered from a menu and prepared or served for pay for consumption on-premises.
- (14) Retail plant nurseries (including outside display but not landscape contractors requiring heavy equipment or vehicles in excess of one-ton capacity).

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- (15) Express or parcel delivery offices, telephone exchanges, motor bus or other transportation terminals (but not freight or truck terminals) and similar uses.
- (16) Veterinarians subject to the performance standards and development criteria set forth in Part 4.
- (17) Personal property storage establishments meeting the performance development criteria set forth in Part 4.
- (18) Retail outlets for the sale of used wearing apparel, toys, books, luggage, jewelry, cameras, sporting goods, home furnishings and appliances, furniture and similar uses.
- (19) Essential services, including water, sewer, gas, telephone, radio, television and electric, meeting the performance standards and development criteria set forth in Part 4.
- (20) Churches, including a rectory or similar use.
- (21) Outside retail sales of holiday items, subject to the performance standards and development criteria set forth in Part 4.
- (22) Wholesaling or distributorship businesses located within a retail shopping center (but not on an out-parcel or within a stand-alone structure), provided such use is limited to 30 percent of the total gross square footage of the retail shopping center of which the wholesaling use or activity is a part, and further provided there is no warehousing or storage of products not directly associated with the wholesaling or distributorship businesses located on the premises.
  - (b) *Permitted accessory uses and structures.* See Section 656.403.
  - (c) *Permissible uses by exception.*
    - (1) An establishment or facility which includes the retail sale and service of all alcoholic beverages including liquor, beer or wine for on-premises consumption or off-premises consumption or both, including permanent or restricted outside sale and service, meeting the performance standards and development criteria set forth in Part 4.
    - (2) Bottle clubs as defined in Section 656.902(f).
    - (3) Residential treatment facilities and emergency shelters.
    - (4) Crematories.
    - (5) Service stations, service garages for minor repairs and car washer.
    - (6) Pawn shops (limited to items permitted in the CCG-1 Zoning District).
    - (7) Recycling collection points meeting the performance standards and development criteria set forth in Part 4.
    - (8) Retail outlets for the sales of new or used automobiles and trucks (empty gross vehicle weight of 5,000 pounds or less).
    - (9) Essential services, including water, sewer, gas, telephone, radio, television and electric, meeting the performance standards and development criteria set forth in Part 4.
    - (10) Blood donor stations, plasma centers and similar uses.
    - (11) Private clubs.
    - (12) Restaurants with the outside sale and service of food meeting the performance standards and development criteria set forth in Part 4.
    - (13) Billiard parlors.
    - (14) Service and repair of general appliances and small engines.
    - (15) Assembly of components and light manufacturing when in conjunction with a retail sales or service establishment, conducted without outside storage or display.
    - (16) Schools meeting the performance standards and development criteria set forth in Part 4.

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- (d) *Minimum lot requirements (width and area).* None, except as otherwise required for certain uses.
  - (e) *Maximum lot coverage by all buildings.* None, except as otherwise required for certain uses.
  - (f) *Minimum yard requirements.*
    - (i) Front--None.
    - (ii) Side--None, if the building on the adjacent lot is built to the property line or if the adjacent lot is vacant. Unless no space is left between buildings on adjacent lots, a space of not less than six feet shall be provided between buildings. Where the lot is adjacent to a residential district, a minimum setback of 15 feet shall be provided.
    - (iii) Rear--Ten feet.
  - (g) *Maximum height of structures.* Sixty feet; provided, however, that height may be unlimited where the building is set back on all sides not less than one horizontal foot for each six vertical feet in excess of 60 feet.
  - (h) *Limitations on permitted or permissible uses by exception.* All of the permitted and permissible uses by exception in the CCG-1 District are subject to the following provisions:
    - (1) Sale, service and display, preparation and storage shall be conducted within a completely enclosed building, unless otherwise provided for, and no more than 30 percent of the floor space shall be devoted to storage.
    - (2) Products shall be sold only at retail.
- IV. *Commercial Community/General-2 (CCG-2) District.*
- (a) *Permitted uses and structures.*
    - (1) Retail outlets for sale of food and drugs, wearing apparel, toys, sundries and notions, books and stationery, leather goods and luggage, jewelry (including watch repair), art, cameras or photographic supplies (including camera repair), sporting goods, hobby shops and pet shops, musical instruments, florist or gift shops, delicatessens, bakeries, home furnishing and appliances (including repair incidental to sales), office equipment or furniture antiques, hardware, new automobile parts (including rebuilt parts), accessories new or used automobiles, trucks and tractors, mobile homes, boats, pawn shops, automotive vehicle parts (but not automobile wrecking yards, junkyards or scrap processing yards), heavy machinery and equipment, dairy supplies, feed, fertilizer, plant nurseries, lumber and building supplies and similar products.
    - (2) Service establishments of all kinds, including automobile service stations or truck stops, minor and major repair and service garages, rental of automotive vehicles, trailers and trucks, auto laundries, restaurants, laundry or dry cleaning establishments, veterinarians or animal boarding kennels meeting the performance standards and development criteria set forth in Part 4, pest control, carpenter or cabinet shops, home equipment rentals, ice delivery stations, job printing or newspapers, marinas, radio or television offices and studios, broadcasting transmitters, telephone and cellular telephone antenna facilities, blood donor stations and similar uses.
    - (3) Commercial, recreational and entertainment facilities such as carnivals or circuses, theaters (including open-air theaters), shooting galleries, skating rinks, pony riders, athletic complexes, arenas, auditoriums, convention centers, dance halls, go-cart tracks, driving ranges and similar uses.
    - (4) Palmists, astrologists, psychics, clairvoyants, phrenologists and similar uses.

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- (5) Fruit, vegetable, poultry or fish markets.
- (6) All types of professional and business offices.
- (7) All establishment or facility which includes the retail sale and service of all alcoholic beverages including liquor, beer or wine for off-premises consumption and on-premises consumption in conjunction with the service of food which is ordered from a menu and prepared or served for pay for consumption on premises.
- (8) Wholesaling, warehousing, storage, jobber, distributorship business or construction equipment rental where the total operation does not require more than 10,000 square feet of floor space, no vehicle is used in excess of one and one-half ton capacity, all merchandise is stored within an enclosed building and no heavy machinery or manufacturing is located on the premises.
- (9) Hotels and motels.
- (10) Day care centers and care centers meeting the performance standards and development criteria set forth in Part 4.
- (11) Hospitals, sanitariums, nursing homes, housing for the elderly or orphans.
- (12) Boatyards.
- (13) Racetracks for animals or vehicles.
- (14) Light manufacturing, processing (including food processing but not slaughterhouse), packaging or fabricating.
- (15) Off-street commercial parking lots meeting the performance standards and development criteria set forth in Part 4.
- (16) Pawnshops.
- (17) Retail outlets for sale of used wearing apparel, toys, books, luggage, jewelry, cameras, sporting goods, home furnishing and appliances, furniture and similar uses.
- (18) Recycling collection points meeting the performance standards and development criteria set forth in Part 4.
- (19) Essential services, including water, sewer, gas, telephone, radio, television and electric, meeting the performance standards and development criteria set forth in Part 4.
- (20) Private clubs.
- (21) Service stations meeting the performance standards and development criteria set forth in Part 4.
- (22) Churches, including a rectory or similar use.
- (23) Personal property storage establishments meeting the performance standards and development criteria set forth in Part 4.
- (24) Vocational, trade and business schools.
  - (b) *Permitted accessory uses.* See Section 656.403.
  - (c) *Permissible uses by exception.*
    - (1) Residential treatment facilities or emergency shelter.
    - (2) Bottle clubs as defined in Section 656.902(f).
    - (3) Rescue missions.
    - (4) Day labor pools.
    - (5) Crematories.
    - (6) An establishment or facility which includes the retail sale and service of all alcoholic beverages including liquor, beer or wine for on-premises consumption.
    - (7) Adult entertainment and service activities.
    - (8) Building trades contractors with outside storage yards meeting the performance standards and development criteria set forth in Part 4.

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- (9) Essential services, including water, sewer, gas, telephone, radio, television and electric, meeting the performance standards and development criteria set forth in Part 4.
- (10) Travel trailer parks meeting the performance standards and development criteria set forth in Part 4.
- (11) Automobile storage yards.
- (12) Bus, semi-tractor (but not trailer) or truck parking and/or storage.
- (13) Schools meeting the performance standards and development criteria set forth in Part 4.
- (d) *Minimum lot requirements (width and area)*. None, except as otherwise required for certain uses.
- (e) *Maximum lot coverage by all buildings*. None, except as otherwise required for certain uses.
- (f) *Minimum yard requirements*.
  - (i) Front--None.
  - (ii) Side--None, if the building on the adjacent lot is built to the property line or if the adjacent lot is vacant. Unless no space is left between buildings on adjacent lots, a space of not less than six feet shall be provided between buildings. Where the lot is adjacent to a residential district, a minimum setback of 15 feet shall be provided.
  - (iii) Rear--Ten feet.
  - (iv) Where the lot is adjacent to a residential district without an intervening street, a minimum yard of 25 feet shall be provided along private property lines adjoining the residential district of which 15 feet along the property line adjoining the district shall not be used for principle uses and structures, off-street parking loading or unloading, storm water retention/detention, signs, or similar uses, but may be used for required landscaping pursuant to Part 12 of this Chapter.
- (g) *Maximum height of structures*. Sixty feet; provided, however, that height may be unlimited where the building is set back on all sides not less than one horizontal foot for each six vertical feet in excess of 60 feet.

V. *Public Buildings and Facilities-2 (PBF-2) District*. The permitted uses and structures, accessory uses and structures, permissible uses by exception, minimum lot and yard requirements, and maximum lot coverage and height of buildings and structures shall be as provided in Section 656.332.

- B. *Secondary zoning districts*. The following secondary zoning districts may be permitted in the Community/General Commercial Category as depicted on the Future Land Use Maps of the Comprehensive Plan, subject to the district regulations for same.
- (1) Industrial Business Park-1 (IBP-1); Section 656.321.
  - (2) Agriculture (AGR); Section 656.331.
  - (3) Public Buildings and Facilities-1 (PBF-1); Section 656.332.
  - (4) Public Buildings and Facilities-3 (PBF-3); Section 656.332.
  - (5) Conservation (CSV); Section 656.333.
  - (6) Recreation and Open Space (ROS); Section 656.334.
  - (7) Planned Unit Development (PUD); Section 656.340.

The aforementioned secondary zoning districts may be permitted provided that the supplemental criteria and standards for same specified in Subpart G, Part 3 are met.

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(Ord. 91-59-148, § 1; Ord. 91-522-167, § 1; Ord. 91-761-410, § 1; Ord. 92-58-30, § 1; Ord. 92-955-674, § 4; Ord. 93-178-112, § 1; Ord. 94-505-349, § 1; Ord. 94-77-412, § 1; Ord. 94-769-462, § 1; Ord. 94-1309-746, § 1; Ord. 95-305-296, § 2; Ord. 96-579-380, § 1; Ord. 97-405-E, § 1; Ord. 1999-242-E, § 1; Ord. 1999-834-E, § 1; Ord. 1999-546-E, § 1; Ord. 2000-743-E, § 1)

Sec. 656.314. Regional Commercial Category.

Regional commercial uses serve the City and surrounding areas. Combined service populations generally exceed 80,000 dwelling units. In size and scale, regional commercial development will meet generally the thresholds and guidelines for a development of regional impact (DRI) pursuant to F.S. § 380.06.

Regional commercial nodes will generally be located with convenient access to transit corridors and within a 30-minute drive time of the service population. Regional commercial uses may only be developed in a nodal pattern. Regional commercial uses should abut a roadway classified as a principal arterial or higher facility on the adopted Functional Highway Classification Map of the Comprehensive Plan. Sites with two or more boundaries on transportation rights-of-way classified as principal arterials or higher are preferred locations.

The following primary and secondary zoning districts may be considered in the Regional Commercial Category depicted on the Future Land Use Maps of the Comprehensive Plan.

- A. *Primary zoning districts.* The primary zoning districts shall include the following:
- (1) Commercial Community/General-1 (CCG-1); Section 656.313.
  - (2) Commercial Regional (CR); Section 656.314.

The Regional Commercial Category offers a full range of shopping goods, including general merchandise, apparel, home furnishings and related items community and neighborhood commercial uses and projects may be developed as part of a regional commercial node. Office, other service commercial, entertainment and residential uses may be developed within retail projects, as supporting freestanding uses within the area of the regional commercial node, or as autonomous office-professional use projects. Freestanding retail and other supporting uses developed in and around a primary regional commercial shopping center will be sited within the area of the regional commercial node.

I. *Commercial Community/General (CCG-1) District.* The permitted uses and structures, accessory uses and structures, permissible uses by exception, minimum lot and yard requirements, and minimum lot coverage and height of buildings and structures shall be as provided in Section 656.313.

II. *Commercial Regional (CR) District.*

(a) *Permitted uses and structures.*

- (1) Retail outlets for sale of food and drugs, wearing apparel, toys, sundries and notions, books and stationery, leather goods and luggage, jewelry (including watch repair but not pawnshops), art, cameras or photographic supplies (including camera repair), sporting goods, hobby shops and pet shops (but not animal boarding kennels), musical instruments, florist or gift shops, delicatessens, bakeries (but not wholesale bakeries), home furnishings and appliances (including repair incidental to sales), office equipment or furniture, antiques (but not secondhand merchandise), hardware.
- (2) Service establishments such as barber or beauty shops, shoe repair shops, restaurants, interior decorators, reducing salons or gymnasiums, tailors or dressmakers.
- (3) Banks, loan companies, mortgage brokers, stockbrokers and similar financial institutions.
- (4) All types of professional and business offices.



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- (5) Commercial indoor recreational or entertainment facilities such as billiard parlors, theaters (including motion picture theaters but not open-air theaters), games rooms and similar uses.
  - (6) Art galleries, museums, dance, art or music studios, vocational, trade or business schools and similar uses.
  - (7) An establishment or facility which includes the retail sale and service of all alcoholic beverages including liquor, beer or wine for off-premises consumption or for on-premises consumption in conjunction with the service of food which is ordered from a menu and prepared or served for pay for consumption on premises.
  - (8) Medical or dental offices and clinics.
  - (9) Daycare centers or care centers meeting the performance standards and development criteria set forth in Part 4.
  - (10) Schools meeting the performance standards and development criteria set forth in Part 4.
  - (11) Essential services, including water, sewer, gas, telephone, radio, television and electric, meeting the performance standards and development criteria set forth in Part 4.
    - (b) *Permitted accessory uses and structures.* See Section 656.403.
    - (c) *Permissible uses by exception.*
      - (1) An establishment or facility which includes the retail sale and service of all alcoholic beverages including liquor, beer or wine for on-premises consumption and off-premises consumption or both.
      - (2) Bottle clubs as defined in Section 656.802(c).
      - (3) Essential services, including water, sewer, gas, telephone, radio, television and electric, meeting the performance standards and development criteria set forth in Part 4.
    - (d) *Minimum lot requirements.* None.
    - (e) *Maximum lot coverage by all buildings.* None.
    - (f) *Minimum yard requirements.*
    - (i) Front--None.
    - (ii) Side--None.
    - (iii) Rear--Ten feet.
    - (g) *Maximum height of structures.* Sixty feet; provided, however, that height may be unlimited where the building is set back on all sides not less than one horizontal foot for each six vertical feet in excess of 60 feet.
    - (h) *Minimum zoning district size.* 40 acres.
- B. *Secondary zoning districts.* The following secondary zoning districts may be permitted in the Regional Commercial Category as depicted on the Future Land Use Maps of the Comprehensive Plan, subject to the district regulations for same.
- (1) Residential High Density-A (RHD-A); Section 656.307.
  - (2) Residential High Density-B (RHD-B); Section 656.307.
  - (3) Commercial Residential Office (CRO); Section 656.311.
  - (4) Commercial Community/General-2 (CCG-2); Section 656.313.
  - (5) Industrial Business Park-1 (IBP-1); Section 656.321.
  - (6) Public Building and Facilities-1 (PBF-1); Section 656.332.
  - (7) Public Building and Facilities-2 (PBF-2); Section 656.332.
  - (8) Public Building and Facilities-3 (PBF-3); Section 656.332.
  - (9) Conservation (CSV); Section 656.333.
  - (10) Planned Unit Development (PUD); Section 656.340.

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The aforementioned secondary zoning districts may be permitted provided that the supplemental criteria and standards for same specified in Subpart G, Part 3 are met.  
(Ord. 91-59-148, § 1; Ord. 91-761-410, § 1; Ord. 96-730-461, § 1)

**Sec. 656.306. Medium Density Residential Category.**

This category permits residential developments in a gross density range of up to 20 dwelling units per acre when full urban services are available to the site. Secondary and supporting nonresidential uses described in this category may also be permitted at appropriate locations subject to the performance standards and development criteria set forth in this Section. Medium density residential developments may be sited as transitional uses between single-family and commercial or public/semi-public use areas. Developments with higher densities should be sited in or adjacent to mass transit corridors and stations.

The following primary and secondary zoning districts may be considered in the Medium Density Residential Category depicted on the Future Land Use Maps of the Comprehensive Plan.

**A. Primary zoning districts.** The primary zoning districts shall include the following:

- (1) Residential Low Density-G (RLD-G); Section 656.305.
- (2) Reserved.
- (3) Residential Medium Density-A (RMD-A); Section 656.306.
- (4) Residential Medium Density-B (RMD-B); Section 656.306.
- (5) Residential Medium Density-C (RMD-C); Section 656.306.
- (6) Residential Medium Density-D (RMD-D); Section 656.306.
- (7) Residential Medium Density-E (RMD-E); Section 656.306.
- (8) Residential Medium Density-MH (RMD-MH); Section 656.306.

Generally, multiple-family dwellings such as apartments, condominiums, townhomes and rowhouses will be the predominant land use in the Residential Medium Density Districts, although other multiple-family, mobile home parks, single-family and mobile home subdivisions may also be developed in appropriate locations. Certain supporting open space uses, community facilities and utilities also may be permitted.

The district requirements for the Residential Low Density (RLD-G) and the Residential Medium Density (RMD-A, RMD-B, RMD-C, RMD-D, RMD-E and RMD-MH) zoning districts are specified below.

**I. Residential Low Density-G (RLD-G) District.** The permitted uses and structures, accessory uses and structures, permissible uses by exception, minimum lot and yard requirements and maximum lot coverage and height of building and structures shall be as provided in Section 656.305.

**II. Residential Medium Density-A (RMD-A), Residential Medium Density-B (RMD-B), Residential Medium Density-C (RMD-C), Residential Medium Density-D (RMD-D), Residential Medium Density-E (RMD-E) and Residential Medium Density-MH (RMD-MH) Districts.**

**(a) Permitted uses and structures.**

- (1) Single-family dwellings (RMD-A and RMD-B Districts only).
- (2) Mobile homes [RMD-MH (mobile home) district only].
- (3) Multiple-family dwellings (RMD-B, RMD-C, RMD-D and RMD-E Districts only).
- (4) Housing for the elderly.
- (5) Family day care homes meeting the performance standards and development criteria set forth in Part 4.
- (6) Foster care homes.
- (7) Community residential homes of six or fewer residents meeting the performance standards and development criteria set forth in Part 4.

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- (8) Essential services, including water, sewer, gas, telephone, radio, television and electric, meeting the performance standards and development criteria set forth in Part 4.
  - (9) Churches, including a rectory or similar use, meeting the performance standards and development criteria set forth in Part 4.
  - (10) Golf courses meeting the performance standards and development criteria set forth in Part 4.
  - (11) Parks, playgrounds and playfields or recreational or community structures meeting the performance standards and development criteria set forth in Part 4.
  - (12) Country clubs meeting the performance standards and development criteria set forth in Part 4.
  - (13) Home occupations meeting the performance standards and development criteria set forth in Part 4.
- (b) *Permitted accessory uses and structures.*
- (1) See Section 656.403.
  - (2) In connection with multiple-family dwellings, including housing for the elderly, coin-operated laundromats and other vending machine facilities, day care centers, establishments for sale of convenience goods, personal and professional service establishments; provided, however, that these establishments shall be designed and scaled to meet only the requirements of the occupants of these multiple-family dwellings or housing for the elderly and their guests and are located not less than 250 feet from and RLD District with no signs or other external evidence of the existence of these establishments.
  - (3) In connection with housing for the elderly, in projects with a minimum of 150 bedrooms, facilities for the sale of alcoholic beverages to occupants and their guests in accordance with (i) a Special Restaurant Exception beverage license issued pursuant to F.S. Ch. 561, as may be amended from time to time, and (ii) Part 8 of the City's Zoning Code; provided, that these facilities are located not less than 250 feet from an RLD district with no signs or other external evidence of the existence of these facilities.
- (c) *Permissible uses by exception.*
- (1) Cemeteries and mausoleums but not funeral home or mortuaries.
  - (2) Schools meeting the performance standards and development criteria set forth in the Part 4.
  - (3) Borrow pits subject to the regulations contained in Part 9.
  - (4) Bed and breakfast establishments meeting the performance standards and development criteria set forth in Part 4.
  - (5) Essential services, including water, sewer, gas, telephone, radio, television and electric, meeting the performance standards and development criteria set forth in Part 4.
  - (6) Day care centers meeting the performance standards and development criteria set forth in Part 4.
  - (7) Nursing homes.
  - (8) Residential treatment facilities.
  - (9) Private clubs.
  - (10) Mobile home parks (RMD-MH District only) subject to the requirements of Part 5.
  - (11) Reserved.
  - (12) Churches, including a rectory or similar use, meeting the performance standards and development criteria set forth in Part 4.
  - (13) Home occupations meeting the performance standards and development criteria set forth in Part 4.
  - (14) Emergency shelter homes (RMD-C, RMD-D and RMD-E Districts only).

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- (15) Community residential homes of seven to 14 residents meeting the performance standards and development criteria set forth in Part 4.
- (16) Golf driving ranges.
- (17) Rooming houses (RMD-D and RMD-E Districts only).
- (18) Group care homes (RMD-B, RMD-C, RMD-D and RMD-E Districts only).
- (19) Single-family residential use; provided, that the single-family residential use is a lawful, nonconforming use before an exception is granted. (RMD-B, RMD-C, RMD-D, and RMD-E Districts only).
- (d) *Minimum lot requirements (width and area).* For single-family dwellings, mobile homes and multiple-family dwellings the minimum lot requirements (width and area), except as otherwise required for certain other uses, are as follows:
  - (1) Single-family dwellings and mobile homes (RMD-A and RMD-B):
    - (i) Width--50 feet.
    - (ii) Area--5,000 square feet.
  - (2) Mobile home dwellings (RMD-MH) shall be specifically limited to the requirements of Part 5.
  - (3) All other uses:
    - (i) Width--60 feet.
    - (ii) Area:
      - (A) RMD-B--5,000 square feet for the first two family units and 1,000 square feet for each additional unit, not to exceed four family units on each lot.
      - (B) RMD-C--6,000 square feet for the first two family units and 4,400 square feet for each additional unit.
      - (C) RMD-D--6,000 square feet for the first two family units and 2,900 square feet for each additional unit.
      - (D) RMD-E--6,000 square feet for the first two family units and 2,100 square feet for each additional unit.
- (e) *Maximum lot coverage by all buildings and structures.* 35 percent; provided, however, that a minimum lot area or lands used for multiple-family dwellings and housing for the elderly and accessory buildings used in conjunction therewith to be retained in natural ground cover or landscaped shall be 35 percent of the total lot area.
- (f) *Minimum yard requirements.*
  - (1) Multiple-family dwellings and housing for the elderly on individual lot:
    - (i) Front--20 feet.
    - (ii) Side--Ten feet.
    - (iii) Rear--20 feet.
  - (2) Multiple-family dwellings and housing for the elderly with more than one principal structure on the lot:
    - (i) Front--20 feet.
    - (ii) Side--20 feet.
    - (iii) Rear--20 feet.
  - (3) Single-family dwellings and mobile homes located on individual lots:
    - (i) Front--20 feet.
    - (ii) Side--Five feet; provided, that combined side yards shall not be less than 15 feet.
    - (iii) Rear--Ten feet.
  - (4) All other uses:
    - (i) Front--20 feet.
    - (ii) Side--20 feet.
    - (iii) Rear--20 feet.

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- (5) Accessory use structures used in conjunction with multiple-family or housing for the elderly:
    - (i) Front--Accessory use structures shall not be permitted in front yards as they are established by the location of the principal structures.
    - (ii) Side and rear--Ten feet; provided, however, that no accessory use structures shall be placed in a side or rear yard in such a manner that the highest point of the structure will pierce an imaginary line extending from the highest point of the principal structure to a point on the ground at the property line of the side or rear yard in question.
  - (g) *Maximum height of structures.*
    - (1) Single-family dwellings, rooming houses, boardinghouses, child care centers, day care centers and accessory use structures--35 feet.
    - (2) All other uses--35 feet; provided, however, that height may be unlimited where all required yards are increased by one foot for each three feet of building height or fraction thereof in excess of 35 feet.
- B. *Secondary zoning districts.* The following secondary zoning districts may be permitted in the Medium Density Residential Category as depicted on the Future Land Use Maps of the Comprehensive Plan, subject to the district regulations for same.
- (1) Commercial Office (CO); Section 656.311.
  - (2) Commercial, Residential and Office (CRO); Section 656.311.
  - (3) Commercial Neighborhood (CN); Section 656.312.
  - (4) Agriculture (AGR); Section 656.331.
  - (5) Public Buildings and Facilities-1 (PBF-1); Section 656.332.
  - (6) Public Buildings and Facilities-2 (PBD-2); Section 656.332.
  - (7) Conservation (CSV); Section 656.333.
  - (8) Planned Unit Development (PUD); Section 656.340.

The aforementioned secondary zoning districts may be permitted provided that the supplemental criteria and standards for same specified in Subpart G herein are met.

(Ord. 91-59-148, § 1; Ord. 91-761-410, § 1; Ord. 93-1760-1177, § 2; Ord. 94-485-373, § 1; Ord. 94-504-375, § 1; Ord. 94-503-392, § 1; Ord. 2002-608-E, § 1)

Sec. 656.334. Recreation and Open Space Category.

This category includes lands used for activities that are associated with outdoor recreation in public and/or private ownership. Pastoral open space managed by the Recreation and Parks Department is also included.

The following primary and secondary zoning districts may be considered in the Recreation and Open Space Category depicted on the Future Land Use Maps of the Comprehensive Plan.

- A. *Primary zoning district.* The primary zoning district shall include the following:  
Recreation and Open Space (ROS); Section 656.334.

These districts allow outdoor recreation uses such as parks, playgrounds, golf courses, driving ranges, marinas, and spectator sports facilities. Public open space, developed or undeveloped, is also allowed, as are natural preservation areas which need to be protected.

- I. *Recreation and Open Space (ROS) District.*  
(a) *Permitted uses and structures.*

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- (1) Dude ranches, riding academies or boarding stables, if structures for the housing of animals are not located within 100 feet of a property line.
  - (2) Private camps, camping grounds, parks and recreational areas and travel trailer parks.
  - (3) Playgrounds and playfields.
  - (4) Country clubs, private clubs and golf courses meeting the performance standards and development criteria set forth in Part 4.
  - (5) Rifle, shotgun or pistol shooting ranges, field archery ranges, golf driving ranges and par-three golf courses.
  - (6) Marina, bait and tackle shops, commercial hunting or fishing camps.
  - (7) Fairgrounds.
  - (8) Open space, including natural vegetation/landscaping, water-bodies, etc.
  - (9) Essential services, including water, sewer, gas, telephone, radio and electric, meeting the performance standards and development criteria set forth in Part 4.
  - (b) *Permitted accessory uses and structures.* See Section 656.403.
  - (c) *Permissible uses by exception.*
    - (1) Sale and service of alcoholic beverages for on-premises or off-premises consumption in conjunction with a permitted or permissible use by exception.
    - (2) Race tracks for animals or vehicles.
    - (3) Ball parks, stadiums and arenas, etc.
    - (4) Essential services, including water, sewer, gas, telephone, radio and electric, meeting the performance standards and development criteria set forth in Part 4.
  - (d) *Minimum lot requirement (width and area).* The minimum lot requirement (width and area) for all uses is:
    - (1) Width--100 feet.
    - (2) Area--65,340 square feet.
  - (e) *Maximum lot coverage by all buildings.* Ten percent.
  - (f) *Minimum yard requirements.* The minimum yard requirements for all uses and structures are:
    - (1) Front--25 feet.
    - (2) Side--25 feet.
    - (3) Rear--25 feet.
  - (g) *Minimum height of structures.* 35 feet.
- B. *Secondary zoning districts.* The following secondary zoning districts may be permitted in the Recreation and Open Space Category as depicted on the Future Land Use Maps of the Comprehensive Plan, subject to the district regulations for same.
- (1) Public Buildings and Facilities-1 (PBF-1); Section 656.332.
  - (2) Agriculture (AGR); Section 656.331.
  - (3) Conservation (CSV); Section 656.333.
  - (4) Planned Unit Development (PUD); Section 656.340.

The aforementioned secondary zoning districts may be permitted provided that the supplemental criteria and standards for same specified in Subpart G, Part 3 are met.

(Ord. 91-59-148, § 1; Ord. 91-761-410, § 1)

Sec. 656.332. Public Buildings and Facilities Category.

This is a broad land use category that depicts major public use or community service activities. Uses include all types of public facilities; i.e., institutional, communication and utilities, and transportation. With the exception of utility substations and other similar non-trip generation uses, community and

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regional serving public/semi-public sites should abut a roadway classified as a collector or higher facility on the adopted Functional Highway Classification Map of the Comprehensive Plan.

The following primary and secondary zoning districts may be considered in the Public Buildings and Facilities Category depicted on the Future Land Use Maps of the Comprehensive Plan.

A. *Primary zoning districts.* The primary zoning districts shall include the following:

- (1) Public Building and Facilities-1 (PBF-1); Section 656.332.
- (2) Public Building and Facilities-2 (PBF-2); Section 656.332.
- (3) Public Building and Facilities-3 (PBF-3); Section 656.332.

These districts allow uses and activities that provide community service functions such as public buildings and grounds, schools, colleges and universities, criminal justice facilities, military installations, public/private institutions, major public utilities, and transportation facilities as described below.

It is the intent of this Zoning Code that all lands which are owned or leased by federal, state or local governments and are used for a purpose which is particularly and peculiarly related to governmental functions shall be located in a PBF-1 District. Any lawful government activity is permitted upon the lands without restriction so long as the title to the land is vested in or leased by the government and provided such use is consistent with the Comprehensive Plan. However, public housing shall not be considered a purpose which is particularly and peculiarly related to governmental functions, and need not be located in a PBF-1 District, so long as such housing is located in a zoning district which would otherwise permit such housing.

I. *Public Building and Facilities-1 (PBF-1) District.*

- (a) *Permitted uses and structures.* All lawful government uses, except those governmental uses listed hereunder as being permissible only by exception and further provided such uses are consistent with the Comprehensive Plan. This provision shall not preclude essential services meeting the performance standards and development criteria set forth in Part 4 herein from being also located in residential, commercial, industrial and other zoning districts which specifically provide for the same.
- (b) *Permissible uses by exception.*
  - (1) Essential services, including water, sewer, gas, telephone, radio and electric, meeting the performance standards and development criteria set forth in Part 4.
  - (2) Solid waste management facilities.
  - (3) Any government use structure containing more than 40,000 square feet.
  - (4) Any government use structures containing more than one principal structure on a lot.
- (c) *Minimum lot requirements (width and area).* None.
- (d) *Minimum yard requirements.* None.
- (e) *Maximum height of structures.* None.

II. *Public Buildings and Facilities-2 (PBF-2) District.*

- (a) *Permitted uses and structures.*
  - (1) Institutions, including middle and secondary schools, colleges and universities.
  - (2) Private clubs.
  - (3) Art galleries, libraries, museums and community centers.
  - (4) Homes for the aged or orphans, including professional offices associated therewith, and similar uses.
  - (5) Parks, playgrounds and playfields or recreational or community structures meeting the performance standards and development criteria set forth in Part 4.

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- (6) Churches, including a rectory or similar uses, meeting the performance standards and development criteria set forth in Part 4.
- (7) Nursing homes.
- (8) Essential services, including water, sewer, gas, telephone, radio and electric, meeting the performance standards and development criteria set forth in Part 4.
- (9) Hospitals, including professional offices, medical clinics and pharmacies, associated therewith.
- (10) Cemeteries.
- (11) Hospice facilities.
- (b) *Permitted accessory uses and structures.* See Section 656.403.
- (c) *Permissible uses by exception.*
  - (1) Communication facilities.
  - (2) Private utilities.
  - (3) Care centers meeting the performance standards and development criteria set forth in Part 4.
  - (4) Essential services, including water, sewer, gas, telephone, radio and electric, meeting the performance standards and development criteria set forth in Part 4.
  - (5) Sale and service of alcoholic beverages for on-premises or off-premises consumption in conjunction with a permitted or permissible use by exception.
  - (6) Churches, including a rectory or similar uses, meeting the performance standards and development criteria set forth in Part 4.
  - (7) Off-street parking lots meeting the performance standards and development criteria set forth in Part 4.
  - (8) Homeless center.
- (d) *Minimum lot requirements (width and area).*
  - (1) Width--70 feet.
  - (2) Area--7,000 square feet.
- (e) *Minimum yard requirements.*
  - (1) Front--20 feet.
  - (2) Side--Ten feet.
  - (3) Rear--Ten feet.
- (f) *Maximum height of structures.* 35 feet; provided, that height may be unlimited where all required yards are increased by one foot for each three feet of building height or fraction thereof in excess of 35 feet.
- (g) *Maximum lot coverage by all buildings.* 35 percent.

III. *Public Buildings and Facilities-3 (PBF-3) District.*

- (a) *Permitted uses and structures.*
  - (1) Institutions, including middle and secondary schools, colleges and universities.
  - (2) Private clubs.
  - (3) Art galleries, libraries, museums and community centers.
  - (4) Homes for the aged or orphans and similar uses.
  - (5) Parks, playgrounds and playfields or recreational or community structures meeting the performance standards and development criteria set forth in Part 4.
  - (6) Churches, including a rectory or similar uses, meeting the performance standards and development criteria set forth in Part 4.
  - (7) Nursing homes.
  - (8) Communication facilities.
  - (9) Group care homes meeting the performance standards and development criteria set forth in Part 4.



**APPENDIX C**  
**City of Jacksonville Zoning Districts**  
**Soutel / Moncrief / Retail Redevelopment Area**

- (10) Hospitals, professional offices, medical clinics and pharmacies, associated therewith.
- (11) Essential services, including water, sewer, gas, telephone, radio and electric, meeting the performance standards and development criteria set forth in Part 4.
- (12) Public transportation terminals such as airports and bus or train stations including normal uses customarily associated therewith including gift or curio shops, service establishments such as auto rental and travel agencies, barber or beauty shops, retail sales and service of alcoholic beverages for either on- or off-premise consumption, professional and business offices, wholesaling, warehousing, distribution and freight terminals, light manufacturing and fabricating, aeronautical schools, golf courses, agricultural, horticultural and forestry uses, aircraft maintenance and restoration facilities, and similar uses.
- (13) Cemeteries.
- (14) Hospice facilities.
- (b) *Permitted accessory uses and structures.* See Section 656.403.
- (c) *Permissible uses by exception.*
  - (1) Private utilities.
  - (2) Sanitary landfills and construction and demolition debris landfills meeting the performance standards and development criteria set forth in Part 4.
  - (3) Care centers meeting the performance standards and development criteria set forth in Part 4.
  - (4) Criminal justice facilities.
  - (5) Ball parks, stadiums and arenas, etc.
  - (6) Essential services, including water, sewer, gas, telephone, radio and electric, meeting the performance standards and development criteria set forth in Part 4.
  - (7) Reserved.
  - (8) Military installations.
  - (9) Sale and service of alcoholic beverages for on-premises or off-premises consumption in conjunction with a permitted or permissible use by exception.
  - (10) Churches, including a rectory or similar uses, meeting the performance standards and development criteria set forth in Part 4.
  - (11) Off-street parking lots meeting the performance standards and development criteria set forth in Part 4.
  - (12) Yard waste composting including the mulching process, meeting the performance standards and development criteria set forth in Part 4.
  - (13) Uses which are directly related to a permitted or permissible use by exception in the PBF-3 District, such as:
    - (i) Hotels or motels.
    - (ii) Golf courses.
    - (iii) Service stations.
    - (iv) Restaurants.
    - (v) Car rental agencies.
    - (vi) Fire stations.
    - (vii) Warehousing.
- (d) *Minimum lot requirements (width and area).*
  - (1) Width--100 feet.
  - (2) Area--10,000 square feet.
- (e) *Minimum yard requirements.*
  - (1) Front--20 feet.
  - (2) Side--Ten feet.
  - (3) Rear--Ten feet.

**APPENDIX C**  
**City of Jacksonville Zoning Districts**  
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- (f) *Maximum height of structures.* 35 feet; provided, however, that height maybe unlimited where all required yards are increased by one foot for each three feet of building height or fraction thereof in excess of 35 feet.
- (g) *Maximum lot coverage by all buildings.* 60 percent.

B. *Secondary zoning district.* The following secondary zoning districts may be permitted in the Public Buildings and Facilities Category as depicted on the Future Land Use Maps of the Comprehensive Plan, subject to the district regulations for same.

- (1) Agriculture (AGR); Section 656.331.
- (2) Conservation (CSV); Section 656.333.
- (3) Planned Unit Development (PUD); Section 656.340.

The aforementioned secondary zoning districts may be permitted provided that the supplemental criteria and standards for same specified in Subpart G, Part 3 are met.

(Ord. 91-59-148, § 1; Ord. 91-761-410, § 1; Ord. 91-1290-590, § 5; Ord. 92-955-674, § 6; Ord. 93-1968-1157, § 1; Ord. 94-697-395, § 1; Ord. 94-968-555, § 1; Ord. 98-1018-E, § 1)

# APPENDIX D

# STRUCTURAL CONDITIONS

APPENDIX D  
Structural Conditions  
Soutel / Moncrief / Retail Redevelopment Area



**APPENDIX D**  
**Structural Conditions**  
**Soutel / Moncrief / Retail Redevelopment Area**



APPENDIX D  
Structural Conditions  
Soutel / Moncrief / Retail Redevelopment Area



**APPENDIX D**  
**Structural Conditions**  
**Soutel / Moncrief / Retail Redevelopment Area**



# APPENDIX E PARCEL LISTING



**APPENDIX E**  
**Parcel Listing**  
**Soutel / Moncrief / Retail Redevelopment Area**

<b>Parcel #</b>	<b>RE #</b>	<b>Use</b>	<b>Condition</b>
Parcel 1	037568-0000	Magic City-Shops of Soutel	Standard/Vacant
Parcel 2	029983-0100	Commercial-Renovation in progress	Commercial Rehab/Vacant Under Const.
Parcel 3	029983-0200	Convenient Store/Barber/Hair Solon	Substandard/Deteriorating Parking lot
Parcel 4	030060-0000	Converted Gas Station into a dry cleaner	Substandard/Deteriorating Parking lot
Parcel 5	030113-0000	Bishop Lawrence Memorial Bldg Church	Standard/Occupied
Parcel 6	030116-0000	Restaurant	Substandard/Vacant
Parcel 7	030125-0000	Commercial/Neighborhood Shops	Partial Substandard/Partial Vacancy
Parcel 8	030147-0120	Car Wash	Substandard
Parcel 9	030077-0000	Church/Hair Salon/Notary	Standard
Parcel 10	030107-0000	Undeveloped-demolished gas station	Deteriorated/Vacant lot (Broken Concrete)
Parcel 11	030049-0000	Park land	Environmentally sensitive
Parcel 12	029866-0000	Park land	Environmentally sensitive
Parcel 13	029866-0000	Lonnie Miller Park	Environmentally sensitive
Parcel 14	029969-0000	Church	Standard
Parcel 15	030119-0050	City Retention Pond	Water
Parcel 16	030119-0000	Two buildings	Deteriorated/Vacant buildings
Parcel 17	030047-0000	Gas Station & Convenient Store	Standard
Parcel 18	041630-0000	Winn Dixie Supermarket	Standard
Parcel 19	041721-0000	Tax Collector's Office	Standard
Parcel 20	041721-0100	Professional Office	Standard
Parcel 21	041721-0130	Undeveloped Parcel	Standard
Parcel 22	041717-0000	Retail-Hardware	Standard
Parcel 23	041720-0000	Retail-Hardware-parking lot	Standard
Parcel 24	041718-0000	Retail-Hardware-parking lot	Standard
Parcel 25	022232-0000	Walgreens & partial undeveloped parcel	Standard/Underdeveloped
Parcel 26	038261-0000	Restaurant/Liquor Store	Substandard
Parcel 27	038259-0020	Vacant Land	Undeveloped
Parcel 28	038259-0060	Vacant Land	Undeveloped
Parcel 29	038259-0050	Commerical Shopping Center	Deteriorated-vacant buildings
Parcel 30	038259-0010	City of Jacksonville	n/a
Parcel 31	038259-0100	Holzendorf Properties	Undeveloped
Parcel 32	038259-0200	Mortuary	Standard
Parcel 33	038259-0500	Park land	Standard
Parcel 34	038259-0000	Holzendorf Properties-Shoppes of Sherwood	Partial Substandard/Partial Vacancy

# APPENDIX F

# TIF ESTIMATIONS

**APPENDIX F**  
**TIF Estimations**  
**Soutel / Moncrief / Retail Redevelopment Area**

**TIF Estimation Assuming 2% Tax Increase**

	<b>Tax</b>	<b>County Tax</b>	<b>Total TIF</b>
<b>2005</b>	\$ 157,273.39	\$ 81,703.77	
<b>2006*</b>	\$ 404,677.06	\$ 210,230.35	
<b>2007</b>	\$ 412,770.61	\$ 214,434.96	\$ 4,204.61
<b>2008</b>	\$ 421,026.02	\$ 218,723.66	\$ 4,288.70
<b>2009</b>	\$ 429,446.54	\$ 223,098.13	\$ 4,374.47
<b>2010</b>	\$ 438,035.47	\$ 227,560.10	\$ 4,461.96
<b>2011</b>	\$ 446,796.18	\$ 232,111.30	\$ 4,551.20
<b>2012</b>	\$ 455,732.10	\$ 236,753.52	\$ 4,642.23
<b>2013</b>	\$ 464,846.74	\$ 241,488.59	\$ 4,735.07
<b>2014</b>	\$ 474,143.68	\$ 246,318.37	\$ 4,829.77
<b>2015</b>	\$ 483,626.55	\$ 251,244.73	\$ 4,926.37
<b>2016</b>	\$ 493,299.08	\$ 256,269.63	\$ 5,024.89
<b>2017</b>	\$ 503,165.07	\$ 261,395.02	\$ 5,125.39
<b>2018</b>	\$ 513,228.37	\$ 266,622.92	\$ 5,227.90
<b>2019</b>	\$ 523,492.93	\$ 271,955.38	\$ 5,332.46
<b>2020</b>	\$ 533,962.79	\$ 277,394.49	\$ 5,439.11
<b>2021</b>	\$ 544,642.05	\$ 282,942.38	\$ 5,547.89
<b>2022</b>	\$ 555,534.89	\$ 288,601.22	\$ 5,658.85
<b>2023</b>	\$ 566,645.59	\$ 294,373.25	\$ 5,772.02
<b>2024</b>	\$ 577,978.50	\$ 300,260.71	\$ 5,887.46
<b>2025</b>	\$ 589,538.07	\$ 306,265.93	\$ 6,005.21
<b>2026</b>	\$ 601,328.83	\$ 312,391.25	\$ 6,125.32
<b>Total</b>	\$ 10,591,190.50	\$ 5,502,139.67	\$ 102,160.89

\*Assumes a CRA Adoption in 2006 and conversion of park land to retail

**APPENDIX F**  
**TIF Estimations**  
**Soutel / Moncrief / Retail Redevelopment Area**

**TIF Estimation Assuming 5% Tax Increase**

	<b>Tax</b>	<b>County Tax</b>	<b>Total TIF</b>
<b>2005</b>	\$ 157,273.39	\$ 81,703.77	
<b>2006</b>	\$ 416,579.33	\$ 216,413.60	
<b>2007</b>	\$ 437,408.30	\$ 227,234.28	\$ 10,820.68
<b>2008</b>	\$ 459,278.71	\$ 238,595.99	\$ 11,361.71
<b>2009</b>	\$ 482,242.65	\$ 250,525.79	\$ 11,929.80
<b>2010</b>	\$ 506,354.78	\$ 263,052.08	\$ 12,526.29
<b>2011</b>	\$ 531,672.52	\$ 276,204.69	\$ 13,152.60
<b>2012</b>	\$ 558,256.15	\$ 290,014.92	\$ 13,810.23
<b>2013</b>	\$ 586,168.95	\$ 304,515.67	\$ 14,500.75
<b>2014</b>	\$ 615,477.40	\$ 319,741.45	\$ 15,225.78
<b>2015</b>	\$ 646,251.27	\$ 335,728.52	\$ 15,987.07
<b>2016</b>	\$ 678,563.83	\$ 352,514.95	\$ 16,786.43
<b>2017</b>	\$ 712,492.03	\$ 370,140.70	\$ 17,625.75
<b>2018</b>	\$ 748,116.63	\$ 388,647.73	\$ 18,507.03
<b>2019</b>	\$ 785,522.46	\$ 408,080.12	\$ 19,432.39
<b>2020</b>	\$ 824,798.58	\$ 428,484.12	\$ 20,404.01
<b>2021</b>	\$ 866,038.51	\$ 449,908.33	\$ 21,424.21
<b>2022</b>	\$ 909,340.44	\$ 472,403.75	\$ 22,495.42
<b>2023</b>	\$ 954,807.46	\$ 496,023.93	\$ 23,620.19
<b>2024</b>	\$ 1,002,547.83	\$ 520,825.13	\$ 24,801.20
<b>2025</b>	\$ 1,052,675.22	\$ 546,866.39	\$ 26,041.26
<b>2026</b>	\$ 1,105,308.98	\$ 574,209.71	\$ 27,343.32
<b>Total</b>	\$ 15,037,175.41	\$ 7,811,835.63	\$ 357,796.11

\*Assumes a CRA Adoption in 2006 and conversion of park land to retail

**APPENDIX F**  
**TIF Estimations**  
**Soutel / Moncrief / Retail Redevelopment Area**

**TIF Estimation Assuming 7% Tax Increase**

	<b>Tax</b>	<b>County Tax</b>	<b>Total TIF</b>
<b>2005</b>	\$ 157,273.39	\$ 81,703.77	
<b>2006</b>	\$ 416,579.33	\$ 220,535.76	
<b>2007</b>	\$ 437,408.30	\$ 235,973.27	\$ 15,437.50
<b>2008</b>	\$ 459,278.71	\$ 252,491.40	\$ 16,518.13
<b>2009</b>	\$ 482,242.65	\$ 270,165.79	\$ 17,674.40
<b>2010</b>	\$ 506,354.78	\$ 289,077.40	\$ 18,911.61
<b>2011</b>	\$ 531,672.52	\$ 309,312.82	\$ 20,235.42
<b>2012</b>	\$ 558,256.15	\$ 330,964.71	\$ 21,651.90
<b>2013</b>	\$ 586,168.95	\$ 354,132.24	\$ 23,167.53
<b>2014</b>	\$ 615,477.40	\$ 378,921.50	\$ 24,789.26
<b>2015</b>	\$ 646,251.27	\$ 405,446.01	\$ 26,524.51
<b>2016</b>	\$ 678,563.83	\$ 433,827.23	\$ 28,381.22
<b>2017</b>	\$ 712,492.03	\$ 464,195.13	\$ 30,367.91
<b>2018</b>	\$ 748,116.63	\$ 496,688.79	\$ 32,493.66
<b>2019</b>	\$ 785,522.46	\$ 531,457.01	\$ 34,768.22
<b>2020</b>	\$ 824,798.58	\$ 568,659.00	\$ 37,201.99
<b>2021</b>	\$ 866,038.51	\$ 608,465.13	\$ 39,806.13
<b>2022</b>	\$ 909,340.44	\$ 651,057.69	\$ 42,592.56
<b>2023</b>	\$ 954,807.46	\$ 696,631.72	\$ 45,574.04
<b>2024</b>	\$ 1,002,547.83	\$ 745,395.94	\$ 48,764.22
<b>2025</b>	\$ 1,052,675.22	\$ 797,573.66	\$ 52,177.72
<b>2026</b>	\$ 1,105,308.98	\$ 853,403.82	\$ 55,830.16
<b>Total</b>	\$ 15,037,175.41	\$ 9,976,079.78	\$ 632,868.05

\*Assumes a CRA Adoption in 2006 and conversion of park land to retail

APPENDIX G  
BROWNFIELD PROGRAM  
ADMINISTRATION  
PROCESS

**376.80 Brownfield program administration process.--**

(1) A local government with jurisdiction over the brownfield area must notify the department of its decision to designate a brownfield area for rehabilitation for the purposes of ss. [376.77](#)-376.85. The notification must include a resolution, by the local government body, to which is attached a map adequate to clearly delineate exactly which parcels are to be included in the brownfield area or alternatively a less-detailed map accompanied by a detailed legal description of the brownfield area. If a property owner within the area proposed for designation by the local government requests in writing to have his or her property removed from the proposed designation, the local government shall grant the request. For municipalities, the governing body shall adopt the resolution in accordance with the procedures outlined in s. [166.041](#), except that the notice for the public hearings on the proposed resolution must be in the form established in s. [166.041](#)(3)(c)2. For counties, the governing body shall adopt the resolution in accordance with the procedures outlined in s. [125.66](#), except that the notice for the public hearings on the proposed resolution shall be in the form established in s. [125.66](#)(4)(b)2.

(2)(a) If a local government proposes to designate a brownfield area that is outside community redevelopment areas, enterprise zones, empowerment zones, closed military bases, or designated brownfield pilot project areas, the local government must conduct at least one public hearing in the area to be designated to provide an opportunity for public input on the size of the area, the objectives for rehabilitation, job opportunities and economic developments anticipated, neighborhood residents' considerations, and other relevant local concerns. Notice of the public hearing must be made in a newspaper of general circulation in the area and the notice must be at least 16 square inches in size, must be in ethnic newspapers or local community bulletins, must be posted in the affected area, and must be announced at a scheduled meeting of the local governing body before the actual public hearing. In determining the areas to be designated, the local government must consider:

1. Whether the brownfield area warrants economic development and has a reasonable potential for such activities;
2. Whether the proposed area to be designated represents a reasonably focused approach and is not overly large in geographic coverage;
3. Whether the area has potential to interest the private sector in participating in rehabilitation; and
4. Whether the area contains sites or parts of sites suitable for limited recreational open space, cultural, or historical preservation purposes.

(b) A local government shall designate a brownfield area under the provisions of this act provided that:

1. A person who owns or controls a potential brownfield site is requesting the designation and has agreed to rehabilitate and redevelop the brownfield site;
2. The rehabilitation and redevelopment of the proposed brownfield site will result in economic productivity of the area, along with the creation of at least 10 new permanent jobs, whether full-time or part-time, which are not associated with the implementation of the rehabilitation agreement or an agreement, between the person responsible for site rehabilitation and the local government with jurisdiction, which contains terms for the redevelopment of the brownfield site or brownfield area;

**APPENDIX G**  
**Brownfield Program Administration Process**  
**Soutel / Moncrief / Retail Redevelopment Area**

3. The redevelopment of the proposed brownfield site is consistent with the local comprehensive plan and is a permissible use under the applicable local land development regulations;

4. Notice of the proposed rehabilitation of the brownfield area has been provided to neighbors and nearby residents of the proposed area to be designated, and the person proposing the area for designation has afforded to those receiving notice the opportunity for comments and suggestions about rehabilitation. Notice pursuant to this subsection must be made in a newspaper of general circulation in the area, at least 16 square inches in size, and the notice must be posted in the affected area; and

5. The person proposing the area for designation has provided reasonable assurance that he or she has sufficient financial resources to implement and complete the rehabilitation agreement and redevelopment plan.

(c) The designation of a brownfield area and the identification of a person responsible for brownfield site rehabilitation simply entitles the identified person to negotiate a brownfield site rehabilitation agreement with the department or approved local pollution control program.

(3) When there is a person responsible for brownfield site rehabilitation, the local government must notify the department of the identity of that person. If the agency or person who will be responsible for the coordination changes during the approval process specified in subsections (4), (5), and (6), the department or the affected approved local pollution control program must notify the affected local government when the change occurs.

(4) Local governments or persons responsible for rehabilitation and redevelopment of brownfield areas must establish an advisory committee or use an existing advisory committee that has formally expressed its intent to address redevelopment of the specific brownfield area for the purpose of improving public participation and receiving public comments on rehabilitation and redevelopment of the brownfield area, future land use, local employment opportunities, community safety, and environmental justice. Such advisory committee should include residents within or adjacent to the brownfield area, businesses operating within the brownfield area, and others deemed appropriate. The person responsible for brownfield site rehabilitation must notify the advisory committee of the intent to rehabilitate and redevelop the site before executing the brownfield site rehabilitation agreement, and provide the committee with a copy of the draft plan for site rehabilitation which addresses elements required by subsection (5). This includes disclosing potential reuse of the property as well as site rehabilitation activities, if any, to be performed. The advisory committee shall review the proposed redevelopment agreement required pursuant to paragraph (5)(i) and provide comments, if appropriate, to the board of the local government with jurisdiction over the brownfield area. The advisory committee must receive a copy of the executed brownfield site rehabilitation agreement. When the person responsible for brownfield site rehabilitation submits a site assessment report or the technical document containing the proposed course of action following site assessment to the department or the local pollution control program for review, the person responsible for brownfield site rehabilitation must hold a meeting or attend a regularly scheduled meeting to inform the advisory committee of the findings and recommendations in the site assessment report or the technical document containing the proposed course of action following site assessment.

(5) The person responsible for brownfield site rehabilitation must enter into a brownfield site rehabilitation agreement with the department or an approved local pollution control program if actual contamination exists at the brownfield site. The brownfield site rehabilitation agreement must include:



**APPENDIX G**  
**Brownfield Program Administration Process**  
**Soutel / Moncrief / Retail Redevelopment Area**

- (a) A brownfield site rehabilitation schedule, including milestones for completion of site rehabilitation tasks and submittal of technical reports and rehabilitation plans as agreed upon by the parties to the agreement;
  - (b) A commitment to conduct site rehabilitation activities under the observation of professional engineers or geologists who are registered in accordance with the requirements of chapter 471 or chapter 492, respectively. Submittals provided by the person responsible for brownfield site rehabilitation must be signed and sealed by a professional engineer registered under chapter 471, or a professional geologist registered under chapter 492, certifying that the submittal and associated work comply with the law and rules of the department and those governing the profession. In addition, upon completion of the approved remedial action, the department shall require a professional engineer registered under chapter 471 or a professional geologist registered under chapter 492 to certify that the corrective action was, to the best of his or her knowledge, completed in substantial conformance with the plans and specifications approved by the department;
  - (c) A commitment to conduct site rehabilitation in accordance with an approved comprehensive quality assurance plan under department rules;
  - (d) A commitment to conduct site rehabilitation consistent with state, federal, and local laws and consistent with the brownfield site contamination cleanup criteria in s. [376.81](#), including any applicable requirements for risk-based corrective action;
  - (e) Timeframes for the department's review of technical reports and plans submitted in accordance with the agreement. The department shall make every effort to adhere to established agency goals for reasonable timeframes for review of such documents;
  - (f) A commitment to secure site access for the department or approved local pollution control program to all brownfield sites within the eligible brownfield area for activities associated with site rehabilitation;
  - (g) Other provisions that the person responsible for brownfield site rehabilitation and the department agree upon, that are consistent with ss. [376.77](#)-376.85, and that will improve or enhance the brownfield site rehabilitation process;
  - (h) A commitment to consider appropriate pollution prevention measures and to implement those that the person responsible for brownfield site rehabilitation determines are reasonable and cost-effective, taking into account the ultimate use or uses of the brownfield site. Such measures may include improved inventory or production controls and procedures for preventing loss, spills, and leaks of hazardous waste and materials, and include goals for the reduction of releases of toxic materials; and
  - (i) Certification that an agreement exists between the person responsible for brownfield site rehabilitation and the local government with jurisdiction over the brownfield area. Such agreement shall contain terms for the redevelopment of the brownfield area.
- (6) Any contractor performing site rehabilitation program tasks must demonstrate to the department that the contractor:
- (a) Meets all certification and license requirements imposed by law; and

**APPENDIX G**  
**Brownfield Program Administration Process**  
**Soutel / Moncrief / Retail Redevelopment Area**

- (b) Has obtained approval for the comprehensive quality-assurance plan prepared under department rules.
- (7) The contractor must certify to the department that the contractor:
- (a) Complies with applicable OSHA regulations.
  - (b) Maintains workers' compensation insurance for all employees as required by the Florida Workers' Compensation Law.
  - (c) Maintains comprehensive general liability and comprehensive automobile liability insurance with minimum limits of at least \$1 million per claim and \$1 million annual aggregate, sufficient to protect it from claims for damage for personal injury, including accidental death, as well as claims for property damage which may arise from performance of work under the program, designating the state as an additional insured party.
  - (d) Maintains professional liability insurance of at least \$1 million per occurrence and \$1 million annual aggregate.
  - (e) Has the capacity to perform or directly supervise the majority of the work at a site in accordance with s. [489.113](#)(9).
- (8) Any professional engineer or geologist providing professional services relating to site rehabilitation program tasks must carry professional liability insurance with a coverage limit of at least \$1 million.
- (9) During the cleanup process, if the department or local program fails to complete review of a technical document within the timeframe specified in the brownfield site rehabilitation agreement, the person responsible for brownfield site rehabilitation may proceed to the next site rehabilitation task. However, the person responsible for brownfield site rehabilitation does so at its own risk and may be required by the department or local program to complete additional work on a previous task. Exceptions to this subsection include requests for "no further action," "monitoring only proposals," and feasibility studies, which must be approved prior to implementation.
- (10) If the person responsible for brownfield site rehabilitation fails to comply with the brownfield site rehabilitation agreement, the department shall allow 90 days for the person responsible for brownfield site rehabilitation to return to compliance with the provision at issue or to negotiate a modification to the brownfield site rehabilitation agreement with the department for good cause shown. If an imminent hazard exists, the 90-day grace period shall not apply. If the project is not returned to compliance with the brownfield site rehabilitation agreement and a modification cannot be negotiated, the immunity provisions of s. [376.82](#) are revoked.
- (11) The department is specifically authorized and encouraged to enter into delegation agreements with local pollution control programs approved under s. [403.182](#) to administer the brownfield program within their jurisdictions, thereby maximizing the integration of this process with the other local development processes needed to facilitate redevelopment of a brownfield area. When determining whether a delegation pursuant to this subsection of all or part of the brownfields program to a local pollution control program is appropriate, the department shall consider the following. The local pollution control program must:

**APPENDIX G**  
**Brownfield Program Administration Process**  
**Soutel / Moncrief / Retail Redevelopment Area**

(a) Have and maintain the administrative organization, staff, and financial and other resources to effectively and efficiently implement and enforce the statutory requirements of the delegated brownfields program; and

(b) Provide for the enforcement of the requirements of the delegated brownfields program, and for notice and a right to challenge governmental action, by appropriate administrative and judicial process, which shall be specified in the delegation.

The local pollution control program shall not be delegated authority to take action on or to make decisions regarding any brownfield site on land owned by the local government. Any delegation agreement entered into pursuant to this subsection shall contain such terms and conditions necessary to ensure the effective and efficient administration and enforcement of the statutory requirements of the brownfields program as established by the act and the relevant rules and other criteria of the department.

(12) Local governments are encouraged to use the full range of economic and tax incentives available to facilitate and promote the rehabilitation of brownfield areas, to help eliminate the public health and environmental hazards, and to promote the creation of jobs and economic development in these previously run-down, blighted, and underutilized areas.

# APPENDIX H COMMUNITY CENTER

**City of Jacksonville**  
**Northwest Regional Medical Needs Community Center**

With the help of local, state and federal funds, the City of Jacksonville is working to construct a much-needed 13,900 square foot multi-purpose, medical needs community center in northwest Duval County. The building would be built as part of the new Bob Hayes Sports Complex – a 25-acre site in the Soutel/Moncrief area of Jacksonville.

A recent report completed by the Jacksonville Economic Development Commission indicates that the community surrounding this project is economically disadvantaged. The community's per capita income of \$21,500 is only 71% of the City-wide per capita income. And the unemployment rate of 7% is almost twice the City-wide rate.

The City is now entering the design phase of the project, which will consist of a one story multi-purpose community and medical needs center that will serve a large area of low to moderate-income residents.

The City has applied for federal Community Development Block Grant funds, which will be matched with local funds, for the center.

In addition, the City's Emergency Preparedness Division has applied for a federal grant in the amount of \$325,000 to receive additional dollars to harden the community facility to AARC 4496 shelter standards. This would make the facility available to use as an emergency shelter as needed. The availability of these grant dollars is unknown at this time, however other programs are available to receive retrofit funds and the Division will continue to search out these grants.

Combined, the funds applied for and made available to date should fund approximately half of the \$8 million facility. As a result, the City is requesting an appropriation from the state of Florida in the amount of \$4 million. Through this partnership, the facility would serve the community on a daily basis with medical needs services and community activities, while also being available as an emergency shelter facility.

**APPENDIX I  
BOB HAYES  
SOCCER COMPLEX**



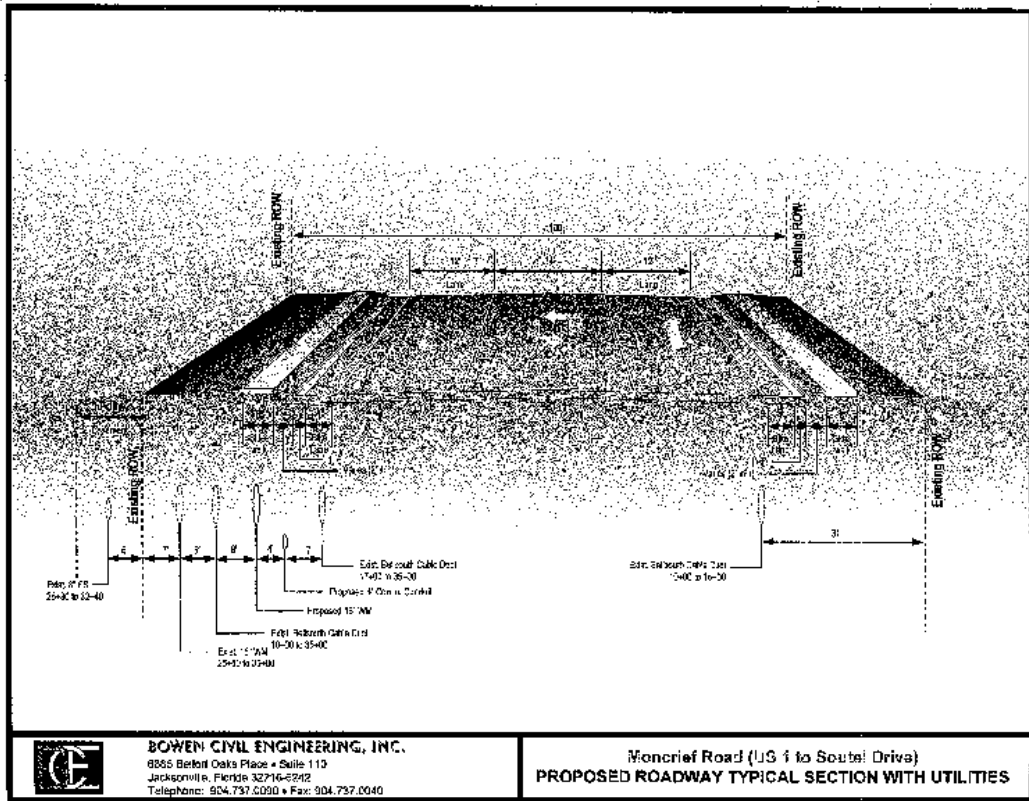
APPENDIX J  
SOUTEL DRIVE/  
MONCRIEF ROAD  
INTERSECTION  
IMPROVEMENTS  
PROJECT



**APPENDIX J**  
**Soutel Drive/Moncrief Road Intersection Improvement Project**  
**Soutel / Moncrief / Retail Redevelopment Area**



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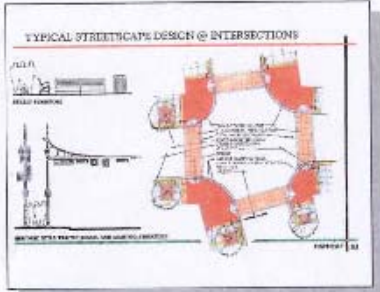


# **APPENDIX K**

# **STREETSCAPE**

**APPENDIX J**  
**Soutel Drive/Moncrief Road Intersection Improvement Project**  
**Soutel / Moncrief / Retail Redevelopment Area**

Recommended Streetscape



Recommended Signage



Example of a Business Sign



Example of a Neighborhood Entrance Sign



Suggested New Parking



Example of Commercial Uses Facing the Street

**APPENDIX J**  
**Soutel Drive/Moncrief Road Intersection Improvement Project**  
**Soutel / Moncrief / Retail Redevelopment Area**



ATTACHMENT

1 Introduced by the Council President at the request of  
2 the Mayor and Council Member Jones and amended by the Economic,  
3 Community and International Development Committee:  
4

5  
6 **RESOLUTION 2006-357-A**

7 A RESOLUTION FINDING THAT A NECESSITY EXISTS  
8 FOR THE REHABILITATION, CONSERVATION, OR  
9 REDEVELOPMENT OF BLIGHTED AREAS WITHIN THE  
10 CITY; REPEALING RESOLUTION 2006-134-A;  
11 PROVIDING AN EFFECTIVE DATE.  
12

13 **WHEREAS**, it is in the public's interest that the City of  
14 Jacksonville carry out a redevelopment project within certain lands  
15 within the City; and

16 **WHEREAS**, the City desires to exercise those powers provided in  
17 Florida Statutes Chapter 163, Part III, to identify and rectify  
18 blighted areas; and

19 **WHEREAS**, the City has previously adopted a Neighborhood Action  
20 Plan for the Lem Turner/Ribault Scenic Drive Neighborhood and is  
21 finalizing the Neighborhood Action Plan for the Sherwood  
22 Forest/Lincoln Villas areas; and

23 **WHEREAS**, the City in conjunction with the Jacksonville  
24 Regional Chamber of Commerce has conducted a Social Compact Study;  
25 and

26 **WHEREAS**, the Plans and the Study demonstrate the need for  
27 nationally-recognized commercial retail services and for enhanced  
28 opportunities for employment; and

29 **WHEREAS**, barriers exist to fulfilling such needs, including

1 the requirement that there be environmental remediation in the  
2 area; and

3 **WHEREAS**, the City wishes to take action to implement the  
4 tenets of the Plans, and redevelop the areas described below; and

5 **WHEREAS**, section 24.01 of the City's Charter designates the  
6 Jacksonville Economic Development Commission ("JEDC") as the sole  
7 community redevelopment agency for the City according to Part III,  
8 Chapter 163, Florida Statutes;

9 **WHEREAS**, a concern has been raised regarding the adequacy of  
10 the notice and public hearings for Resolution 2006-134-A which is  
11 substantially identical to this resolution; and the adoption of  
12 this resolution and its associated public hearings will cure any  
13 perceived defects associated with Resolution 2006-134-A; now  
14 therefore

15 **BE IT RESOLVED** by the Council of the City of Jacksonville:

16 **Section 1. Soutel/Moncrief Retail Redevelopment Area**  
17 **Defined.** It is hereby found and determined that (i) the area of  
18 land lying within the city limits of the City of Jacksonville  
19 identified as **attached Exhibit 1**, which shall be known as the  
20 "Soutel/Moncrief Retail Redevelopment Area", is a slum or blighted  
21 area and qualifies as an eligible project under Chapter 163, Part  
22 III, Florida Statutes, and (ii) the rehabilitation, conservation,  
23 and redevelopment of such area is necessary in the interest of the  
24 public health, safety, morals or welfare of the residents of the  
25 City of Jacksonville. This finding is based on studies formalized  
26 in the Finding of Necessity identified in the Proposed  
27 Soutel/Moncrief Retail Redevelopment Area, Community Redevelopment  
28 Area Finding of Necessity **revised on file** with the Legislative  
29 Services Division.

30 **Section 2. Priority of Redevelopment.** The first priority  
31 for the redevelopment area is to foster redevelopment of the retail

1 corridor as a means of community development.

2       **Section 3.       Community Redevelopment Agency.** It is hereby  
3 found and determined that there is a need for a community  
4 redevelopment agency to carry out the redevelopment of the  
5 Soutel/Moncrief Retail Redevelopment Area. Pursuant to section  
6 24.01 of the Charter, the JEDC has been designated as the sole  
7 community redevelopment agency for the City, and therefore the JEDC  
8 shall act as the community redevelopment agency under the  
9 provisions of Chapter 163, Part III, Florida Statutes, for the  
10 redevelopment of the Soutel/Moncrief Retail Redevelopment Area.  
11 The Chairman of the JEDC shall appoint an advisory committee to  
12 make recommendations to the JEDC regarding the implementation of  
13 the area's Community Redevelopment Plan to be adopted by separate  
14 Ordinance. The District Council Member for the area will make  
15 recommendations to the JEDC Chairman for the appointment of the  
16 advisory committee members, and the JEDC Chairman will appoint the  
17 committee members pursuant to Article 24 of the Charter.

18       **Section 4.       Resolution Repealed.** The Council hereby  
19 repeals Resolution 2006-134-A.

20       **Section 5.       Effective Date.** This resolution shall become  
21 effective upon signature by the Mayor or upon becoming effective  
22 without the Mayor's signature.

23 Form Approved:

24  
25           /s/ Margaret M. Sidman          

26 Office of General Counsel

27 Legislation Prepared By: Margaret M. Sidman

28 G:\shared\LEGIS.CC\2006\res\Lonnie Miller Park CRA Part II.doc

29  
30

## **APPENDIX 2**

### **NEW KINGS ROAD CORRIDOR AREA**

#### **EXHIBIT A. “BLIGHT STUDY”**

#### **EXHIBIT B. “FINDING OF NECESSITY RESOLUTION”**



---

# NEW KINGS ROAD CORRIDOR STUDY “FINDING OF NECESSITY REPORT”

JACKSONVILLE, FLORIDA

---

APRIL 13, 2006

PREPARED FOR:

**City of Jacksonville**

Florida Theatre Building  
128 E. Forsyth Street, Suite 700  
Jacksonville, Florida 32202



# New Kings Road Corridor Study

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- V. Inventory of Blighted Conditions**
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  - b. Deterioration of Site or Other Improvements
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### **III. Introduction**

---

The City of Jacksonville contracted with MSCW, Inc. to conduct a Finding of Necessity Analysis, often referred to as a “Blight Study,” for the New Kings Road/Soutel-Pritchard corridor. The purpose of the study is to determine if the criteria under Florida State Statute 163.335 and 163.340 that define “blight” are present in sufficient quantity to provide an objective basis for City Council adoption of a Resolution for a Finding of Necessity.

The Blight Study is a first step in preparation of a Corridor Redevelopment Plan. If the area meets the statutory criteria for a Finding of Necessity and the City Council decides to proceed with creation of an official Chapter 163 Redevelopment Area designation, then the Corridor Redevelopment Plan will be structured to meet the statutory requirements for a Community Redevelopment Plan in accordance with the Community Redevelopment Act of 1969, as amended.

Results of this Blight Study indicate that there exists significant examples of blight meeting statutory criteria to support an official Finding of Necessity within the study area. The Finding of Necessity is the platform for creation of a Redevelopment Plan that will provide strategies and mechanisms to build upon the positive improvements in the area’s interchange and road network.

### **III. The Community Redevelopment Act of 1969 (as amended)**

---

The Community Redevelopment Act of 1969, Chapter 163 Part III, Florida Statutes, authorizes local governments to establish community redevelopment agencies to improve blighted areas within their jurisdiction. The act sets forth the legal process by which local governments may establish community redevelopment agencies, and provide financing and regulatory tools to accomplish the goal of improving selected areas.

Section 163.355 F.S. requires local governments desiring the establishment of a community redevelopment agency to adopt by resolution, a finding that:

- One or more slum or blighted areas exists within its jurisdiction, and
- That the rehabilitation, conservation, or redevelopment, or a combination thereof, of these areas is necessary in the interest of public health, safety, morals, or welfare of the residents of the area.

This resolution is titled a “Finding of Necessity.” The City of Jacksonville has previously established a Community Redevelopment Agency. Thus, this study is to provide information to the City of Jacksonville’s City Council that will serve as the basis for a “finding” that the study area meets the test for establishing blight, and that there is a need for establishment of a new community redevelopment area.

### **III. Study Area Boundary and Characteristics**

---

Study Area Defined: The area to be included within the potential redevelopment area was determined after careful evaluation of a much larger area surrounding the corridor. Final determination for parcels to be included in the proposed boundary was based upon the following criteria:

- Statutory criteria pertaining to site conditions;
- Consideration of future corridor development or redevelopment potential that would either assist in elimination or reduction of blight;
- Consideration of sound planning principles for continuity of future land use based upon land use pattern and potentials, the transportation system and efficient provision of government services and facilities;
- Consideration of irregular, small or surrounding parcels that might at some time be assembled for a larger, more efficient use of land; and
- Locations providing a logical terminus for the boundaries such as roads and property lines reflecting potential accessibility and marketability for quality redevelopment or development.

The study area is generally located along and around the New Kings Road and Soutel/Pritchard Road Corridor. Commencing north of the New Kings Road and I-295 Interchange the study area runs south to the Soutel Road and New Kings Road intersection and then west to beyond the Pritchard Road and I-295 Interchange. Older, well-established residential neighborhoods lie to the east and west of New Kings Road, and some limited residential areas are closely related to and are included in the study area.

Study Figures and Maps: Figure 1 illustrates the New Kings Road Corridor Study Area boundary. Additional figures on the following pages are used to present information collected during the study that confirms the existence of blighted conditions.

Study Area Characteristics: Demographic estimates were compiled using approximate boundaries of the New Kings Road study area. According to those estimates based on the 2000 U.S. Census, approximately 1,961 people reside within the corridor study area. Of that total study area population, 69% are African-American, 29% are Caucasian, slightly less than 10% are Hispanic, and another 1.2% listed their ethnicity as “Other/Multi-Racial” (note: the total percentage is greater than 100% due respondents identifying themselves as multiple ethnic categories) The population is 47.5% male and 52.5% female with a median age of 37.1 years. The study area currently contains a population with mix of lower than average and average household incomes. Figure 2 illustrates the Median Household Income.

The current land use breakdown (based upon a parcel-by-parcel survey) within the study area is as follows:

Table 1: Existing Land Use

Land Use	Acres	Percent of Total*
<b>Undeveloped (Vacant)</b>	1,290.43	73%
<b>Developed:</b>		
Commercial	82.22	5%
Industrial	138.20	8%
Residential:		
Single Family	136.98	8%
Multi-family	0.31	--
Mobile Home/ Mobile Home Park	35.27	2%
Civic	46.59	3%

\*Percents will not add up to 100% due to additional land uses not calculated within table above.

The high percentage of vacant land within the study area along a heavily traveled corridor exemplifies the weakness of market conditions supporting new investment. Figure 3 illustrates the Existing Land Use.

Of the commercial buildings facing the corridor, 19 are occupied or operating, 15 are unoccupied or derelict; of those buildings, 6 were for posted as “for sale.” Thus, approximately 44% of the commercial structures were either unoccupied, for sale, or derelict.

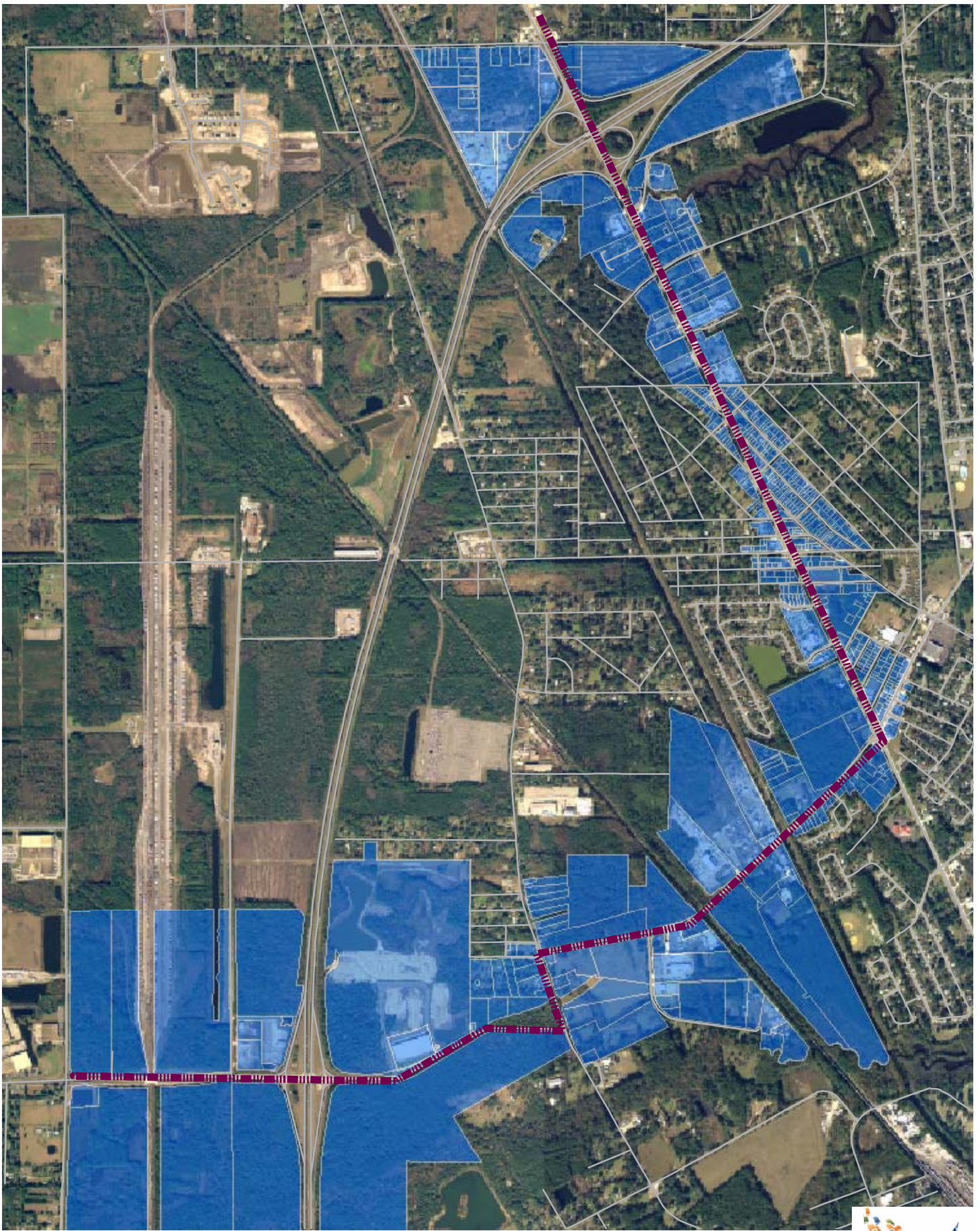


The New Kings Road segment of the corridor contains smaller residential parcel sizes reflective of its older history. Some platting occurred in the early 1940s and slow incremental development of housing and some limited commercial and industrial has occurred over time. The area was initially developed to attract suburban residents to a green field location away from the core City. The Pritchard Road/Soutel Road segment of the corridor is largely an industrial area and contains much larger parcels. The study area remains sparsely developed and underdeveloped but contains two major I-95 interchanges and a corridor that will become more economically important overtime as the City continues to develop outward to the north and northwest.

A portion of the Lincoln Villas neighborhood is located within the study area. The Lincoln Villas neighborhood is the subject of a Neighborhood Action Plan through the City of Jacksonville's Planning and Development Department. Some key statistics from the City's analysis of the neighborhood include the following: from 1990 to 2000 vacant housing increased by 21.1% in Lincoln Villas while it decreased by 5.6% for the entire City; occupied housing decreased by 6.8% while occupied housing increased within the entire City by 15.6% during 1990-2000; and in 2000, 17.8% of total households living in the neighborhood were below the poverty level compared to 12.2% for the City. Issues identified within the Neighborhood Action Plan were a lack of City potable water and sewage disposal facilities in a portion of the neighborhood, an aging population, crime, deterioration of the commercial area, a threat to the rural character of the neighborhood by new urban developments, code violations, inadequate street lighting, vacant and neglected lots, swales on both sides of the streets, and a lack of new affordable housing.

Of the 656 parcels that are vacant or contain unoccupied/dilapidated structures in the Lincoln Villas neighborhood, 573 are vacant and developable land (149.3 acres), 45 are unoccupied/vacant structures (40.9 acres), and 38 are dilapidated structures (11.2 acres). These properties were identified for potential growth. One of the recommendations identified in the report is to replat the area to correct the faulty lot layout in terms of sizes and usefulness.

In summary, the area has languished behind the rapid higher quality development of the City as a whole and has experienced a very low increase in assessed value over previous years. Recognizing this limitation and the unattractive characteristics create a poor image to major entrance corridors into downtown, the City contracted with MSCW to provide the basis for a determination of blight, if warranted. This could ultimately result in a Redevelopment Plan that provides a framework and administrative, legal and funding mechanisms to reverse the negative characteristics resulting from a long term trend.



NOTE: THE INFORMATION PROVIDED ON THIS DOCUMENT SHOULD BE TREATED AS CONCEPTUAL ONLY AND MAY BE SUBJECT TO CHANGE BASED ON MORE DETAILED SURVEY, ENVIRONMENTAL AND SPECIFIC BUILDING PRODUCT INFORMATION.

**PROJECT STUDY AREA**  
**CITY OF JACKSONVILLE**  
**NEW KINGS ROAD CORRIDOR REVITALIZATION PLAN**

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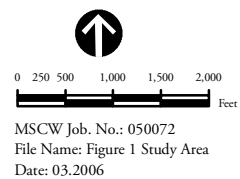
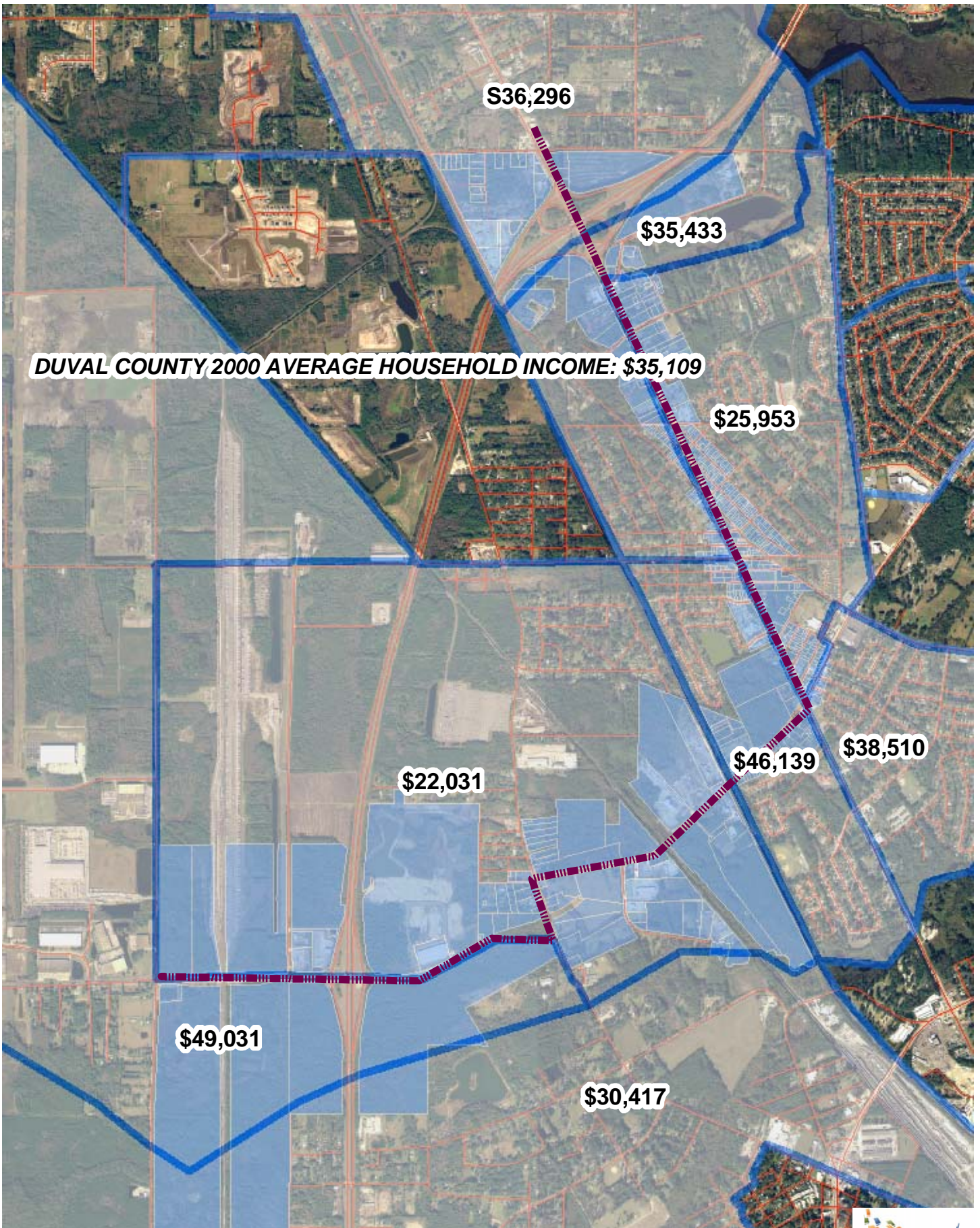


FIGURE:  
**1**





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**CENSUS 2000 MEDIAN HOUSEHOLD INCOME**  
**CITY OF JACKSONVILLE**  
**NEW KINGS ROAD CORRIDOR REVITALIZATION PLAN**

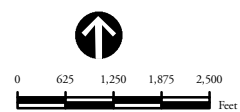


FIGURE:  
**2**

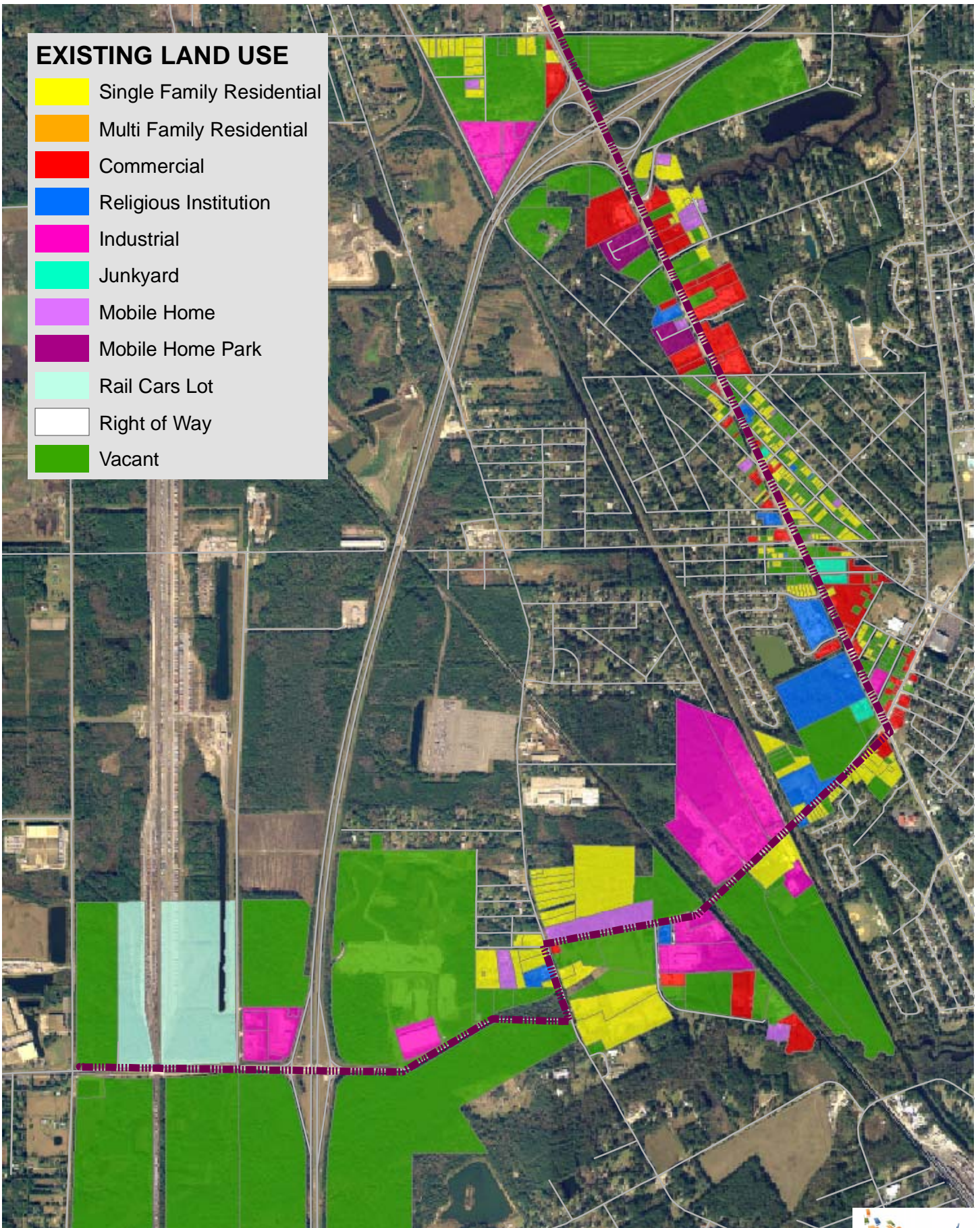
MSCW Job. No.: 050072  
 File Name: Figure 2 Census Block Groups  
 Date: 03.2006





## EXISTING LAND USE

- Single Family Residential
- Multi Family Residential
- Commercial
- Religious Institution
- Industrial
- Junkyard
- Mobile Home
- Mobile Home Park
- Rail Cars Lot
- Right of Way
- Vacant



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### EXISTING LAND USE

### CITY OF JACKSONVILLE

### NEW KINGS ROAD CORRIDOR REVITALIZATION PLAN

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0 250 500 1,000 1,500 2,000  
Feet

MSCW Job. No.: 050072  
File Name: Figure 3 Existing Land Use  
Date: 03.2006

FIGURE:

3



## **IV. Methodology**

---

The Statutory Definition of Blight: The following section of the statute provides a definition of “Blighted Area” as follows:

8) **"Blighted area"** means an area in which there are a substantial number of deteriorated, or deteriorating structures, in which conditions, as indicated by government-maintained statistics or other studies, are leading to economic distress or endanger life or property, and in which two or more of the following factors are present:

- (a) Predominance of defective or inadequate street layout, parking facilities, roadways, bridges, or public transportation facilities;
- (b) Aggregate assessed values of real property in the area for ad valorem tax purposes have failed to show any appreciable increase over the 5 years prior to the finding of such conditions;
- (c) Faulty lot layout in relation to size, adequacy, accessibility, or usefulness;
- (d) Unsanitary or unsafe conditions;
- (e) Deterioration of site or other improvements;
- (f) Inadequate and outdated building density patterns;
- (g) Falling lease rates per square foot of office, commercial, or industrial space compared to the remainder of the county or municipality;
- (h) Tax or special assessment delinquency exceeding the fair value of the land;
- (i) Residential and commercial vacancy rates higher in the area than in the remainder of the county or municipality;
- (j) Incidence of crime in the area higher than in the remainder of the county or municipality;
- (k) Fire and emergency medical service calls to the area proportionately higher than in the remainder of the county or municipality;
- (l) A greater number of violations of the Florida Building Code in the area than the number of violations recorded in the remainder of the county or municipality;
- (m) Diversity of ownership or defective or unusual conditions of title which prevent the free alienability of land within the deteriorated or hazardous area; or
- (n) Governmentally owned property with adverse environmental conditions caused by a public or private entity.

The methodology for data collection and analysis utilizes a breakdown of the criteria described in the Florida Statute to determine the existence of blight. Each statutory criterion was examined to determine the presence or absence of that condition. For those conditions existing in the study area, a more detailed analysis was undertaken. The study focused on the following conditions indicative of blight:

- **Structural conditions of buildings**
- **Deterioration of site or other improvements:**
  - **Age of structure**
  - **Site conditions**
  - **Utilities**
- **Inadequate street layout, roadways, or public transportation facilities**
- **Aggregate assessed values that have failed to show any appreciable increase**
- **Faulty lot layout combined with diversity of ownership.**

## V. Inventory of Blighted Conditions

### Structural Conditions of Buildings

The Northwest Vision Plan identifies the corridor as a Gateway to Downtown and stresses the importance of creating a more positive image for this entrance to the City. The presence of a significant number of deteriorated buildings is readily apparent to potential new homeowners, business interests, lenders, and people just traveling through the area. Not only is the area impacted, but the image and budget of the larger community is impacted. A significant number of deteriorated buildings indicate a lack of private investment in maintenance and upkeep, a serious problem in maintaining property values and a major impediment to private redevelopment and development.

The presence of deteriorated buildings impairs growth and quality development in a geographic area by having a very negative impact upon the investment decisions by individuals that would otherwise capitalize on market opportunities. Additionally, deteriorated buildings create added cost to a community for increased code enforcement programs.

For the purpose of establishing an objective determination of the condition of primary buildings within the study area, a field survey was conducted by MSCW. Each parcel was externally inspected and rated according to criteria that defined the general condition of buildings. The buildings were rated according to the following criteria:

- 4 Sound** No visible need for repair to the exterior
- 3 Standard** Building is sound but requires a number of minor repairs such as minor repainting, gutters repaired/replaced, and windows replaced
- 2 Deteriorating** Buildings show multiple signs of deterioration but appears to be structurally sound, so that rehabilitation is possible
- 1 Dilapidated** Building is in severe decline, such that rehabilitation may be impractical or not economically feasible.

Table 2: Structural Conditions of Building

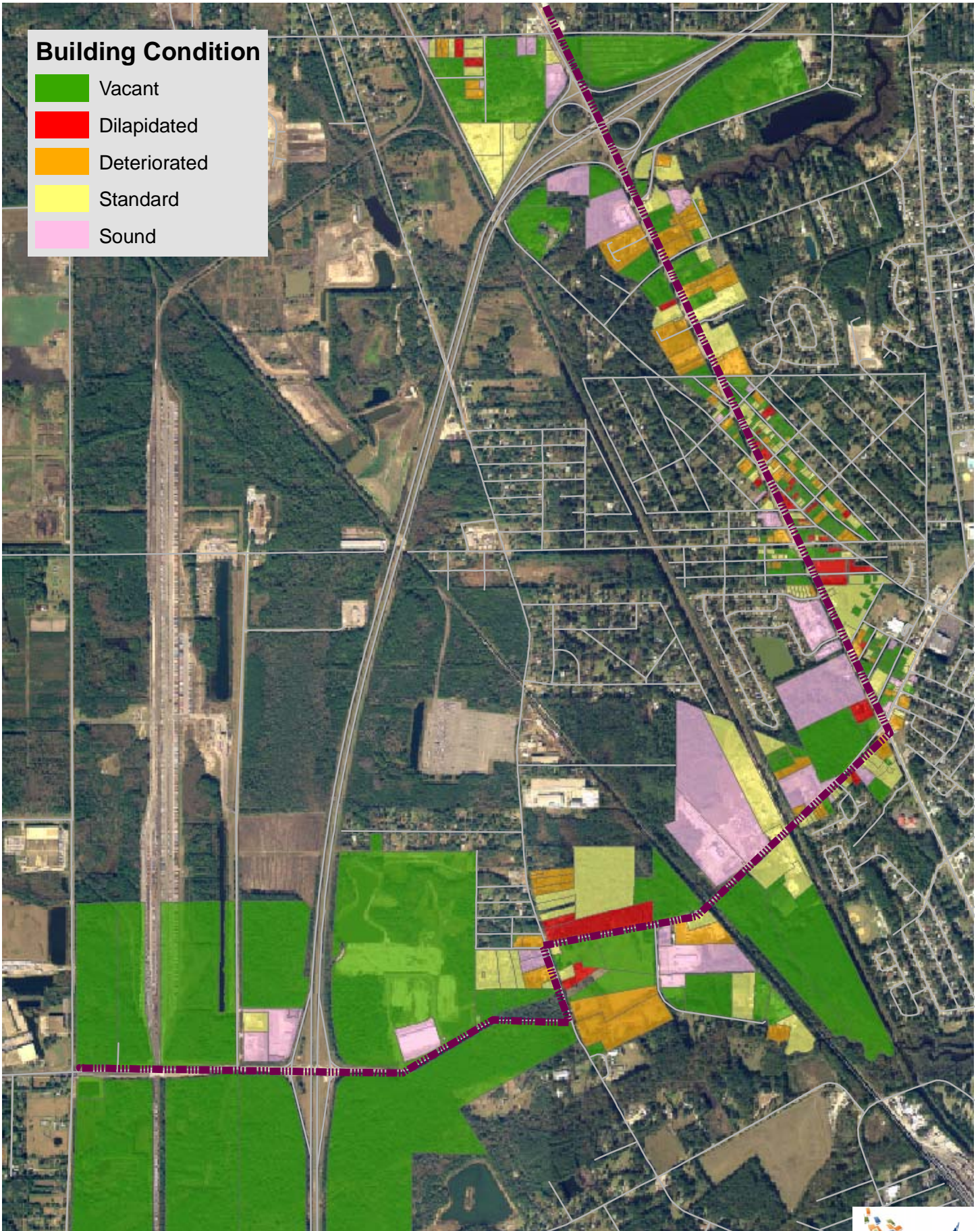
<b>Building Conditions</b>	<b>Number of Buildings</b>	<b>Percentage of Total</b>
Sound (4)	36	13%
Standard (3)	112	40%
Deteriorating (2)	95	34%
Dilapidated (1)	37	13%

Forty-seven percent (47%) of the buildings surveyed were observed to have either deteriorating or dilapidated conditions. This is a strong indicator of blight and supports the need for adoption of a Finding of Necessity. Figure 4 indicates the structural condition of buildings. Below are several photographs from a site visit conducted by MSCW in October 2005.



### Building Condition

- Vacant
- Dilapidated
- Deteriorated
- Standard
- Sound



NOTE: THE INFORMATION PROVIDED ON THIS DOCUMENT SHOULD BE TREATED AS CONCEPTUAL ONLY AND MAY BE SUBJECT TO CHANGE BASED ON MORE DETAILED SURVEY, ENVIRONMENTAL AND SPECIFIC BUILDING PRODUCT INFORMATION.

## BUILDING CONDITION CITY OF JACKSONVILLE

## NEW KINGS ROAD CORRIDOR REVITALIZATION PLAN

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Feet

MSCW Job. No.: 050072  
File Name: Figure 4 Building Condition  
Date: 03.2006

FIGURE:  
**4**



### Deterioration of Site or Other Improvements: Age of Structure

Another indicator of the building stock quality in an area can be the relative age of the buildings. Aging buildings typically require increased maintenance and repair. In addition, the interior space, exterior appearance, and functional aspects of older buildings may be obsolete for modern market demands. When market demand declines, lease revenues decline and investment in upkeep or enhancement may suffer. Age of the primary structure of a site is recorded by the Property Appraiser's Office, but is not available for all sites in the study area. The following table summarizes the percentages of buildings by age for which data was available:

Table 3: Age of Structure

Age	45 yrs or more	25-44 yrs	24 yrs or fewer
Number of sites with recorded building age	111	83	61
Percentage	43%	33%	24%

However, the age of a building in and of itself is not a blighting condition. If adequate investment and maintenance is made, older buildings can remain viable and desirable in the real estate market. Conversely, a concentration of older, poorly maintained and dilapidated buildings create many negative influences in an area: a loss of economic status, lack of interest in new development, increased crime, and decreased revenues for businesses.

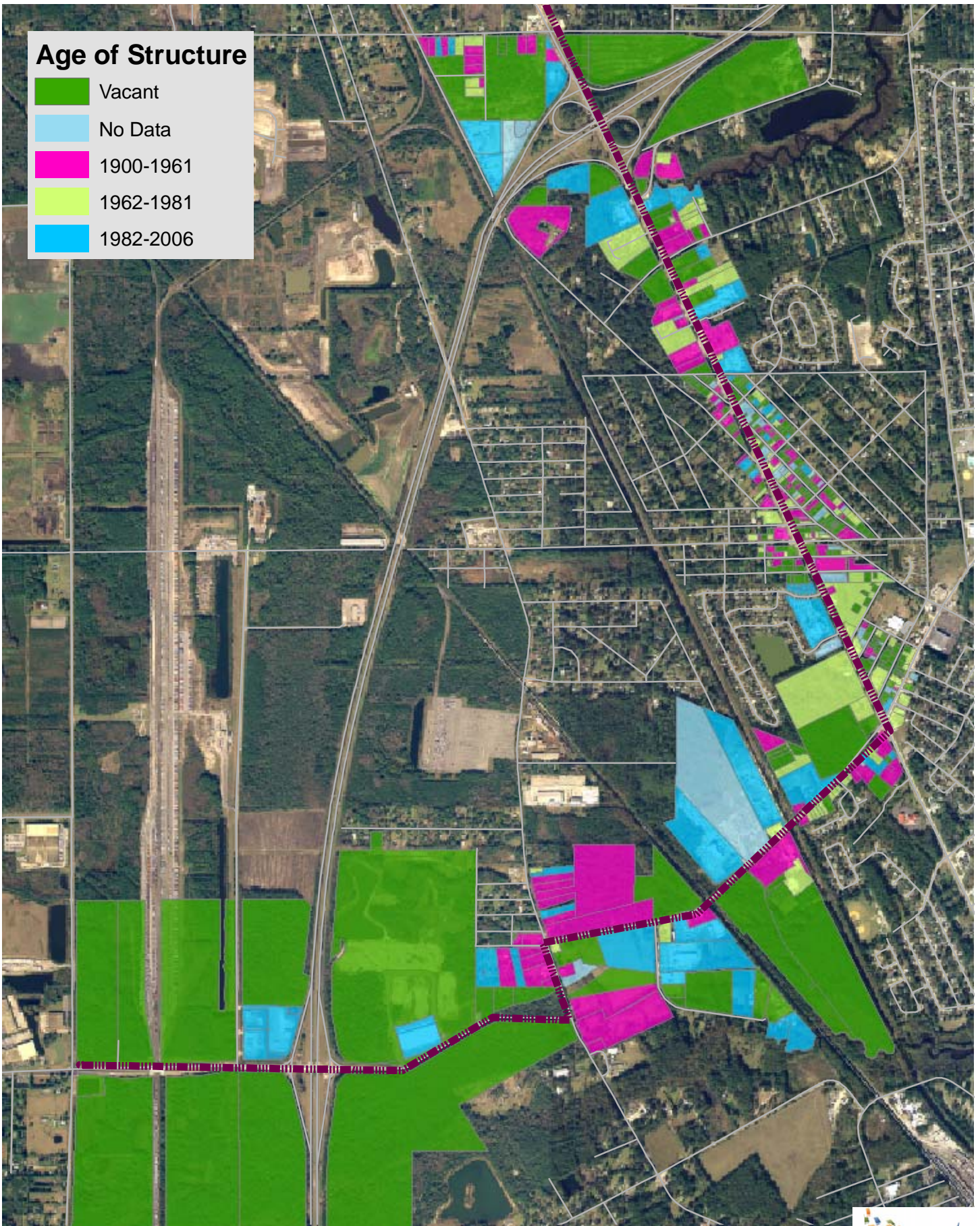
Older buildings are often more costly to maintain. Combined with the potential for lower market demand, the likelihood is increased that deterioration will occur in areas with a concentration of older buildings. The combination of older building stock with deteriorating conditions results in other negative factors in the community, including:

- Depressed property values, resulting in lower local tax revenues
- Increased fire hazard potential
- Increased code enforcement demands
- Concentration of low-income groups and marginal businesses with decreased potential for investment to reverse the blighting conditions
- An environment that is attractive to transients and conducive to criminal activity
- Low potential for rents can promote the illegal conversion of single family homes to multi-family units, resulting in a decrease in long-term homeowners
- Creation of a poor market environment, where existing businesses relocate to other, more stable areas and new businesses do not replace them, ergo the number of vacant buildings along the corridor.

In summary, the concentration of old and deteriorating buildings within the study area is a blighting condition. It discourages investment in development and re-development, ultimately decreasing the property values and tax revenues for the surrounding area. Figure 5 indicates the age of buildings within the study area.

# Age of Structure

- Vacant
- No Data
- 1900-1961
- 1962-1981
- 1982-2006



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## AGE OF STRUCTURE CITY OF JACKSONVILLE NEW KINGS ROAD CORRIDOR REVITALIZATION PLAN



0 250 500 1,000 1,500 2,000  
Feet

MSCW Job. No.: 050072  
File Name: Figure 5 Age of Structure  
Date: 03.2006

FIGURE:  
5



## Deterioration of Site or Other Improvements: Site Conditions

Similar to the Structural Condition of Building survey, a survey of Site Conditions was also conducted via field survey by MSCW. Each parcel was externally inspected and rated according to criteria that defined the general condition of the site. The sites were rated according to the following criteria:

- 4 Sound** Meets modern development requirements for paved parking, stormwater and landscaping that is well maintained and free of debris
- 3 Standard** Does not meet modern development requirements, but parking is paved and contains landscaping and is well maintained and free of significant amount of trash and debris
- 2 Deteriorating** Does not meet modern development requirements, parking is deteriorating and contains potholes, landscaping is minimal and poorly maintained and trash and debris is present
- 1 Dilapidated** Dirt Parking or crumbling asphalt, no stormwater or landscaping and site is cluttered with trash and debris.

Table 4: Site Conditions of Parcels

Site Conditions	Number of Sites*	Percentage of Total
Sound (4)	169	31%
Standard (3)	203	38%
Deteriorating (2)	97	18%
Dilapidated (1)	70	13%

\*There are 539 total parcels; however, one parcel is a right-of-way and was not surveyed to assess site condition.

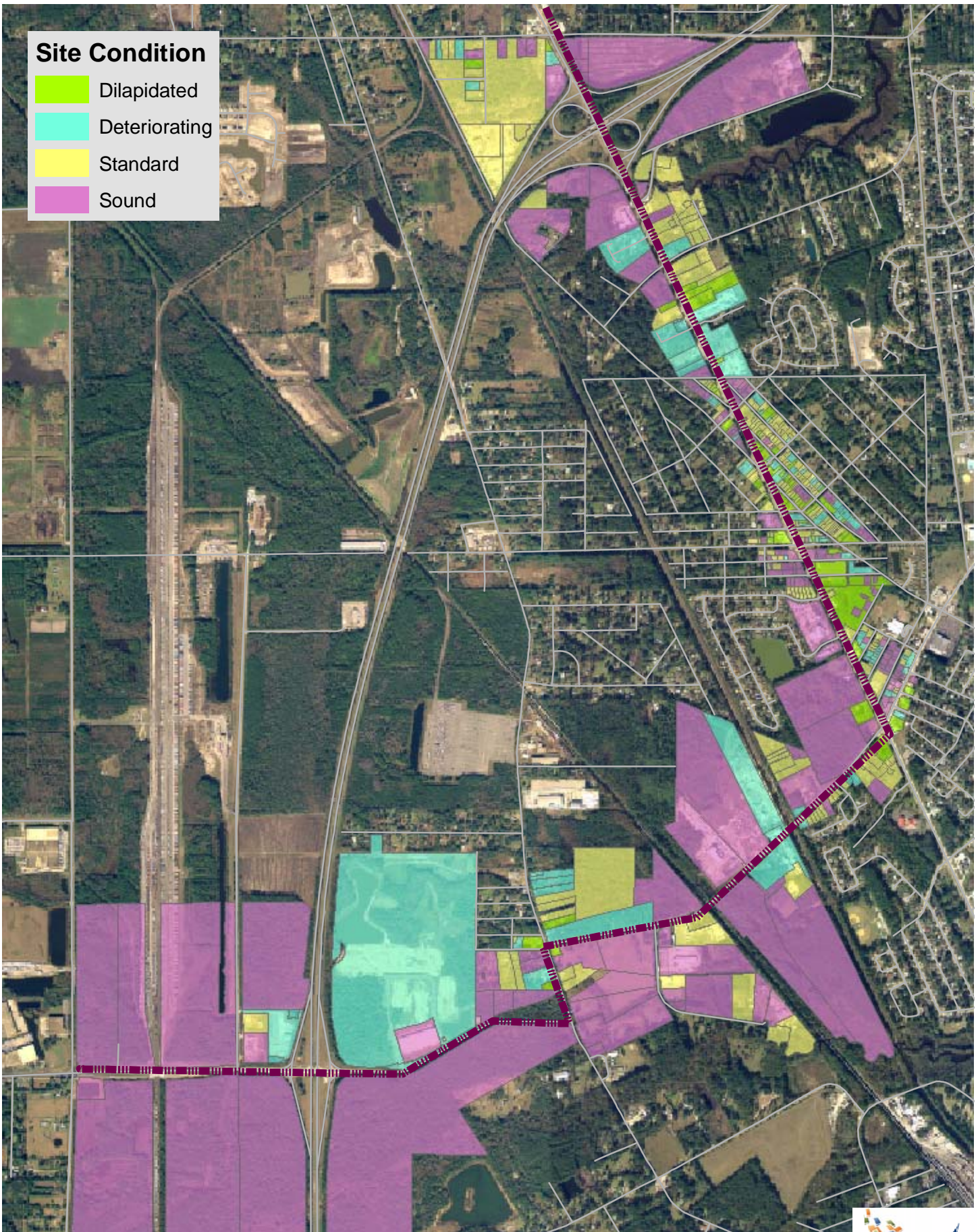
Thirty-one percent (31%) of sites were determined to be in deteriorating or dilapidated condition. Figure 6 illustrates the surveyed site conditions.





### Site Condition

- Dilapidated
- Deteriorating
- Standard
- Sound



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## SITE CONDITION CITY OF JACKSONVILLE NEW KINGS ROAD CORRIDOR REVITALIZATION PLAN

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0 250 500 1,000 1,500 2,000  
Feet

MSCW Job. No.: 050072  
File Name: Figure 6 Site Condition  
Date: 03.2006

FIGURE:  
6



## **Deterioration of Site or Other Improvements: Utilities**

### Potable Water System

Water service to the New Kings Road corridor is provided by the JEA North Grid. This system has a treatment plant capacity of 117 million gallons per day, with an average daily flow of 58.8 million gallons per day. The existing water capacity surplus is 58.9 million gallons per day. The existing water system has adequate potable capacity to serve most any type of redevelopment. The existing water system exceeds the typical commercial fire flow requirement of 1,500 GPM at 20 psi. Based on historical flow tests in the vicinity, the typical system capability is 2,000 to 3,500 GPM at 20 psi. The average hydrant spacing along New Kings Road is approximately 500 to 600 feet. The existing water lines are indicated on the “Water Lines” map located in the Appendix.

Current Deficiencies are as follows:

- There is currently no potable water and fire hydrant infrastructure along Soutel Road between Old Kings Road and New Kings Road
- The existing water system has ample fire flow capabilities with the possible exception of fire demands for heavy industrial development (greater than 2,500 GPM at 20 psi).

In summary, there exists water system deficiencies creating a condition that will need to be corrected to support current development and redevelopment.

### Sanitary Sewer System

Sewer service to the New Kings Road corridor is provided by the JEA Cedar Bay wastewater plant. This system has a capacity of 10 million gallons per day, with an average daily flow of 5.3 million gallons per day. The existing water capacity surplus is 4.7 million gallons per day. The existing sewer system has adequate sewer capacity to serve residential and typical commercial redevelopment. The existing sewer lines are indicated on the “Sewer/Pump Stations” map located in the Appendix.

Current Deficiencies are as follows:

- Sewer system upgrades including sewer force-main installation will be necessary along New Kings Road between I-295 and Soutel Road to accommodate the redevelopment to intensive commercial or heavy industrial as allowed in the Comprehensive Plan.

In summary, there exists sewer system deficiencies creating a condition that will need to be corrected to support current development and redevelopment.

### **Faulty Lot Layout Combined with Diversity of Ownership**

Faulty lot layout in relation to size, adequacy, accessibility, or usefulness is a determinate of blight, as is, diversity of ownership or defective or unusual conditions of title which prevent

the free alienability of land within the deteriorated area. A significant number of lots that are too small to accommodate modern regulatory and market requirements when combined with a diversity of ownership creates a major determinate of blight.

Of the 528 parcels located within the study area for which parcel size by square foot data is available, 15% of parcels are smaller than 5,000 square feet. Another 29% are between 5,001 and 10,000 square feet. At almost half the total parcels, this substantial number of smaller lots contributes to the conditions of blight. The range of parcel sizes by square foot is presented in the following table, and is illustrated on Figure 7.

Table 5: Parcel Size by Square Foot

<b>Area in Square Feet</b>	<b>Number of Parcels</b>	<b>Percentage</b>
0-5,000	76	15%
5,001-10,000	155	29%
10,001-25,000	129	24%
25,001-43,000	43	8%
43,000 +	125	24%
Total	528	100%

The existing development of these smaller lots occurred as long as 90 years ago. Since that time, land development regulations have changed in an effort to reflect better aesthetics, function, and protection of natural resources.

The Future Land Use Element of the City’s Comprehensive Plan provides the policy framework for application of modern land use controls. Six Future Land Use (FLU) designations exist in the study area:

- BP (Business Park);
- CGC (Community/General Commercial);
- HI (Heavy Industrial);
- LDR (Low Density Residential);
- LI (Light Industrial); and
- PBF (Public Buildings and Facilities).

CGC is the dominant land use designation along New Kings Road. Figure 8 illustrates the Future Land Use Map for the study area.

The City’s Zoning Regulation is the primary regulatory implementation mechanism controlling lot size and design standards. The study area zoning districts are as follows:

- RR (Rural Residential);
- RLD-G (Residential Low Density-G);
- RMD-A (Residential Medium Density-A);
- CCG-2 (Commercial Community/General-2);
- CO (Commercial Office);
- IL (Industrial Light);
- IH (Industrial Heavy);
- IBP-1 (Industrial Business Park-1); and
- PUD (Planned Unit Development).

RLD-G is the most prevalent zoning district along New Kings Road, comprising the residential portion of the study area. CGC-2 is the dominant zoning district for businesses fronting New Kings Road, whereas the Soutel-Pritchard portion of the study area is predominantly Industrial with Rural Residential on the northeast corner of Soutel and Old Kings Road and a PUD on the northeast corner of Pritchard and I-295. Figure 9 illustrates the Zoning Map for the study area.

Smaller land areas (parcels) are often inadequate to accommodate new development that could result in higher property values and tax revenues. Within the study area there is a deficiency in parcel/lot sizes as it relates to Future Land Use and zoning requirements, and dictates of modern market criteria. For the parcels in which data was available, the following percentages were determined:

- 42% of all residential parcels, for which parcel data is available, do not meet the minimum lot size requirements for development under their designated zoning district.
- 71% of all commercial parcels, for which parcel data is available, are less than 1 acre, which is generally the minimum required for a commercial project consisting of a one tenant building on one parcel to accommodate modern regulatory requirements and market demands for square feet and parking. Any attached building project such as a neighborhood shopping center or mixed use center envisioned in the Northwest Vision Plan will require much larger acreages.
- 25% of all industrial parcels, for which parcel data is available, are less than 15 acres, which is generally considered to be a minimum requirement for most business demand involving a one tenant building on one parcel according to national industrial surveys. Obviously, subdivision of parcels to create multiple 15 acre lots requires a much larger acreage.

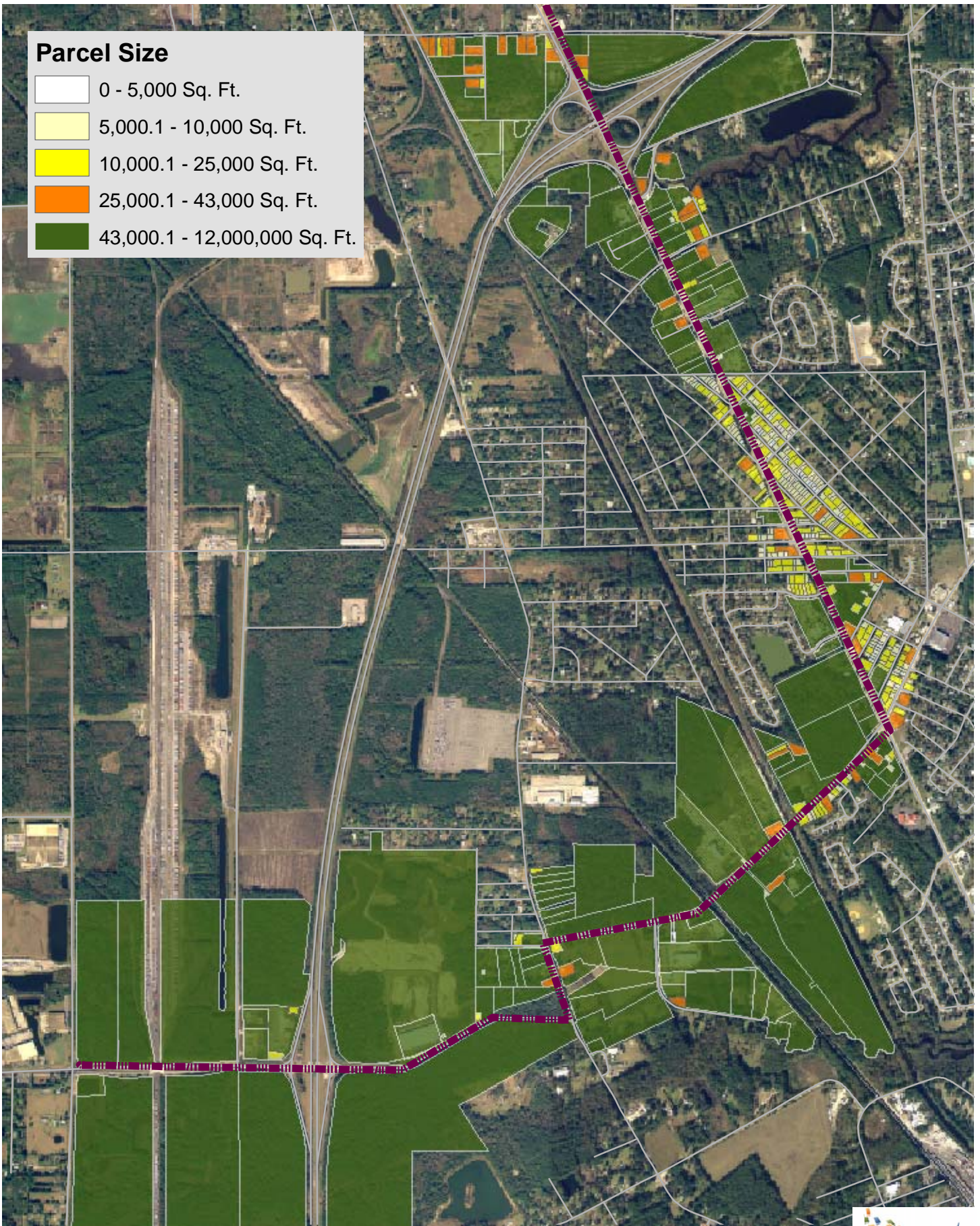
For an increased redevelopment potential, especially for sites fronting the corridor roadway, smaller lots must be assembled to create properties adequate in size to support new development and other required site improvements. Unfortunately, site assembly may be difficult or impossible, depending on the number (diversity) of individual property owners. Within the study area, there is a substantial diversity of property owners. Thus, the task of assembling a site of even one acre may require negotiations with multiple property owners of contiguous lots who are willing to sell at costs that are financially feasible for new development.

In summary, the combination of a high percentage of parcels smaller than required by modern standards and market conditions, along with a large diversity of ownership that is also present within the study area, is a blighting influence. Additionally, the inadequately sized parcels fronting the corridor roadways creates far too many curb cuts and resulting turning movements, which dramatically limits accessibility as an area builds out. Finally, the small commercial parcels/lots lack any degree of usefulness unless assembled.

These conditions impede sound growth in the study area by creating a combination of physical, legal, and economic constraints, which can make substantial redevelopment either improbable or at the very least difficult. Further, these impediments to growth perpetuate the negative physical and social conditions, prevent economic revitalization, and suppress surrounding property values and tax revenues.

### Parcel Size

- 0 - 5,000 Sq. Ft.
- 5,000.1 - 10,000 Sq. Ft.
- 10,000.1 - 25,000 Sq. Ft.
- 25,000.1 - 43,000 Sq. Ft.
- 43,000.1 - 12,000,000 Sq. Ft.



NOTE: THE INFORMATION PROVIDED ON THIS DOCUMENT SHOULD BE TREATED AS CONCEPTUAL ONLY AND MAY BE SUBJECT TO CHANGE BASED ON MORE DETAILED SURVEY, ENVIRONMENTAL AND SPECIFIC BUILDING PRODUCT INFORMATION.

## PARCEL SIZE CITY OF JACKSONVILLE NEW KINGS ROAD CORRIDOR REVITALIZATION PLAN



0 250 500 1,000 1,500 2,000  
Feet

MSCW Job. No.: 050072  
File Name: Figure 7 Parcel Size  
Date: 03.2006

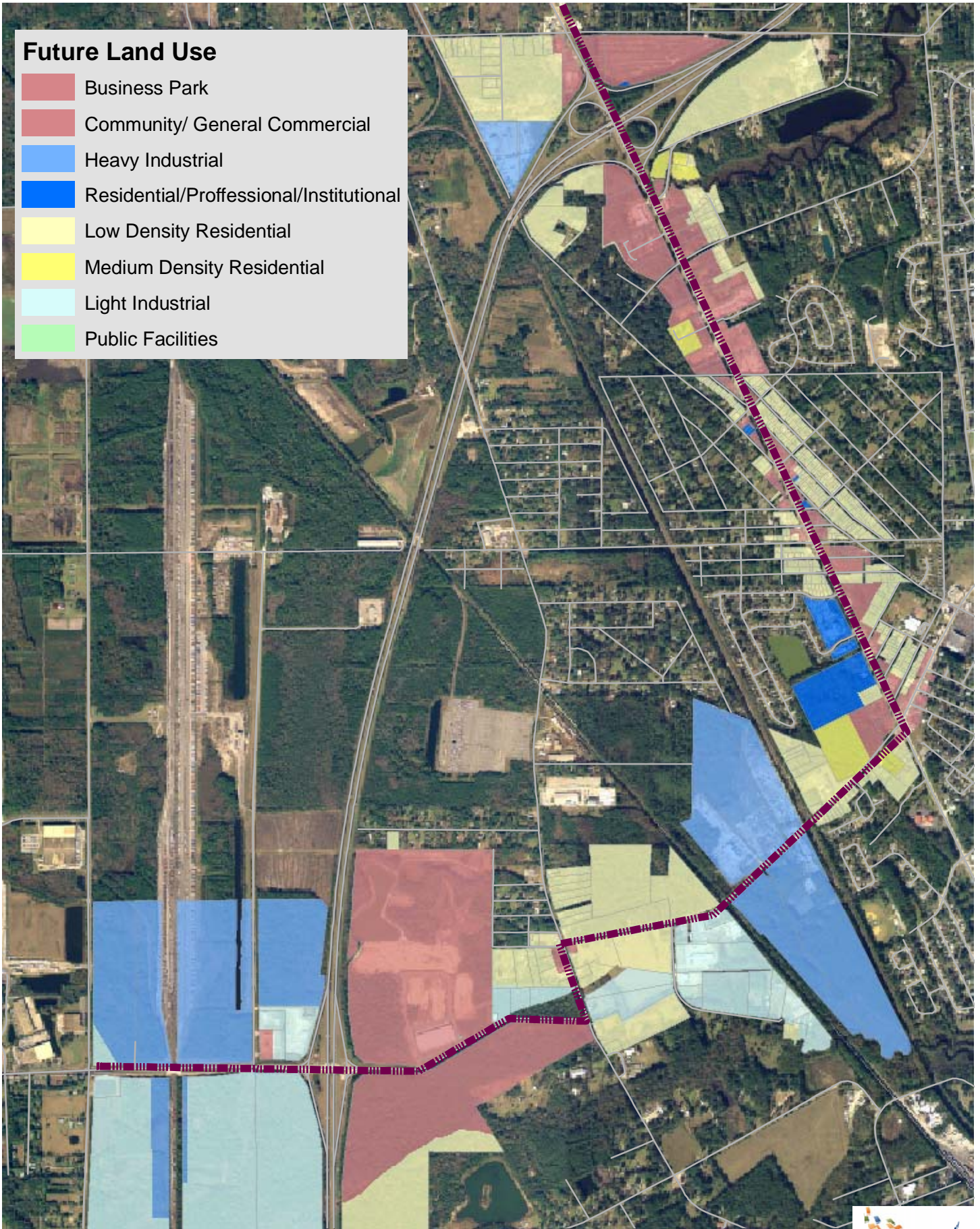
FIGURE:

7



## Future Land Use

- Business Park
- Community/ General Commercial
- Heavy Industrial
- Residential/Professional/Institutional
- Low Density Residential
- Medium Density Residential
- Light Industrial
- Public Facilities



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**FUTURE LAND USE**  
**CITY OF JACKSONVILLE**  
**NEW KINGS ROAD CORRIDOR REVITALIZATION PLAN**

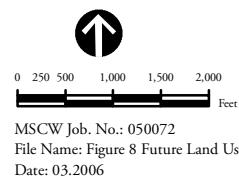
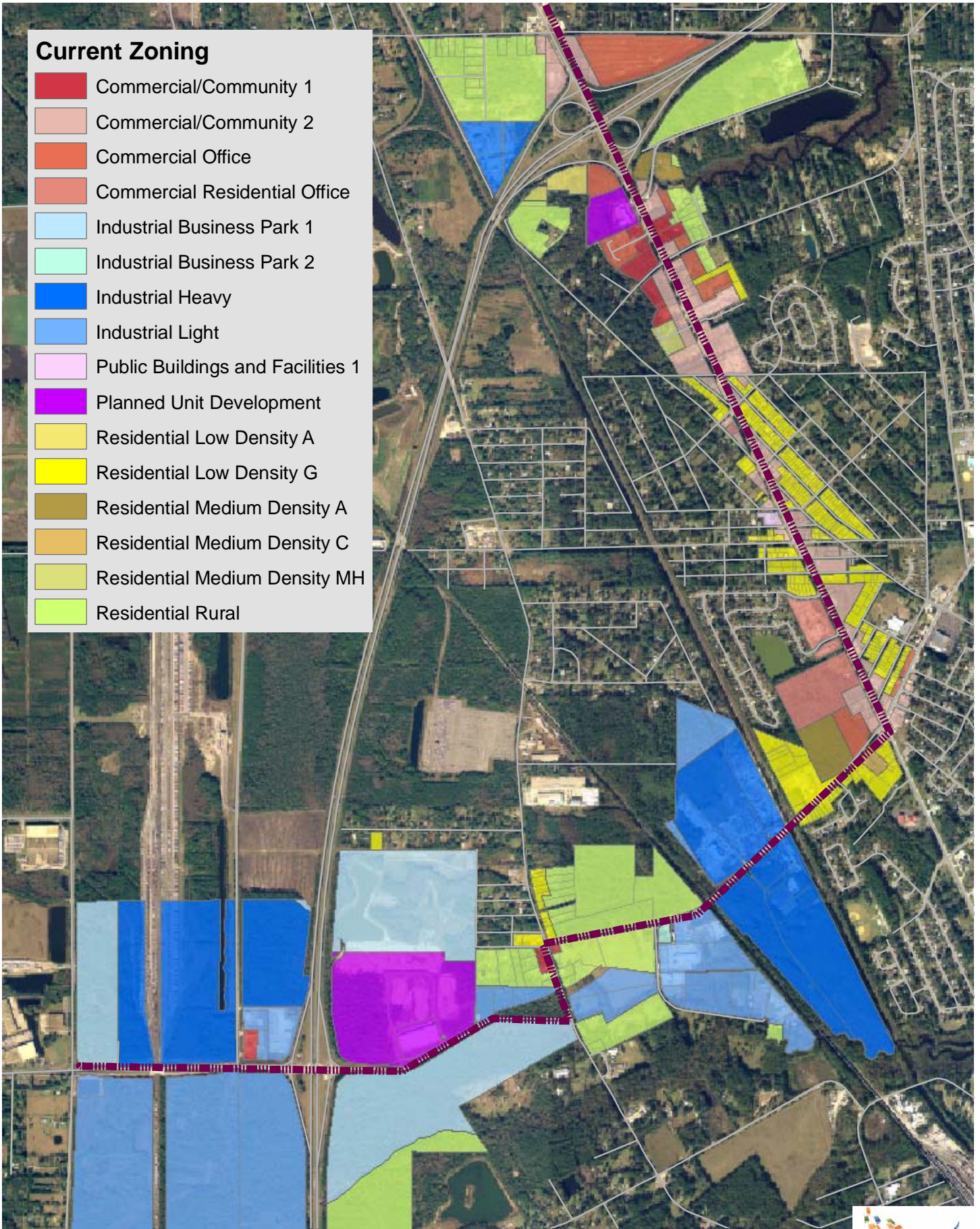


FIGURE:  
**8**



## Current Zoning

- Commercial/Community 1
- Commercial/Community 2
- Commercial Office
- Commercial Residential Office
- Industrial Business Park 1
- Industrial Business Park 2
- Industrial Heavy
- Industrial Light
- Public Buildings and Facilities 1
- Planned Unit Development
- Residential Low Density A
- Residential Low Density G
- Residential Medium Density A
- Residential Medium Density C
- Residential Medium Density MH
- Residential Rural



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**CURRENT ZONING**  
**CITY OF JACKSONVILLE**  
**NEW KINGS ROAD CORRIDOR REVITALIZATION PLAN**

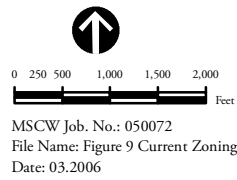


FIGURE:  
**9**





**Inadequate street layout, roadways, or public transportation facilities.**

A clear indication of blight is the predominance of defective or inadequate street layout, parking facilities, roadways, bridges, or public transportation facilities.

In terms of street layout, the New Kings Road segment has recently been improved and is adequate for vehicular traffic, but lacks continuous sidewalks and modern bus shelters. The Soutel Road and Pritchard Road segment has a major deficiency that consists of a large off set that requires construction of a major new linkage in order to adequately connect the two roads creating an appropriate alignment and one continuous corridor.

Some local subdivision layouts were platted prior to constructing New Kings Road and were bisected creating extremely small lots fronting the corridor and an irregular disjointed street layout. Thus, the street layout for the area is generally outdated, defective and inadequate and will become even more inadequate if the area is successfully redeveloped. Sidewalks and modern bus facilities are integral public transportation components of a healthy neighborhood and this area’s lack there of is an indicator of blight.

In terms of roadway condition, nearly all of the neighborhood collector and local streets were constructed well before the establishment of modern subdivision design standards for right-of-way width, pavement width, sidewalks, storm water retention, curve radius, street lighting, handicapped accessibility, and curb and gutter, and offer strong evidence of blight. The lack of roads meeting modern standards for safety and design is another indicator of blight.



**Aggregate Assessed Values That Have Failed To Show Any Appreciable Increase**

If aggregate assessed values of real property in the area for ad valorem tax purposes have failed to show any appreciable increase over the 5 years prior to the finding of such conditions, it is a significant indication that the area is blighted.

The New Kings Road corridor lies within Area 3 of the Jacksonville Neighborhood Market Drill Down Profile. Area 3 contains the lowest median home values of \$51,300 as compared to \$51,900 for all of the Drill Down neighborhoods.

We are currently waiting on data from the Property Appraiser's Office to determine the percentage increase in real property for the study area in relationship to the City as a whole.

Figure 10 illustrates the Assessed Value of Parcels Map for the study area.

### **Incidence of Crime**

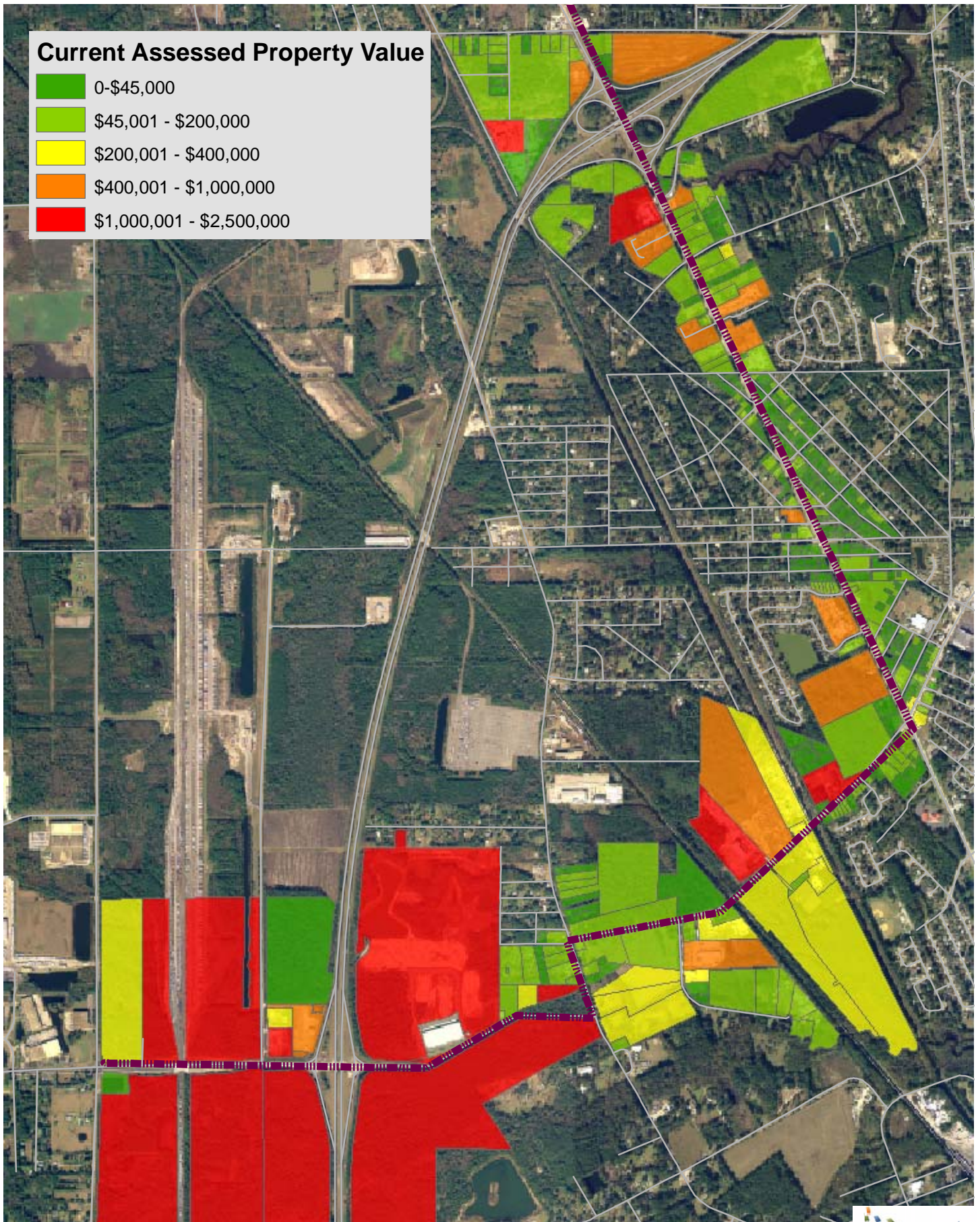
For the time period of January 2004 to October 2005, there were 668 total crime incidents committed in the New Kings Road Corridor study area. This equates to a 33% greater per capita incidence of crime as compared to the City of Jacksonville.

The crime statistics are compared below:

<b>Type of Crime</b>	<b>New Kings Road vs. Duval County</b>
Homicide	114%
Robbery	-1%
Burglary (Residential)	-71%
Theft	-44%
All Other Offenses	73%
<b>Total Crime Incidents</b>	<b>33%</b>

# Current Assessed Property Value

- 0-\$45,000
- \$45,001 - \$200,000
- \$200,001 - \$400,000
- \$400,001 - \$1,000,000
- \$1,000,001 - \$2,500,000



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## CURRENT ASSESSED PROPERTY VALUE CITY OF JACKSONVILLE NEW KINGS ROAD CORRIDOR REVITALIZATION PLAN



0 250 500 1,000 1,500 2,000 Feet

FIGURE:  
10

MSCW Job. No.: 050072  
File Name: Figure 10 Assessed Value  
Date: 03.2006



## **VI Conclusions**

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The New Kings Road Corridor meets the criteria for blight set forth in Chapter 163, Part III Florida Statutes, the Community Redevelopment Act of 1969, as amended. Specifically, the blight study documents the existence of a number of statutory criteria that substantiate a City Council adoption of a Resolution for a Finding of Necessity of a blighted area. There is a substantial number of deteriorated or deteriorating structures, in which conditions, as indicated by government-maintained statistics or other studies, are leading to economic distress or endanger life or property, and in which two or more of the determinants of blight factors are present as follows:

### **Inadequate street layout, roadways, or public transportation facilities**

- Lack of sidewalks and adequate bus shelters
- Pritchard Road and Soutel Road off set alignment
- Roadways that fail to meet standards
- Old platted roads that have been bisected

### **Faulty lot layout combined with diversity of ownership**

- Substandard lot/parcel size
- Inadequate properties in relation to current development codes and market dictates
- Diversity of parcel ownership

### **Deterioration of site or other improvements**

- Deteriorated Building Conditions
- Deteriorated Site Conditions
- Age of Structures
- Utilities

### **Lack of Growth in Tax Base**

- Vacant buildings
- Undeveloped and underdeveloped properties
- Loss of market value

### **Deterrents to Sound Future Growth and Development**

- Economic disuse
- Property assemblage problems
- Visual blight as deterrent to investment

According to Chapter 163, Part III, F. S., the next legal step is adoption of the Finding of Necessity Resolution by the City Council.

Legal Case History: Based upon ample precedence from key court test cases concerning findings of necessity, a finding of blight will be upheld if there is “competent substantial evidence” presented on the record upon which the governing body’s decision was based.

The Florida Supreme court has defined the term “competent substantial evidence” as follows:

“Substantial evidence has been described as such evidence as will establish a substantial basis of fact from which the fact at issue can reasonably be inferred. We have state it to be such relevant evidence, as a reasonable mind would accept as adequate to support a conclusion... We are of the view... that the evidence relied upon to sustain the ultimate finding and should be sufficiently relevant and material that a reasonable mind would accept it as adequate to the conclusion reached. To this extent the “substantial” should also be “competent.”

# **APPENDIX**

## **Parcel Analysis of Study Area**

Parcel ID Number	Acres	Parcel Area (Sq. Ft.)	Building Area (Sq. Ft.)	Zoning	Minimum Lot Size Required per Zoning District	Lot Meets Min. Lot Size Requirement	Street Number	Street Name	Street Type	Street Direction	Property Use Code	Land Use Code	Land Value	Building Value	Year Built	Building Condition	Site Condition	Existing Land Use
002784 0000	5.25	228501.31	1296	RLD-A	43560	N	8741	MONCRIEF-DINSMORE	RD		200	9922	43800	30100	1998	4	4	Vacant
002785 0000	0.84	36695.30	7182	CCG-2	0	Y	9048	U S 1		N	2792	2720	33600	28530	1945	3	2	SFR
002787 0000	3.74	163010.00	17920	CCG-2	0	Y	9012	NEW KINGS	RD		1692	1600	203643	707593	2002	4	4	Commercial
002790 0000	0.34	14795.76	4240	CCG-2	0	Y	9032	NEW KINGS	RD		2792	2720	13053	95597	1985	3	1	Commercial
002792 0000	0.44	19163.37	0	CCG-2	0	Y					1000	1000	4063	0	0000	0	4	Vacant
002793 0000	0.92	39971.66	0	CCG-2	0	Y					1000	1000	38000	0	0000	0	4	Vacant
002793 0010	2.50	109074.08	0	CCG-2	0	Y		U S 1		N	1000	1000	81213	0	0000	0	4	Vacant
002796 0000	0.12	5379.70	0	RR	43560	N		JOLENE	DR		9900	9950	183550	0	0000	0	3	Vacant
002796 0000	2.31	100786.40	0	RR	43560	Y		JOLENE	DR		9900	9950	183550	0	0000	0	3	Vacant
002796 0000	36.21	1577275.09	0	RR	43560	Y		JOLENE	DR		9900	9950	183550	0	0000	0	3	Vacant
002796 0010	1.05	45884.69	2708	RR	43560	Y	6624	TROUT RIVER	BV		100	101	14880	57200	1976	3	3	SFR
002796 1000	1.10	47918.36	0	IH	0	Y		JOLENE	DR		4000	4001	34250	0	0000	3	3	Industrial
002796 1000	4.61	201006.05	0	IH	0	Y		JOLENE	DR		4000	4001	34250	0	0000	3	3	Industrial
002797 0000	0.97	42380.26	1764	RR	43560	N	6518	TROUT RIVER	DR		100	101	14400	38100	1933	4	4	SFR
002798 0000	0.16	7093.19	0	RR	43560	N					9600	9400	170	0	0000	2	2	SFR
002799 0000	0.49	21310.94	720	RR	43560	N	6738	TROUT RIVER	BV		200	201	5760	11000	1987	2	2	SFR
002800 0000	0.65	28251.30	1568	RR	43560	N	6756	TROUT RIVER	BV		100	101	7934	46800	1949	4	4	SFR
002801 0000	0.24	10650.24	1467	RR	43560	N	6704	JOLENE	DR		100	101	5681	34200	1940	4	3	SFR
002802 0000	0.73	31949.32	3260	RR	43560	N	6714	JOLENE	DR		200	201	10054	23000	1974	2	2	Mobile Home
002804 0000	0.98	42825.23	1844	RR	43560	N	6540	TROUT RIVER	BV		100	9921	7650	51900	1948	4	4	SFR
002805 0000	0.68	29419.85	1224	RR	43560	N	6748	TROUT RIVER	BV		100	101	9600	52700	1946	3	3	SFR
002806 0000	1.00	43665.03	1752	RR	43560	Y	6720	TROUT RIVER	BV		100	9921	7650	54100	1971	1	2	SFR
002807 0000	1.02	44532.54	1872	RR	43560	Y	8936	JOHNSON	RD		200	9922	7650	14600	1973	2	3	SFR
002808 0000	0.98	42677.04	0	RR	43560	N	6606	TROUT RIVER	BV		0	9920	7650	0	0000	0	4	Vacant
002809 0000	0.97	42275.61	2590	RR	43560	N	9028	JOHNSON	RD		100	201	13536	70000	1942	3	4	SFR
002810 0000	0.72	31481.42	2292	RR	43560	N	6728	TROUT RIVER	BV		100	9921	11550	51200	1947	2	3	SFR
002810 0010	0.66	28682.41	1336	RR	43560	N	6726	TROUT RIVER	BV		200	201	9525	16000	1988	3	3	SFR
002811 0000	0.98	42599.56	1061	RR	43560	N	9008	JOHNSON	RD		100	101	13632	20700	1951	3	3	SFR
002813 0000	1.02	44389.27	1580	RR	43560	Y	9024	JOHNSON	RD		100	9921	7725	40700	1959	1	1	SFR
002983 0000	10.64	463646.38	3302	RR	43560	Y	8563	MONCRIEF-DINSMORE	RD		5600	9602	96570	59200	1930	0	4	Vacant
003388 0000	196.52	8560200.97	0	IL	0	Y		PRITCHARD	RD		5500	5510	2067858	0	0000	0	4	Vacant
003388 1000	3.15	137055.82	0	IL	0	Y		PRITCHARD	RD		8600	9607	536	0	0000	0	4	Vacant
003426 0000	0.25	10994.18	142873	PUD	N/A		6595	PRITCHARD	RD		4899	4800	960498	3822793	2001	4	4	Industrial
003426 0000	7.09	308954.45	142873	PUD	N/A		6595	PRITCHARD	RD		4899	4800	960498	3822793	2001	4	4	Industrial
003430 0000	38.01	1655523.75	0	IH	0	Y					9600	9604	13401	0	0000	0	4	Vacant
003431 0000	0.45	19433.33	8181	IL	0	Y	4371	SPORTSMANS CLUB	RD		2792	2720	389861	185822	1986	4	2	Industrial
003431 0000	0.47	20293.78	8181	IL	0	Y	4371	SPORTSMANS CLUB	RD		2792	2720	389861	185822	1986	4	2	Industrial
003431 0000	7.43	323841.04	8181	IL	0	Y	4371	SPORTSMANS CLUB	RD		2792	2720	389861	185822	1986	4	2	Industrial
003432 0010	2.74	119217.34	6561	IL	0	Y	4129	SPORTSMANS CLUB	RD		2792	2720	149738	144316	1988	3	3	Industrial
003432 0010	0.02	1069.61	6561	IL	0	Y	4129	SPORTSMANS CLUB	RD		2792	2720	149738	144316	1988	3	3	Industrial
003432 0100	4.13	180016.75	16941	CCG-1	0	Y	4129	SPORTSMANS CLUB	RD		1492	1410	394429	743158	2004	4	4	Industrial
003435 0000	56.34	2454170.71	0	IBP-1	6000	Y		PRITCHARD	RD		5600	9999	1813020	0	0000	0	4	Vacant
003442 0000	37.10	1616272.97	0	IBP-1	6000	Y		IMESON	RD		9100	9102	249795	0	0000	0	4	Vacant
003450 0400	6.00	261199.68	0	RR	43560	Y		SOUTEL	DR		6100	6107	150000	0	0000	0	4	Vacant
003458 0000	11.06	481918.21	5390	RR	43560	Y	6703	OLD KINGS	RD		100	9941	47619	45000	1926	1	2	Mobile Home
003459 0000	8.76	381735.37	1604	IL	0	Y	6505	OLD KINGS	RD		6100	9931	324970	16500	1917	2	4	SFR
003459 0100	11.86	516795.69	1152	RR	43560	Y	6620	PICKETT	DR		200	9942	91725	0	1983	0	4	Vacant
003459 0110	0.51	22256.16	3696	CCG-1	0	Y	6655	OLD KINGS	RD		1492	1410	45000	87853	1979	3	1	Commercial
003459 0210	0.98	42555.53	2448	RR	43560	N	6607	OLD KINGS	RD		100	101	10648	50400	1926	3	3	SFR
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003459 0400	7.20	313638.98	0	IL	0	Y		OLD KINGS	RD		6100	6107	266911	0	0000	0	4	Vacant
003460 0000	1.80	78510.44	2068	CCG-1	0	Y	6646	OLD KINGS	RD		1292	1200	17828	44000	1934	4	4	SFR
003461 0000	132.96	5791844.16	0	IBP-1	6000	Y		PRITCHARD	RD		5600	9999	1500910	0	0000	0	4	Vacant
003462 0000	1.79	77906.36	1094	RR	43560	Y	6606	OSCEOLA	ST		200	9922	17900	0	1990	3	3	SFR
003462 0010	3.11	135301.31	3177	RR	43560	N	6604	OSCEOLA	ST		100	9922	31100	44700	1959	3	3	Mobile Home
003463 0050	0.07	3237.98	2964	RR	43560	N	6664	OSCEOLA	ST		100	101	20226	76800	1988	3	4	SFR
003463 0050	4.80	208906.40	2964	RR	43560	Y	6664	OSCEOLA	ST		100	101	20226	76800	1988	3	4	SFR
003464 0000	2.88	125514.30	7491	CCG-1	0	Y	6608	OLD KINGS	RD		2591	1101	62580	32419	1961	2	2	Church
003464 0010	0.65	28210.92	2134	RR	43560	N	6572	KINGS	RD		100	101	12727	66000	1956	3	1	SFR
003465 0000	1.13	49198.39	2078	CCG-1	0	Y	6658	OLD KINGS	RD		1292	1200	21635	64200	1949	4	4	SFR
003468 0000	0.25	10749.12	0	IL	0	Y	6555	PRITCHARD	RD		9100	9100	47046	0	0000	0	4	Vacant



Parcel ID Number	Acres	Parcel Area (Sq. Ft.)	Building Area (Sq. Ft.)	Zoning	Minimum Lot Size Required per Zoning District	Lot Meets Min. Lot Size Requirement	Street Number	Street Name	Street Type	Street Direction	Property Use Code	Land Use Code	Land Value	Building Value	Year Built	Building Condition	Site Condition	Existing Land Use
003468 0200	3.05	132955.65	0	IL	0	Y		PRITCHARD	RD		4000	4000	132858	0	0000	0	4	Vacant
003468 0500	2.98	129693.71	0	IL	0	Y		PRITCHARD	RD		4000	4000	1149984	0	0000	0	4	Vacant
003468 0550	2.29	99696.75	0	IL	0	Y		PRITCHARD	RD		4000	4000	297951	0	0000	0	4	Vacant
003471 0000	1.43	62254.53	2765	RR	43560	Y	6727	OLD KINGS	RD		100	101	10623	18300	1944	1	1	SFR
003472 0000	1.46	63421.58	2311				6777	OLD KINGS	RD		200	201	10756	23600	1986	3	3	SFR
003473 0000	1.47	64123.35	1846	RLD-G	6000	Y	6827	OLD KINGS	RD		100	101	13834	24400	1958	3	3	SFR
003474 0000	23.38	1018561.74	1870	RLD-G	6000	Y	6829	KINGS	RD		100	101	72572	31000	1946	3	3	SFR
003475 0000	1.73	75515.54	2884	RLD-G	6000	Y	6859	KINGS	RD		100	101	12198	38000	1944	2	2	SFR
003476 0000	1.79	77902.04	2283	RLD-G	6000	Y	6903	OLD KINGS	RD		100	101	16449	35800	1944	2	2	SFR
003477 0000	0.31	13487.47	1288	RLD-G	6000	Y	6921	KINGS	RD		200	201	8140	11200	1980	2	2	SFR
003478 0000	1.52	66288.14	1056	RLD-G	6000	Y	6909	KINGS	RD		200	101	9827	9600	1985	2	2	SFR
003528 0000	0.00	0.00	0	PUD	N/A			PRITCHARD	RD		4000	4000	2420788	0	0000	0	2	Vacant
003528 0000	0.00	0.00	0	PUD	N/A			PRITCHARD	RD		4000	4000	2420788	0	0000	0	2	Vacant
003528 0000	0.00	0.02	0	PUD	N/A			PRITCHARD	RD		4000	4000	2420788	0	0000	0	2	Vacant
003528 0000	0.00	0.00	0	PUD	N/A			PRITCHARD	RD		4000	4000	2420788	0	0000	0	2	Vacant
003528 0000	0.00	0.00	0	PUD	N/A			PRITCHARD	RD		4000	4000	2420788	0	0000	0	2	Vacant
003528 0000	0.00	0.00	0	PUD	N/A			PRITCHARD	RD		4000	4000	2420788	0	0000	0	2	Vacant
003528 0000	0.00	0.00	0	PUD	N/A			PRITCHARD	RD		4000	4000	2420788	0	0000	0	2	Vacant
003528 0000	0.00	0.00	0	PUD	N/A			PRITCHARD	RD		4000	4000	2420788	0	0000	0	2	Vacant
003528 0000	0.00	0.00	0	PUD	N/A			PRITCHARD	RD		4000	4000	2420788	0	0000	0	2	Vacant
003528 0000	0.00	0.00	0	PUD	N/A			PRITCHARD	RD		4000	4000	2420788	0	0000	0	2	Vacant
003528 0000	0.00	0.00	0	PUD	N/A			PRITCHARD	RD		4000	4000	2420788	0	0000	0	2	Vacant
003528 0000	0.00	0.00	0	PUD	N/A			PRITCHARD	RD		4000	4000	2420788	0	0000	0	2	Vacant
003528 0000	0.00	0.00	0	PUD	N/A			PRITCHARD	RD		4000	4000	2420788	0	0000	0	2	Vacant
003528 0000	0.00	0.00	0	PUD	N/A			PRITCHARD	RD		4000	4000	2420788	0	0000	0	2	Vacant
003528 0000	2.18	95082.01	0	PUD	N/A			PRITCHARD	RD		4000	4000	2420788	0	0000	0	2	Vacant
003528 0000	150.42	6552095.52	0	PUD				PRITCHARD	RD		4000	4000	2420788	0	0000	0	2	Vacant
003562 0000	0.11	5000.07	0	RLD-G	6000	N					0	1	4123	0	0000	0	3	Vacant
003563 0000	0.57	24999.84	1512	RLD-G	6000	Y	6542	ZORA	ST		100	103	5628	30900	1955	2	2	SFR
003564 0000	1.49	65101.50	2652	RLD-G	6000	Y	6708	OLD KINGS	RD		100	101	22270	64600	1932	2	1	SFR
003596 0000	26.54	1156134.37	0	IH	0	Y		SPORTSMANS CLUB	RD		9800	9801	1808415	0	0000	0	4	Rail Car Lot
003596 0000	64.79	2822040.39	0	IH	0	Y		SPORTSMANS CLUB	RD		9800	9801	1808415	0	0000	0	4	Vacant
003946 0000	4.41	192203.40	672	IH	0	Y	8825	MONCRIEF-DINSMORE	RD		4900	4900	34739	0	2000	3	3	Industrial
003946 0100	6.18	269364.72	52002	IH	0	Y	8831	MONCRIEF-DINSMORE	RD		4100	4100	67137	1257158	1988	3	3	Industrial
004520 0000	264.95	11541210.01	0	IL	0	Y		IMESON	RD		5500	5510	2165185	0	0000	0	4	Vacant
021354 0100	0.25	10875.09	0	RLD-G	6000	Y		DEKLE	AV		0	1	9974	0	0000	0	4	Vacant
021355 0000	0.29	12617.65	0	RLD-G	6000	Y					0	1	8056	0	0000	0	4	Vacant
021356 0000	0.34	14789.23	0					DEKLE	AV		0	1	8749	0	0000	0	4	Vacant
021358 0000	0.56	24436.18	2400	RLD-G	6000	Y	7840	MATTOX	AV		100	101	15593	36500	1979	3	2	SFR
021359 0000	0.28	12084.88	2584				7844	MATTOX	AV		100	101	7796	41900	1963	1	2	SFR
021360 0000	0.14	6034.33	0								0	9600	508	0	0000	2	2	SFR
021361 0000	0.14	6197.49	1306				7854	MATTOX	AV		100	101	3713	21200	1952	2	2	SFR
021362 0000	0.28	12083.53	3031				7864	MATTOX	AV		100	101	7796	74400	1962	2	2	Mobile Home
021363 0000	0.15	6484.36	0				7876	MATTOX	AV		0	1	3935	0	0000	0	4	Vacant
021364 0000	0.13	5768.33	0								0	1	3935	0	0000	0	4	Vacant
021365 0000	0.28	12172.85	0								0	1	7796	0	0000	0	4	Vacant
021366 0000	0.22	9448.00	898				5768	REDPOLL	AV		100	101	7796	17200	1957	2	1	SFR
021367 0000	0.43	18940.59	1040				7867	DEKLE	AV		100	101	15592	17800	1951	3	1	SFR
021368 0000	0.22	9613.40	780				7863	DEKLE	AV		200	201	7088	6900	1974	0	2	Vacant
021369 0000	0.09	4134.87	776				7851	DEKLE	AV		100	101	3935	17700	1960	2	1	SFR
021370 0000	0.11	4686.59	0					DEKLE	AV		0	1	3935	0	0000	0	2	Vacant
021371 0000	0.27	11599.26	0								0	1	6452	0	0000	0	4	Vacant
021372 0000	0.65	28498.67	1683				7815	MONCRIEF	EX		100	101	13686	16400	1958	2	1	SFR
021373 0000	0.36	15774.91	850				7813	MONCRIEF	EX		100	101	7780	15600	1959	2	1	SFR
021374 0000	0.39	17205.12	0								0	1	7780	0	0000	0	3	Vacant
021376 0000	0.14	6120.81	0								0	1	4099	0	0000	0	3	Vacant
021377 0000	0.11	4655.80	0								0	1	10341	0	0000	0	3	Vacant
021377 0000	0.12	5087.17	0								0	1	10341	0	0000	0	3	Vacant
021377 0010	0.15	6414.15	0								0	1	4175	0	0000	0	3	Vacant
021379 0000	0.26	11222.54	0								0	1	5123	0	0000	0	3	Vacant
021380 0000	0.11	4662.86	0								0	1	4175	0	0000	0	3	Vacant
021381 0000	0.15	6484.11	0					DEKLE	AV		0	1	4099	0	0000	0	3	Vacant
021382 0000	0.13	5790.08	0								0	1	4277	0	0000	0	3	Vacant

Parcel ID Number	Acres	Parcel Area (Sq. Ft.)	Building Area (Sq. Ft.)	Zoning	Minimum Lot Size Required per Zoning District	Lot Meets Min. Lot Size Requirement	Street Number	Street Name	Street Type	Street Direction	Property Use Code	Land Use Code	Land Value	Building Value	Year Built	Building Condition	Site Condition	Existing Land Use
021382 0010	0.15	6720.53	1004				7870	DEKLE	AV		100	101	4010	25000	1965	0	3	Vacant
021383 0000	0.59	25657.62	0	RLD-G	6000	Y		DEKLE	AV		0	1	10247	0	0000	2	2	SFR
021384 0000	0.13	5481.49	0					MONCRIEF-DINSMORE	RD	W	0	1	4277	0	0000	0	3	Vacant
021384 0010	0.17	7533.77	0				7886	DEKLE	AV		0	1	4356	0	0000	0	3	Vacant
021385 0000	0.28	12161.70	0					MONCRIEF-DINSMORE	RD		0	1	4658	0	0000	0	3	Vacant
021386 0000	0.35	15266.60	768				7875	MONCRIEF-DINSMORE	RD		100	101	5346	17000	1940	1	1	SFR
021387 0000	0.54	23722.54	1632	CCG-2	0	Y	7845	NEW KINGS	RD		1292	1200	25300	26100	1955	3	2	SFR
021392 0000	1.45	63375.27	10411	PBF-1	0	Y	7866	NEW KINGS	RD		8600	8600	30318	362197	1979	4	4	Religious Institution
021415 0000	0.11	4911.33	0	CCG-2	0	Y					1000	1000	5228	0	0000	0	3	Vacant
021416 0000	0.12	5042.64	0	CCG-2	0	Y					1000	1000	4574	0	0000	0	3	Vacant
021417 0000	0.11	4683.22	0	CCG-2	0	Y		MONCRIEF	EX		1000	1000	4073	0	0000	1	1	SFR
021418 0000	0.14	5996.03	682	CCG-2	0	Y	7856	MONCRIEF-DINSMORE	RD		1292	1200	6109	4300	1948	2	2	SFR
021418 0010	0.16	7075.93	3440	CCG-2	0	Y	7854	MONCRIEF-DINSMORE	RD		3300	3300	10470	34950	1975	2	2	SFR
021528 0010	0.04	1817.35	0	RLD-G	6000	N		FLICKER	AV		0	2	1000	0	0000	0	3	Vacant
021529 0000	0.23	10154.45	0					MONCRIEF	EX		0	1	7095	0	0000	0	3	Vacant
021530 0000	0.46	20121.75	0	CCG-2	0	Y					1000	1000	15854	0	0000	0	3	Vacant
021533 0000	0.96	41943.01	6895	CCG-2	0	Y	7806	U S 1		N	1192	1100	84000	52938	1957	2	1	Commercial
021534 0000	0.12	5151.28	0	RLD-G	6000	N		FLICKER	AV		0	1	4125	0	0000	0	3	Vacant
021535 0000	0.12	5132.35	0								0	1	3713	0	0000	0	3	Vacant
021549 0000	0.32	14059.20	6504				5913	MONCRIEF	RD	W	3900	3900	15938	131405	1966	2	3	Commercial
021551 0000	0.10	4383.41	812				5911	MONCRIEF	RD	W	100	101	3300	17600	1957	3	3	SFR
021552 0000	0.19	8390.18	0	RLD-G	6000	Y					0	1	6600	0	0000	0	4	Vacant
021553 0000	0.22	9646.93	1558				5889	MONCRIEF	RD		100	101	11550	39700	1977	2	1	SFR
021554 0000	0.04	1601.40	0	CCG-2	0	Y					1000	1000	1961	0	0000	0	3	Vacant
021555 0000	0.11	4583.66	0	CCG-2	0	Y					1000	1000	3150	0	0000	0	3	Vacant
021556 0000	0.11	4689.38	768	CCG-2	0	Y	5749	MONCRIEF	RD	W	1293	1200	3570	6100	1978	0	4	Vacant
021557 0000	0.11	4741.33	1204	CCG-2	0	Y	5753	MONCRIEF	RD	W	1292	1200	3927	35300	1974	1	2	Mobile Home
021558 0000	0.11	4768.91	794				5745	MONCRIEF	RD	W	100	101	3713	7500	1948	1	1	SFR
021559 0000	0.12	5289.42	736				5733	MONCRIEF	RD	W	100	101	3713	16800	1963	3	2	SFR
021560 0000	0.11	4656.55	0								0	1	2980	0	0000	2	2	SFR
021561 0000	0.24	10667.34	2504	RLD-G	6000	Y	5719	MONCRIEF	RD	W	893	810	4929	29600	1962	1	2	SFR
021778 0000	0.13	5529.82	996	RLD-G	6000	N	5753	REDPOLL	AV		100	101	3713	24400	1950	2	1	SFR
021778 0020	0.13	5781.95	0	RLD-G	6000	N		MATTOX	AV		0	1	3713	0	0000	3	2	SFR
021779 0000	0.13	5800.85	1140				7914	MATTOX	AV		100	101	3713	19900	1956	2	3	SFR
021780 0000	0.13	5744.03	1176				7918	MATTOX	AV		100	101	3713	20700	1959	2	2	SFR
021781 0000	0.14	5909.31	0	RLD-G	6000	N					7000	7002	3375	0	0000	0	2	Vacant
021782 0000	0.14	5982.25	0	RLD-G	6000	N					7100	7100	1750	12350	0000	3	3	Religious Institution
021783 0000	0.26	11457.09	2567				7936	MATTOX	AV		100	101	7425	65500	1985	3	3	SFR
021785 0000	0.14	6027.90	640				7942	MATTOX	AV		100	101	3713	19900	1985	3	2	SFR
021786 0000	0.13	5583.64	0								0	1	3713	0	0000	0	3	Vacant
021787 0000	0.27	11737.66	0					MATTOX	AV		0	1	7425	0	0000	0	3	Vacant
021788 0000	0.24	10604.00	831				7949	DEKLE	AV		100	101	4744	19800	1940	2	1	SFR
021789 0000	0.27	11735.62	1924				7972	MATTOX	AV		100	101	7425	30000	1940	3	2	SFR
021790 0000	0.13	5476.73	1738				7978	MATTOX	AV		891	810	3375	42200	1960	3	3	SFR
021791 0000	0.15	6360.81	0								0	1	3713	0	0000	0	3	Vacant
021792 0000	0.21	9351.17	0								0	1	7425	0	0000	0	3	Vacant
021793 0000	0.11	4759.65	0								0	1	3713	0	0000	0	3	Vacant
021794 0000	0.11	4666.17	0								0	1	3713	0	0000	0	3	Vacant
021795 0000	0.22	9507.37	0				7951	DEKLE	AV		7100	7100	5000	32900	0000	2	2	Religious Institution
021796 0000	0.10	4502.98	0								0	1	3713	0	0000	0	3	Vacant
021796 0010	0.11	4864.94	0								0	1	3713	0	0000	0	3	Vacant
021797 0000	0.11	4950.17	0								0	1	3713	0	0000	0	3	Vacant
021798 0000	0.10	4428.60	0								0	1	3713	0	0000	0	3	Vacant
021799 0000	0.12	5054.17	0					DEKLE	AV		0	1	3713	0	0000	0	3	Vacant
021800 0000	0.11	4889.02	0				7923	DEKLE	AV		0	1	3713	0	0000	0	3	Vacant
021801 0000	0.11	4699.89	0				7917	DEKLE	AV		0	1	3713	0	0000	0	3	Vacant
021801 0320	0.11	4804.65	0								0	1	3713	0	0000	0	3	Vacant
021802 0000	0.11	4982.11	0								0	1	3713	0	0000	0	3	Vacant
021803 0000	0.10	4433.60	0								0	1	3713	0	0000	0	3	Vacant
021804 0000	0.25	10910.41	0								0	1	4950	0	0000	0	1	Vacant
021805 0000	0.14	5934.24	0								0	1	3713	0	0000	0	4	Vacant

Parcel ID Number	Acres	Parcel Area (Sq. Ft.)	Building Area (Sq. Ft.)	Zoning	Minimum Lot Size Required per Zoning District	Lot Meets Min. Lot Size Requirement	Street Number	Street Name	Street Type	Street Direction	Property Use Code	Land Use Code	Land Value	Building Value	Year Built	Building Condition	Site Condition	Existing Land Use
021806 0000	0.14	5999.32	0								0	1	3713	0	0000	0	4	Vacant
021807 0000	0.14	5959.90	0				7919	MONCRIEF	EX		0	1	3713	0	0000	0	4	Vacant
021808 0000	0.14	6234.91	0					DEKLE	AV		0	1	3713	0	0000	0	4	Vacant
021809 0000	0.14	6128.55	875				7928	DEKLE	AV		100	101	3713	15700	1940	0	4	Vacant
021809 0000	0.14	6128.55	875				7928	DEKLE	AV		100	101	3713	15700	1940	0	4	Vacant
021810 0000	0.13	5741.72	0				7936	DEKLE	AV		7100	7100	1750	1550	0000	0	4	Vacant
021811 0000	0.07	3159.62	0								0	1	1856	0	0000	1	1	SFR
021812 0000	0.07	3044.12	0								0	1	1856	0	0000	0	3	Vacant
021813 0000	0.14	6148.14	1056				7942	DEKLE	AV		100	101	3713	22600	1951	1	1	SFR
021814 0000	0.11	4801.66	836	RLD-G	6000	N	7946	DEKLE	AV		100	101	2970	17700	1955	2	1	SFR
021814 0005	0.17	7227.64	1241	RLD-G	6000	Y	7952	DEKLE	AV		100	101	4455	32800	1968	2	1	SFR
021814 0010	0.14	5991.42	0	RLD-G	6000	N					0	1	3713	0	0000	2	2	SFR
021815 0000	0.14	6186.35	0					DEKLE	AV		0	1	7425	0	0000	2	1	SFR
021816 0000	0.52	22625.39	0	CCG-2	0	Y					2891	2800	25046	0	0000	2	2	SFR
021817 0000	0.45	19715.53	12030	CCG-2	0	Y	7977	NEW KINGS	RD		1692	1600	25213	101944	1975	3	1	Commercial
021818 0000	0.28	12307.05	2373	CCG-2	0	Y	5786	MC LEOD	AV		1291	1200	10500	66300	1961	2	2	SFR
021819 0050	0.02	804.12	0	CCG-2	0	Y		MC LEOD	AV		1000	1000	786	0	0000	0	2	Vacant
021823 0000	0.15	6502.22	1018	RLD-G	6000	Y	7961	MONCRIEF	EX		100	101	4528	19600	1964	0	3	Vacant
021824 0000	0.13	5535.63	0								0	1	4166	0	0000	0	3	Vacant
021825 0000	0.13	5860.48	0	RLD-G	6000	N					0	1	3713	0	0000	0	3	Vacant
021826 0000	0.13	5800.73	1100	RLD-G	6000	N	7943	MONCRIEF	EX		100	101	3713	22000	1946	1	1	SFR
021826 0010	0.13	5615.75	0	RLD-G	6000	N					0	1	3713	0	0000	0	3	Vacant
021827 0000	0.14	5911.80	0	RLD-G	6000	N		MONCRIEF-DINSMORE	RD		0	1	3713	0	0000	0	3	Vacant
021828 0000	0.14	5954.28	0	RLD-G	6000	N					0	1	3713	0	0000	1	1	SFR
021828 0031	0.13	5743.61	1855				7919	MONCRIEF	EX		100	101	3713	53700	1976	0	3	Vacant
021829 0000	0.14	5920.32	1684	CCG-2	0	Y	7915	MONCRIEF-DINSMORE	RD		1292	1200	6641	35800	1958	3	1	SFR
021830 0000	0.13	5796.40	1006				7911	MONCRIEF-DINSMORE	RD		100	101	4166	17000	1958	3	1	SFR
021831 0000	0.13	5853.40	720	CCG-2	0	Y	5779	MC LEOD	AV		1292	1200	3553	21400	1981	1	2	SFR
021832 0000	0.44	19156.03	0	CCG-2	0	Y					1000	1000	11813	0	0000	1	1	Junkyard
021833 0000	0.13	5598.41	0	CCG-2	0	Y					1000	1000	4750	0	0000	0	1	Vacant
021834 0000	0.23	9941.04	1336	CCG-2	0	Y	8033	NEW KINGS	RD		1292	1200	10278	17700	1973	1	1	Commercial
021835 0000	0.15	6355.41	0	CCG-2	0	Y		DEKLE	AV		1000	1000	3574	0	0000	0	4	Vacant
021837 0000	0.16	7084.10	0	CRO	5000-7000	Y					1000	1000	7375	0	0000	0	4	Vacant
021838 0000	0.07	3155.01	0	CCG-2	0	Y					1000	1000	2890	0	0000	0	4	Vacant
021839 0000	0.47	20569.27	5936	CCG-2	0	Y	8142	NEW KINGS	RD		3900	3900	40200	41639	1953	2	1	Commercial
021840 0000	0.15	6750.50	0	CRO	5000-7000	Y					1000	1000	5063	0	0000	0	4	Vacant
021841 0000	0.16	6815.58	0	CCG-2	0	Y		MONCRIEF-DINSMORE	RD		0	1200	6311	0	0000	0	4	Vacant
021842 0000	0.15	6347.80	2255	CCG-2	0	Y	8104	NEW KINGS	RD		1292	1200	7970	32600	1940	2	2	Commercial
021843 0000	0.13	5519.80	0	CCG-2	0	Y					1000	1000	17710	0	0000	0	4	Vacant
021843 0000	0.22	9452.47	0	CCG-2	0	Y					1000	1000	17710	0	0000	0	4	Vacant
021844 0000	0.08	3588.43	0	CCG-2	0	Y					1000	1000	3500	0	0000	0	4	Vacant
021845 0000	0.12	5243.35	888	CCG-2	0	Y	8011	US 1		N	2792	2700	5750	11135	1985	2	1	SFR
021846 0000	0.03	1155.91	0	CCG-2	0	Y					1000	1000	1350	0	0000	0	3	Vacant
021847 0000	0.24	10645.54	780	RLD-G	6000	Y	5757	MC LEOD	AV		100	101	7425	17300	1958	3	3	SFR
021848 0000	0.12	5246.64	0	RLD-G	6000	N					0	1	4125	0	0000	0	3	Vacant
021849 0000	0.12	5312.98	0	RLD-G	6000	N					0	101	4125	0	0000	0	3	Vacant
021850 0000	0.10	4200.19	0	RLD-G	6000	N		MATTOX	AV		0	1	2970	0	0000	0	3	Vacant
021850 0500	0.14	6205.69	2731	RLD-G	6000	Y	8036	MATTOX	AV		100	101	4455	44500	1946	3	3	SFR
021850 0700	0.25	10728.54	3239	RLD-G	6000	Y	8044	MATTOX	AV		100	101	7425	67600	1961	3	3	SFR
021851 0000	0.24	10466.91	0	RLD-G	6000	Y					0	1	8250	0	0000	3	3	SFR
021852 0000	0.23	10192.39	0	RLD-G	6000	Y					0	1	7500	0	0000	3	3	SFR
021853 0000	0.23	9902.07	0	RLD-G	6000	Y					0	1	7500	0	0000	0	3	Vacant
021854 0000	0.12	5295.28	0	RLD-G	6000	N					0	1	3750	0	0000	0	3	Vacant
021855 0000	1.05	45908.98	0				8119	US 1		N	7100	7002	13349	25551	0000	3	3	Religious Institution
021861 0000	0.09	3857.74	0	RLD-G	6000	N					0	1	3375	0	0000	0	4	Vacant
021863 0000	0.11	4913.45	0	RLD-G	6000	N	8047	DEKLE	AV		0	1	3713	0	0000	0	3	Vacant
021864 0000	0.11	4830.04	378	RLD-G	6000	N	8045	DEKLE	AV		100	101	3713	5200	1956	1	1	SFR
021864 0010	0.11	4994.03	0	RLD-G	6000	N					0	1	3750	0	0000	0	3	Vacant
021865 0000	0.12	5019.52	0	RLD-G	6000	N					0	1	3750	0	0000	0	3	Vacant
021866 0000	0.23	9823.64	1344	RLD-G	6000	Y	8019	DEKLE	AV		200	201	6750	28600	1997	2	4	SFR
021867 0000	0.12	5046.01	0	RLD-G	6000	N					0	1	3713	0	0000	0	3	Vacant

Parcel ID Number	Acres	Parcel Area (Sq. Ft.)	Building Area (Sq. Ft.)	Zoning	Minimum Lot Size Required per Zoning District	Lot Meets Min. Lot Size Requirement	Street Number	Street Name	Street Type	Street Direction	Property Use Code	Land Use Code	Land Value	Building Value	Year Built	Building Condition	Site Condition	Existing Land Use
021867 0010	0.11	4907.91	728	RLD-G	6000	N	8013	DEKLE	AV		200	201	3375	7500	1980	2	4	SFR
021868 0000	0.23	9968.61	792	RLD-G	6000	Y	5767	MC LEOD	AV		100	101	7425	23100	1951	3	1	SFR
021869 0000	0.12	5298.04	0				5741	MC LEOD	AV		0	1	3812	0	0000	0	3	Vacant
021869 0100	0.10	4545.04	0								0	1	3465	0	0000	0	3	Vacant
021869 0200	0.16	6873.39	0								0	1	4455	0	0000	0	3	Vacant
021870 0000	0.25	10826.92	0								0	1	7425	0	0000	0	3	Vacant
021871 0000	0.12	5333.31	821				8022	SISKIN	AV		100	101	3713	16000	1943	0	3	Vacant
021874 0000	0.13	5617.00	0								0	1	3713	0	0000	0	3	Vacant
021875 0000	0.13	5460.69	0								0	1	3713	0	0000	1	1	Mobile Home
021877 0000	0.36	15826.58	0								0	1	11138	0	0000	0	3	Vacant
021878 0000	0.49	21390.95	3551				8136	SISKIN	AV		100	101	14850	63900	1964	2	2	SFR
021880 0000	0.21	9113.16	0								0	1	7425	0	0000	0	3	Vacant
021881 0000	0.21	9320.23	644				8125	MATTOX	AV		200	201	6750	11000	1990	3	3	SFR
021882 0000	0.11	4617.61	0								0	1	3713	0	0000	3	2	SFR
021882 0100	0.45	19757.14	1632	RLD-G	6000	N	8113	MATTOX	AV		200	201	8625	40900	2001	1	1	SFR
021883 0000	0.11	4585.19	240				8057	MATTOX	AV		200	201	3375	0	1962	3	2	SFR
021884 0000	0.10	4538.81	0								0	1	3713	0	0000	0	3	Vacant
021885 0000	0.36	15600.44	784				8033	MATTOX	AV		200	201	7688	10400	1984	0	3	Vacant
021886 0000	0.21	9072.94	2688				8027	MATTOX	AV		100	101	7425	72200	1976	0	3	Vacant
021887 0000	0.11	4589.50	0	RLD-G	6000	N					0	1	3713	0	0000	0	3	Vacant
021888 0000	0.10	4479.86	0								0	1	3713	0	0000	0	3	Vacant
021889 0000	0.21	9088.00	0								0	1	7425	0	0000	0	3	Vacant
021890 0000	0.11	4637.89	960				8005	MATTOX	AV		200	201	3375	8500	1986	2	2	Mobile Home
021918 0000	0.25	11024.52	2196	RLD-G	6000	Y	5815	FINCH	AV		100	101	7425	78000	2001	4	4	SFR
021919 0000	0.12	5196.59	0	RLD-G	6000	N					0	2	4909	0	0000	0	4	Vacant
021920 0000	0.23	10102.23	0	CCG-2	0	Y					1000	1000	7875	0	0000	0	4	Vacant
021921 0000	0.43	18893.24	0	RLD-G	6000	Y	8227	MATTOX	AV		0	2	19537	0	0000	0	4	Vacant
021921 0010	0.18	7624.53	2079	CCG-2	0	Y	8233	MATTOX	AV		200	200	7809	49700	1999	0	4	Vacant
021923 0000	0.11	4896.80	0	RLD-G	6000	N	8215	MATTOX	AV		0	2	4909	0	0000	0	4	Vacant
021924 0000	0.24	10476.68	0	RLD-G	6000	Y					0	2	9818	0	0000	0	4	Vacant
021925 0000	0.52	22577.09	0	CCG-2	0	Y		KINGS	RD		1000	1000	12994	0	0000	0	4	Vacant
021930 0000	0.05	2163.84	0	CCG-1	0	Y					1000	1000	1488	0	0000	0	3	Vacant
021931 0000	0.18	7918.56	0	CCG-2	0	Y					1000	1000	4875	0	0000	0	3	Vacant
021932 0000	0.15	6341.02	0	CCG-2	0	Y					1000	1000	4463	0	0000	0	3	Vacant
021933 0000	0.14	5957.94	0	CCG-2	0	Y		NEW KINGS	RD		1000	1000	3938	0	0000	0	3	Vacant
021934 0000	0.15	6422.39	0	CCG-2	0	Y					1000	1000	3938	0	0000	2	1	Commercial
021935 0000	0.11	4885.80	0	CCG-2	0	Y					1000	1000	5250	0	0000	2	1	Commercial
021936 0000	0.20	8697.28	725	CCG-2	0	Y	8232	NEW KINGS	RD		2691	2600	25650	8233	1957	2	1	Commercial
021938 0000	0.27	11575.20	0	RLD-G	6000	Y					0	1	7875	0	0000	0	4	Vacant
021939 0000	0.43	18611.61	1566	RLD-G	6000	Y	8214	KINGS	RD		100	101	11138	36800	1949	3	3	SFR
021940 0000	0.31	13687.27	3040	CCG-2	0	Y	5881	FINCH	AV		1292	1200	11138	66500	1954	4	4	SFR
021941 0000	0.13	5878.11	0	RLD-G	6000	N					0	1	3750	0	0000	3	3	SFR
021942 0000	0.26	11449.31	4116	RLD-G	6000	Y	8226	DEKLE	AV		100	101	7425	77300	1956	2	2	SFR
021943 0000	0.13	5492.65	1256	RLD-G	6000	N	8238	DEKLE	AV		100	101	3713	32200	1973	2	2	SFR
021944 0000	0.25	11022.40	0	RLD-G	6000	Y		DEKLE	AV		0	2	10500	0	0000	0	4	Vacant
021946 0000	0.26	11121.57	0				8462	DEKLE	AV		9600	9605	460	0	0000	0	4	Vacant
021947 0000	0.13	5647.89	0	RLD-G	6000	N					0	1	1238	0	0000	0	4	Vacant
021948 0000	0.13	5705.55	0	RLD-G	6000	N					0	1	1238	0	0000	0	4	Vacant
021949 0000	0.13	5763.80	0	RLD-G	6000	N					0	1	1238	0	0000	0	4	Vacant
021950 0000	0.13	5820.62	0	RLD-G	6000	N					0	1	1238	0	0000	0	4	Vacant
021951 0000	0.18	7930.52	0	RLD-G	6000	Y					0	1	1355	0	0000	0	4	Vacant
022007 0000	0.23	10152.54	1297	RLD-G	6000	Y	8026	MONCRIEF	EX		100	101	5079	19900	1959	3	3	SFR
022009 0000	0.74	32032.04	2052	CO	6000	Y	8020	MONCRIEF-DINSMORE	RD		1293	1200	32235	47700	1998	2	2	Mobile Home
022010 0000	0.24	10446.41	0	RLD-G	6000	Y	8004	MONCRIEF	RD		0	1	4617	0	0000	0	3	Vacant
022011 0000	0.42	18467.20	3608	CCG-2	0	Y	7992	NEW KINGS	RD		3900	3900	22950	102420	1987	3	3	Commercial
022012 0000	0.32	13962.69	1910	CCG-2	0	Y	7954	U S 1		N	2792	2720	15065	10141	1946	1	1	Commercial
022013 0000	0.22	9590.90	0	CCG-2	0	Y					1000	1000	12825	0	0000	0	3	Vacant
022014 0000	0.31	13681.00	920	RLD-G	6000	Y	7938	KINGS	RD		100	101	5301	11400	1969	4	3	SFR
022015 0000	0.06	2397.54	588	CCG-2	0	Y	7930	KINGS	RD		1292	1200	3484	11300	1962	1	1	Commercial
022016 0000	0.03	1189.95	540	CCG-2	0	Y	7924	KINGS	RD		1292	1200	8321	4500	1959	2	1	SFR
022016 0000	0.14	6266.91	540	CCG-2	0	Y	7924	KINGS	RD		1292	1200	8321	4500	1959	2	1	SFR

Parcel ID Number	Acres	Parcel Area (Sq. Ft.)	Building Area (Sq. Ft.)	Zoning	Minimum Lot Size Required per Zoning District	Lot Meets Min. Lot Size Requirement	Street Number	Street Name	Street Type	Street Direction	Property Use Code	Land Use Code	Land Value	Building Value	Year Built	Building Condition	Site Condition	Existing Land Use
022016 0000	0.14	6266.91	540	CCG-2	0	Y	7924	KINGS	RD		1292	1200	8321	4500	1959	2	1	SFR
022017 0000	0.31	13623.66	2798	CCG-2	0	Y	7913	NEW KINGS	RD		1292	820	12375	41700	1946	1	1	MFR
022019 0000	0.38	16375.94	1850	RLD-G	6000	Y	7903	NEW KINGS	RD		100	101	10725	45900	1972	1	1	SFR
022020 0000	0.56	24556.12	3998	CCG-2	0	Y	7900	U S 1		N	7100	7100	25800	34697	1960	3	3	Commercial
022260 0000	1.12	48772.37	1800	CCG-2	0	Y	5604	MONCRIEF	RD		2792	2720	10185	7272	1956	1	2	Commercial
022262 0000	0.12	5011.63	0	RLD-G	6000	N					0	1	2670	0	0000	0	4	Vacant
022265 0000	0.36	15487.54	0	RLD-G	6000	Y					0	1	5725	0	0000	0	4	Vacant
022269 0000	0.37	16100.04	0	CCG-2	0	Y		MONCRIEF	RD	W	1000	1000	9148	0	0000	1	1	Commercial
022269 0000	0.02	983.49	0	CCG-2	0	Y		MONCRIEF	RD	W	1000	1000	9148	0	0000	1	1	Commercial
022270 0000	0.57	24945.86	460	CCG-2	0	Y	5736	MONCRIEF	RD	W	1292	1200	29185	200	1942	0	4	Vacant
022271 0000	0.52	22443.07	0	CCG-2	0	Y					1000	1000	26572	0	0000	0	4	Vacant
022272 0000	0.11	4730.63	0	CCG-2	0	Y					1000	1000	2250	0	0000	0	4	Vacant
022272 0010	0.21	9265.07	0	CCG-2	0	Y					1000	1000	6000	0	0000	0	4	Vacant
022273 0000	0.26	11443.77	0	CCG-2	0	Y	5717	DUNMIRE	AV		1000	1000	7500	0	0000	0	4	Vacant
022273 0010	0.41	17739.71	2021	RLD-G	6000	Y	5658	MONCRIEF	RD	W	2191	2100	10600	29499	1973	3	2	SFR
022274 0000	2.28	99165.85	920	CCG-2	0	Y	7709	NEW KINGS	RD		4900	4900	35066	6377	1956	1	1	Junkyard
022276 0000	0.08	3581.56	0	CCG-2	0	Y					1000	1000	1780	0	0000	1	1	Junkyard
022279 0000	1.33	58077.84	0	RLD-G	6000	N					0	1	25870	0	0000	1	1	Junkyard
022281 0000	0.02	942.18	0	RLD-G	6000	N					0	1	1005	0	0000	1	1	Vacant
022282 0000	0.01	225.14	0	RLD-G	6000	N					0	1	834	0	0000	1	1	Vacant
022284 0000	0.07	2911.59	0	RLD-G	6000	N					0	1	1515	0	0000	0	4	Vacant
022287 0000	0.93	40307.68	0	RLD-G	6000	Y					7000	7100	13800	0	0000	0	4	Vacant
022287 0050	0.24	10505.27	1643				5846	MONCRIEF	RD	W	100	101	6675	35300	1960	3	3	SFR
022288 0000	0.15	6691.12	0	RLD-G	6000	Y					0	1	4005	0	0000	0	4	Vacant
022289 0000	0.19	8121.53	1152				5851	DUNMIRE	AV		100	101	5340	25200	1940	3	3	SFR
022290 0000	0.20	8529.22	1961	RLD-G	6000	Y	5843	DUNMIRE	AV		1192	3900	10000	14061	1961	3	3	SFR
022291 0000	0.10	4325.73	2584	RLD-G	6000	N	5827	DUNMIRE	AV		7100	7100	2500	15100	1967	3	2	Religious Institution
022293 0000	0.21	9095.44	1088				7720	U S 1			100	101	5082	17400	1935	0	4	Vacant
022294 0000	0.12	5289.28	0	RLD-G	6000	N	5812	DUNMIRE	AV		0	1	2670	0	0000	0	4	Vacant
022295 0000	0.41	17745.08	0	RLD-G	6000	Y					0	1	5340	0	0000	0	4	Vacant
022297 0000	0.34	14809.10	1754				5842	DUNMIRE	AV		100	101	8010	25100	1943	2	3	SFR
022299 0000	0.06	2487.92	0	RLD-G	6000	N					0	1	1335	0	0000	0	4	Vacant
022299 0010	0.12	5285.57	505				5866	DUNMIRE	AV		100	101	2670	11600	1964	2	3	SFR
022300 0000	0.54	23736.11	0	RLD-G	6000	Y		PATTERSON	AV		7000	7000	22250	0	0000	0	4	Vacant
022301 0000	0.33	14448.50	0	RLD-G	6000	Y					0	1	8010	0	0000	0	4	Vacant
022302 0000	0.12	5070.12	0	RLD-G	6000	N		PATTERSON	AV		0	1	2670	0	0000	0	4	Vacant
022303 0000	0.30	13000.29	0	RLD-G	6000	Y					0	1	7478	0	0000	0	4	Vacant
022304 0000	0.19	8365.25	0	RLD-G	6000	Y					0	1	2400	0	0000	0	4	Vacant
022305 0000	0.49	21435.88	0	RLD-G	6000	Y					0	1	7425	0	0000	0	4	Vacant
022306 0000	0.24	10650.28	0	RLD-G	6000	Y					0	1	4500	0	0000	0	4	Vacant
022307 0000	0.16	6846.69	0	RLD-G	6000	Y	5830	PATTERSON	AV		0	1	3000	0	0000	0	4	Vacant
022312 0000	0.19	8469.13	2463				5910	MONCRIEF	RD	W	100	101	4680	52400	1973	3	3	SFR
022324 0010	0.20	8530.59	0				5903	DUNMIRE	AV		0	1	5340	0	0000	0	4	Vacant
029902 0100	15.55	677193.01	11751	IH	0	Y	5937	SOUTEL	DR		4900	4900	187526	166761	1996	3	2	Heavy Industrial
029908 0210	2.04	88658.81	3735	RR	43560	Y	5196	PICKETT	DR		1293	9922	38700	44900	1989	2	3	Mobile Home
029908 0220	4.52	197034.39	2400	IL	0	Y	5196	PICKETT	DR		2792	2500	26379	33767	1985	3	3	Commercial
029908 0230	5.55	241860.71	1344	IL	0	Y	5127	PICKETT	DR		1700	1700	108900	19000	1988	3	3	Commercial
029908 0310	5.87	255729.68	11350	IL	0	Y	6200	SOUTEL	DR		4900	4900	97117	207619	1989	2	3	Industrial
029908 0350	3.16	137793.97	0	IL	0	Y					4000	4000	68313	0	0000	0	4	Vacant
029908 0400	13.43	585057.58	24400	IL	0	Y	6611	PICKETT	DR		4891	4800	306682	659197	1985	4	4	Industrial
029908 0450	3.44	150045.95	0	IL	0	Y					4000	4000	75397	0	0000	0	4	Vacant
029908 0600	10.07	438476.65	0	IL	0	Y		PICKETT	DR		4000	4000	179347	0	0000	0	4	Vacant
029908 0700	1.75	76181.12	6368	IL	0	Y	6585	PICKETT	DR		2792	2720	38551	188796	2001	4	3	Commercial
029908 0800	1.58	68780.61	0	IL	0	Y		PICKETT	DR		4000	4000	33106	0	0000	0	4	Vacant
029908 0900	0.72	31200.22	0	IL	0	Y		PICKETT	DR		4000	4000	10455	0	0000	0	4	Vacant
029910 0000	13.65	594601.30	0	RR	43560	Y		SOUTEL	DR		9900	9930	40285	0	0000	0	4	Vacant
029925 0000	1.91	83259.02	2456	IL	0	Y	6142	SOUTEL	DR		100	1200	28258	63700	1941	3	3	Industrial
029939 0000	0.71	30957.23	0	RLD-G	6000	Y		SOUTEL	DR		0	1	7986	0	0000	0	4	Vacant
029940 0000	3.35	145851.72	14128	IH	0	Y	5930	SOUTEL	DR		4700	4700	145926	53068	1963	3	3	Industrial
029940 0100	25.60	1115003.78	0	IH	0	Y		SOUTEL	DR		4000	4000	311247	0	0000	0	4	Vacant
029940 0200	7.76	338083.10	34914	IH	0	Y	5930	SOUTEL	DR		4700	4700	83995	270394	1947	3	2	SFR

Parcel ID Number	Acres	Parcel Area (Sq. Ft.)	Building Area (Sq. Ft.)	Zoning	Minimum Lot Size Required per Zoning District	Lot Meets Min. Lot Size Requirement	Street Number	Street Name	Street Type	Street Direction	Property Use Code	Land Use Code	Land Value	Building Value	Year Built	Building Condition	Site Condition	Existing Land Use
029940 0350	0.78	33957.16	0	IH	0	Y		SOUTEL	DR		4000	4000	54016	0	0000	0	4	Vacant
029940 0350	0.12	5174.23	0	IH	0	Y		SOUTEL	DR		4000	4000	54016	0	0000	0	4	Vacant
029940 1500	44.84	1953130.87	0	IH	0	Y		SOUTEL	DR		4000	4000	364201	0	0000	0	4	Vacant
029943 0000	0.40	17209.99	1920	RLD-G	6000	Y		SOUTEL	DR		7100	7100	8586	30378	1960	3	2	Religious Institution
029952 0000	1.98	86107.84	1909	RLD-G	6000	Y	5869	SOUTEL	DR		100	103	16470	26000	1948	3	3	SFR
029970 0000	0.28	12166.23	1435	RLD-G	6000	Y	5816	SOUTEL	DR		100	101	4241	49700	1972	2	3	SFR
029970 0010	0.20	8676.25	1435	RLD-G	6000	Y	5838	SOUTEL	DR		100	101	3733	53500	1972	3	3	SFR
029970 0020	0.31	13339.91	1435	RLD-G	6000	Y	5808	SOUTEL	DR		100	101	4278	49700	1972	2	3	SFR
029970 0030	0.25	11051.33	1435	RLD-G	6000	Y	5822	SOUTEL	DR		100	101	4092	53500	1972	2	3	SFR
029970 0040	0.23	9838.67	1435	RLD-G	6000	Y	5830	SOUTEL	DR		100	101	3980	53000	1972	3	3	SFR
029976 0000	39.12	1704121.25	0	IH	0	Y		SOUTEL	DR		4900	4900	807700	0	0000	4	4	Industrial
029976 1000	17.58	765755.20	30479	IH	0	Y	5995	SOUTEL	DR		4100	9999	382021	1173521	1999	4	4	Heavy Industrial
029977 0000	2.20	95711.30	870	RLD-G	6000	Y	5863	SOUTEL	DR		100	103	18630	11500	1962	2	3	SFR
029977 0010	0.43	18829.46	0	RLD-G	6000	Y					0	3	16020	0	0000	0	3	Vacant
029977 0010	1.36	59380.82	0	RLD-G	6000	Y					0	3	16020	0	0000	0	3	Vacant
029977 0030	0.59	25751.89	0	RLD-G	6000	Y					0	1	2993	0	0000	0	3	Vacant
030032 0000	0.86	37508.46	0	RLD-G	6000	Y					7000	7000	840	0	0000	0	4	Vacant
030039 0000	0.48	21124.90	1640	RLD-G	6000	Y	5857	SOUTEL	DR		100	101	6821	35800	1926	2	2	SFR
030087 0000	1.43	62168.82	1750	IBP-2	10000	Y	6242	SOUTEL	DR		7100	7100	17640	55260	1963	4	4	Religious Institution
030095 0000	0.84	36591.58	7160	IH	0	Y	5923	SOUTEL	DR		1700	1700	12807	203979	1978	3	2	Industrial
030289 0520	0.17	7582.64	2009	RLD-G	6000	Y	5871	JOHN F KENNEDY	DR	N	100	101	5630	90000	1990	3	3	SFR
030289 0530	0.25	10894.86	1940	RLD-G	6000	Y	5859	JOHN F KENNEDY	DR	N	100	101	7249	82900	1986	3	3	SFR
030289 0540	0.26	11174.76	1771	RLD-G	6000	Y	5815	JOHN F KENNEDY	DR		100	101	6395	73200	1987	3	3	SFR
030289 0550	0.26	11268.51	1954	RLD-G	6000	Y	5831	JOHN F KENNEDY	DR	N	100	101	6732	85400	1986	3	3	SFR
030289 0560	0.25	11047.95	1535	RLD-G	6000	Y	5843	JOHN F KENNEDY	DR	N	100	101	6782	70700	1988	3	3	SFR
030289 0570	0.21	9295.44	2015	RLD-G	6000	Y	5825	JOHN F KENNEDY	DR	N	100	101	6732	90300	1990	3	3	SFR
030289 0580	0.27	11973.90	1520	RLD-G	6000	Y	5809	JOHN F KENNEDY	DR	N	100	101	6732	67100	1987	2	3	SFR
030289 0600	7.50	326493.43	36444	CRO	5000-7000	Y	7578	U S 1			7100	7100	117540	605160	1986	4	4	Religious Institution
030289 0900	1.54	67243.69	0	CRO	5000-7000	Y					7000	7000	65880	0	0000	4	4	Commercial
039927 0000	40.55	1766424.70	0	RR	43560	Y		SIBBALD	RD		7000	9950	191927	0	0000	0	4	Vacant
039927 7500	24.24	1055894.94	0	CO	6000	Y		TROUT RIVER	BV		5600	5610	693000	0	0000	0	4	Vacant
039928 0000	3.68	160416.80	0	CCG-1	0	Y					1000	1000	107243	0	0000	0	4	Vacant
039929 0000	0.63	27243.69	6385	CCG-2	0	Y	8428	NEW KINGS	RD	N	1700	1700	27720	70075	1951	2	2	Mobile Home
039931 0000	5.08	221100.99	24878	CCG-2	0	Y	8431	NEW KINGS	RD		4899	4800	109641	336399	1986	3	2	Commercial
039932 0000	1.94	84485.51	2786	CCG-1	0	Y	8627	NEW KINGS	RD		1292	1200	64500	76100	1941	2	3	Commercial
039937 0000	0.94	40901.54	5724				6070	GILCHRIST	RD		100	101	8418	199800	1994	3	3	SFR
039938 0000	2.89	126062.62	0	CCG-2	0	Y		NEW KINGS	RD		1000	1000	63220	0	0000	0	3	Vacant
039948 0000	11.21	488477.11	20520	PUD	N/A		8700	NEW KINGS	RD		2792	2720	231650	764093	2000	4	4	Commercial
039954 0000	2.51	109250.54	2736	RMD-A	5000	Y	9454	WAGNER	RD		100	103	25800	30000	1920	3	3	SFR
039954 0100	0.91	39545.86	1228	RMD-A	5000	Y	9466	WAGNER	RD		200	203	10000	8000	1979	2	3	Mobile Home
039962 0000	1.29	56087.72	0	CCG-2	0	Y					1000	1000	94298	0	0000	0	4	Vacant
039962 0100	2.26	98605.18	5590	CCG-2	0	Y	8735	NEW KINGS	RD		1492	1410	244500	182341	1995	3	3	Commercial
039963 0000	0.64	27717.94	0	RR	43560	N	6039	GILCHRIST	RD		0	9900	10881	0	0000	0	3	Vacant
039963 0010	0.50	21795.23	0	RR	43560	N		PINE PARK	DR		9400	9400	550	0	0000	0	0	Right of Way
039963 0020	0.22	9425.14	4270	RMD-A	5000	Y	8660	PINE PARK	DR		100	132	29000	155200	1998	4	3	SFR
039963 0020	2.11	91815.11	4270	RMD-A	5000	Y	8660	PINE PARK	DR		100	132	29000	155200	1998	4	3	SFR
039964 0000	0.61	26374.49	2227	RR	43560	N	8657	PINE PARK	DR		100	132	18700	84900	1987	3	3	SFR
039964 0010	0.25	10918.67	1159	RR	43560	N	8667	PINE PARK	DR		200	201	6008	9000	1973	2	3	Mobile Home
039965 0000	0.29	12562.51	1106	RR	43560	N	8645	PINE PARK	DR		200	9902	6675	4700	1965	2	3	Mobile Home
039967 0000	2.60	113327.48	2572	CCG-2	0	Y	8374	NEW KINGS	RD		1700	1700	58750	41413	1960	2	2	Commercial
039968 0000	1.28	55589.95	575	CO	6000	Y	8519	NEW KINGS	RD		2792	2	13725	515	1966	2	3	Commercial
039968 0010	0.53	22952.38	3696	CCG-2	0	Y	8521	NEW KINGS	RD		2794	1200	19865	9417	1947	2	1	Commercial
039975 0000	1.96	85588.90	4784	CCG-2	0	Y	8611	NEW KINGS	RD	N	3900	3900	83848	66326	1955	2	2	Commercial
039978 0000	1.99	86723.40	2299	RMD-A	5000	Y	9420	WAGNER	RD		100	103	20200	64600	1960	3	3	SFR
039981 0000	0.65	28220.22	2286	CCG-2	0	Y	8506	NEW KINGS	RD		1291	1200	28875	4466	1941	1	1	Commercial
039982 0000	2.11	91989.99	0	CCG-2	0	Y					1000	1000	80144	0	0000	0	3	Vacant
039987 0000	2.31	100445.15	5543	CCG-2	0	Y	8448	NEW KINGS	RD		7100	7100	89027	58424	1946	3	3	Religious Institution
039998 0000	0.75	32458.30	0	CCG-2	0	Y		WAGNER	RD		1000	1000	22150	0	0000	0	4	Vacant
040005 0000	3.67	159846.79	2000	CCG-2	0	Y	8325	U S 1		N	2792	2700	95204	32558	1987	2	2	Commercial
040013 0000	1.10	47945.86	0	CCG-2	0	Y					1000	1000	59000	0	0000	0	4	Vacant
040022 0000	1.19	51624.84	3048	CCG-2	0	Y	8342	NEW KINGS	RD		2792	2720	40800	14896	1952	2	2	Commercial

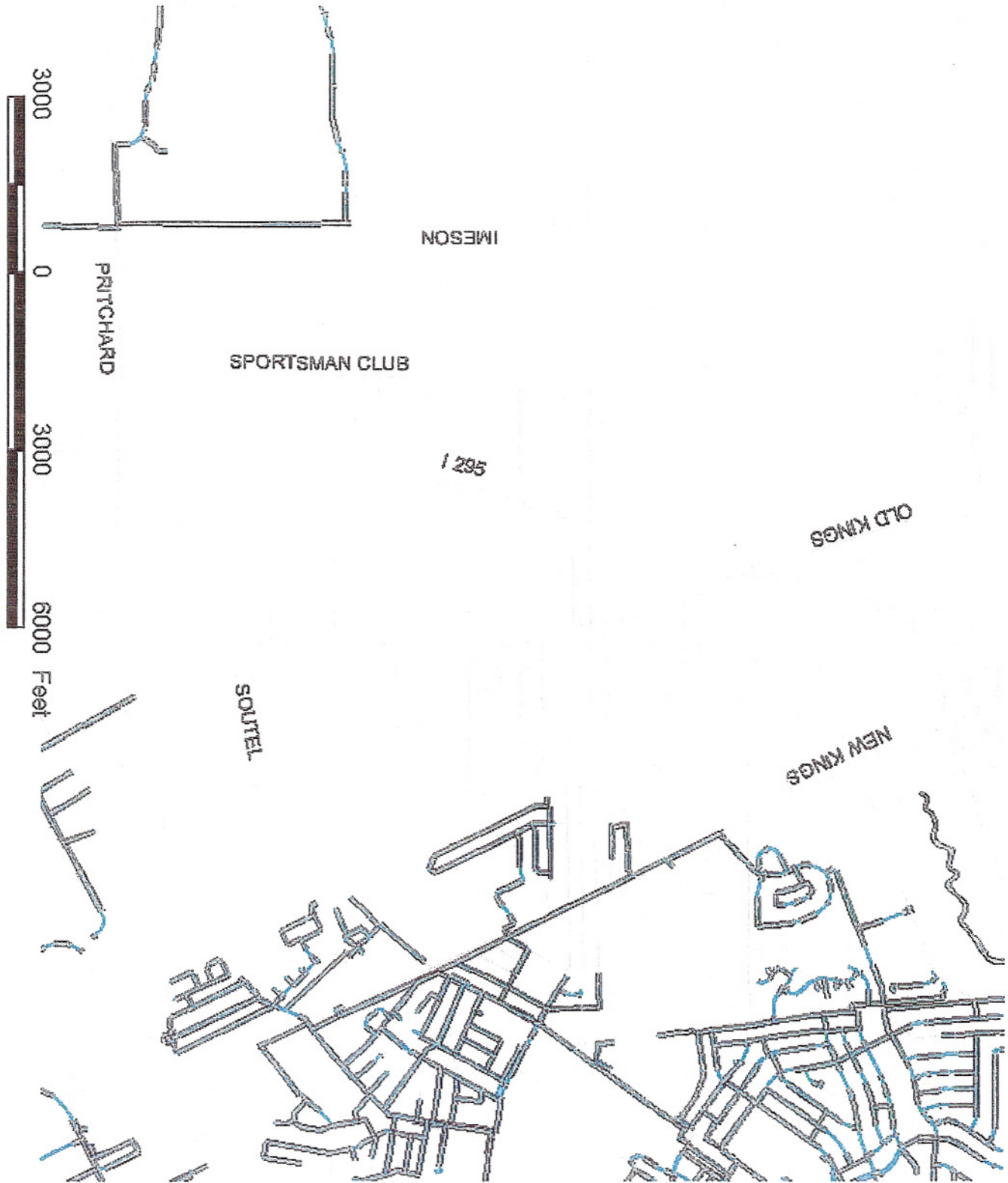
Parcel ID Number	Acres	Parcel Area (Sq. Ft.)	Building Area (Sq. Ft.)	Zoning	Minimum Lot Size Required per Zoning District	Lot Meets Min. Lot Size Requirement	Street Number	Street Name	Street Type	Street Direction	Property Use Code	Land Use Code	Land Value	Building Value	Year Built	Building Condition	Site Condition	Existing Land Use
040029 0000	7.42	323118.52	31478	CO	6000	Y	8654	NEW KINGS	RD	N	2893	2810	244503	679937	1966	2	2	Mobile Home Park
040033 0000	4.70	204818.23	19581	CCG-2	0	Y	8375	NEW KINGS	RD		3900	3900	91395	344954	1953	3	2	Commercial
040035 0000	0.09	3939.65	0	CO	6000	N		NEW KINGS	RD		1000	1010	49035	0	0000	0	4	Vacant
040035 0000	3.96	172350.28	0	CO	6000	Y		NEW KINGS	RD		1000	1010	49035	0	0000	0	4	Vacant
040038 0000	0.22	9556.52	2848	CCG-2	0	Y	8339	U S 1		N	2792	2720	18750	14341	1969	2	1	Commercial
040041 0000	1.91	83336.62	0	CCG-2	0	Y		KINGS	RD		1000	1000	60859	0	0000	0	1	Vacant
040042 0000	0.35	15084.87	1660	RR	43560	N	8655	NEW KINGS	RD		100	9901	6413	46100	1958	2	3	SFR
040044 0000	3.18	138392.75	5518	CCG-2	0	Y	8483	NEW KINGS	RD		1492	1410	79253	67238	1969	2	1	Commercial
040061 0000	3.88	168835.95	0	CCG-2	0	Y	8562	NEW KINGS	RD		1000	1000	67521	0	0000	0	4	Vacant
040062 0000	0.83	36368.08	1160	RR	43560	N	8654	PINE PARK	DR		200	9902	7275	6600	1970	2	3	Mobile Home
040064 0000	0.52	22482.83	1842	RR	43560	N	6029	GILCHRIST	RD		100	9901	8951	60600	1960	3	3	SFR
040065 0000	1.07	46682.70	924	RR	43560	Y	8630	PINE PARK	DR		200	9922	7875	7700	1967	2	3	Mobile Home
040066 0000	0.60	25936.07	720	RR	43560	N	8642	PINE PARK	DR		200	9902	7740	4700	1966	2	3	Mobile Home
040067 0000	0.39	16805.88	1390	RR	43560	N	6049	GILCHRIST	RD		100	9901	7020	36500	1956	2	3	SFR
040067 0010	0.17	7423.31	0	CCG-2	0	Y		NEW KINGS	RD		1000	1000	7875	0	0000	3	2	Vacant
040067 0030	1.43	62436.80	0	CCG-1	0	Y	8653	NEW KINGS	RD		1000	1000	61855	0	0000	0	3	Commercial
040067 0035	0.17	7539.47	680	RR	43560	N	8651	NEW KINGS	RD		100	9901	5106	17300	1963	2	3	SFR
040067 0050	4.99	217237.85	26753	CCG-2	0	Y	8406	NEW KINGS	RD		2893	2811	87923	216964	1973	2	2	Mobile Home Park
041631 0010	0.92	40225.28	4821	CCG-2	0	Y	7257	NEW KINGS	RD		2792	2720	127740	63193	1967	2	1	Commercial
041633 0030	0.43	18813.74	7024	CCG-2	0	Y	5552	SOUTEL	DR		1692	1600	31500	57712	1968	3	2	Commercial
041636 0000	0.87	37758.30	1904	RMD-C	6000	Y	5626	SOUTEL	DR		1291	1200	13800	72717	1975	3	3	SFR
041636 0500	1.46	63679.45	3542	RMD-C	6000	Y	5650	SOUTEL	DR		100	101	13109	80900	1989	3	3	SFR
041637 0000	1.70	74077.14	3063	CCG-2	0	Y	7222	NEW KINGS	RD		2191	2100	38865	54572	1955	2	1	Commercial
041637 0010	0.20	8583.05	1456	RLD-G	6000	Y	5610	SOUTEL	DR		100	101	9100	61800	1980	3	3	SFR
041637 0020	0.20	8765.49	0	RLD-G	6000	Y					0	2	2722	0	0000	0	3	Vacant
041637 0030	0.24	10665.65	1225				7208	NEW KINGS	RD		100	101	12349	46100	1983	3	3	SFR
041637 0040	0.05	2021.44	0	RLD-G	6000	N		SOUTEL	DR		9600	9600	175	0	0000	3	3	SFR
041638 0000	1.89	82424.80	1868				6552	NEW KINGS	RD		100	103	6000	14500	1914	3	3	SFR
041639 0000	0.18	7635.66	1466				7142	NEW KINGS	RD		100	101	8344	37500	1958	3	3	SFR
041641 0000	0.58	25282.40	668				5706	SOUTEL	DR		100	103	1530	13300	1941	1	2	SFR
041642 0000	1.84	80165.47	0					SOUTEL	DR		0	3	5010	0	0000	0	4	Vacant
041643 0000	1.26	54774.28	2084				5676	SOUTEL	DR		100	101	11965	38800	1948	4	3	SFR
041643 0010	0.12	5240.11	0				5656	SOUTEL	DR		0	1	4370	0	0000	3	3	SFR
041646 0000	7.92	344938.54	26047	RLD-G	6000	Y	5755	SOUTEL	DR		7100	7100	37900	1709203	1991	4	4	Religious Institution
041647 0000	1.53	66759.74	0	CCG-2	0	Y	7310	NEW KINGS	RD		1000	1000	33324	0	0000	0	4	Vacant
041648 0000	16.90	736144.79	0	CO	6000	Y		SOUTEL	DR		0	1000	123975	0	0000	0	4	Vacant
041649 0010	2.09	90991.87	2556	CCG-2	0	Y	7380	NEW KINGS	RD		2792	2720	14656	14474	1966	1	1	Junkyard
041651 0000	21.21	923771.48	8160	CRO	5000-7000	Y		NEW KINGS	RD		7100	7100	187709	250482	1971	4	4	Religious Institution
041723 0000	0.24	10502.75	0								0	1	2670	0	0000	0	3	Vacant
041724 0000	0.63	27367.87	2480	CO	6000	Y	5411	SOUTEL	DR		693	600	66988	82438	1977	3	2	Commercial
041725 0000	0.24	10337.60	0	RLD-G	6000	Y					0	1	6675	0	0000	0	3	Vacant
041725 0020	0.20	8872.94	1280	CCG-1	0	Y	5423	SOUTEL	DR		1291	1200	15029	42820	1970	3	2	Commercial
041726 0000	0.22	9429.38	0	RLD-G	6000	Y					0	1	3750	0	0000	0	4	Vacant
041726 0010	0.23	10215.05	0								0	1	6230	0	0000	0	4	Vacant
041727 0000	0.10	4533.72	0	CO	6000	N					1000	1000	7500	0	0000	0	4	Vacant
041728 0000	0.12	5246.64	0								0	1	2225	0	0000	0	4	Vacant
041728 0010	0.11	4993.48	0								0	1	2225	0	0000	0	4	Vacant
041730 0000	0.43	18918.38	3614	RLD-G	6000	Y	5475	SOUTEL	DR		1991	1900	35000	110778	1975	4	4	Commercial
041731 0000	0.10	4439.74	1812	CO	6000	N	5521	SOUTEL	DR		1700	1700	7500	72426	1976	3	3	Commercial
041732 0000	0.23	9956.79	0	RLD-G	6000	Y					0	1	15000	0	0000	0	4	Vacant
041734 0000	0.09	4023.29	600	CO	6000	N					1293	1200	7500	2500	1960	3	3	Commercial
041735 0000	0.33	14580.58	0	RLD-G	6000	Y					0	1	16400	0	0000	2	2	Commercial
041735 0100	0.18	7738.46	0	CO/CC	6000	Y					0	1000	15000	0	0000	0	4	Vacant
041736 0000	0.31	13562.18	0	CCG-2	0	Y					1000	1000	11979	0	0000	0	4	Vacant
041737 0000	0.43	18791.46	1961	CCG-2	0	Y	5581	SOUTEL	DR		2200	2200	83112	153134	1990	4	4	Commercial
041741 0000	0.09	3778.60	0	CCG-2	0	Y					1000	1000	4000	0	0000	0	4	Vacant
041744 0000	0.35	15297.07	3481				5454	AGESON	RD		100	101	9345	88000	1973	4	4	SFR
041746 0000	0.12	5157.48	0								0	1	1335	0	0000	0	4	Vacant
041747 0000	0.23	9991.03	0								0	1	2670	0	0000	0	4	Vacant
041748 0000	0.12	5129.47	0								0	1	1335	0	0000	0	4	Vacant
041749 0000	0.12	5281.62	0								0	1	1335	0	0000	0	4	Vacant

Parcel ID Number	Acres	Parcel Area (Sq. Ft.)	Building Area (Sq. Ft.)	Zoning	Minimum Lot Size Required per Zoning District	Lot Meets Min. Lot Size Requirement	Street Number	Street Name	Street Type	Street Direction	Property Use Code	Land Use Code	Land Value	Building Value	Year Built	Building Condition	Site Condition	Existing Land Use
041750 0000	0.24	10384.52	0								0	1	2670	0	0000	0	4	Vacant
041751 0000	0.25	10710.71	0	RLD-G	6000	Y		MULL	ST		7000	7000	2670	0	0000	0	4	Vacant
041751 0010	0.24	10481.99	0								0	1	2225	0	0000	0	4	Vacant
041751 0100	0.26	11205.83	0								0	1	2225	0	0000	0	4	Vacant
041752 0000	0.13	5457.77	0								0	1	1350	0	0000	0	4	Vacant
041752 0100	0.25	10766.23	0					MARTIN	ST		0	1	1725	0	0000	0	4	Vacant
041754 0000	1.66	72224.19	4256	CCG-2	0	Y	7415	NEW KINGS	RD		4100	4100	96300	60682	1974	3	3	Light Industrial
041764 0000	0.11	4759.55	0								0	1	1335	0	0000	0	4	Vacant
041765 0000	0.24	10358.05	2700				5522	AGESON	RD		100	101	6230	57800	1967	3	3	SFR
041766 0000	0.22	9522.10	0								0	1	2670	0	0000	0	4	Vacant
041767 0000	0.12	5193.93	0								0	1	1335	0	0000	0	3	Vacant
041768 0000	0.12	5233.72	0								0	1	1335	0	0000	0	3	Vacant
041769 0000	0.11	4874.56	0								0	1	1335	0	0000	2	2	SFR
041770 0000	0.11	4822.50	0								0	1	1335	0	0000	0	2	Vacant
041771 0000	0.12	5193.41	0								0	1	1335	0	0000	0	3	Vacant
041772 0000	0.12	5320.75	0								0	1	1335	0	0000	0	3	Vacant
041773 0000	0.11	4862.19	0								0	1	1335	0	0000	0	2	Vacant
041774 0000	0.11	4950.53	0	RLD-G	6000	N					0	1	3750	0	0000	0	2	Vacant
041775 0000	0.24	10402.88	1324	CCG-2	0	Y	7444	JERRY	AV		1292	1200	7500	26100	1955	3	2	SFR
041775 0100	0.11	4952.89	1900	RLD-G	6000	N	7439	ROBERTS	AV		100	101	3750	60700	1960	0	2	Vacant
041775 0200	0.12	5064.87	0	RLD-G	6000	N					0	1	3750	0	0000	2	2	SFR
041776 0000	0.12	5223.12	1320	RLD-G	6000	N	7432	JERRY	AV		100	101	3500	24700	1952	3	2	SFR
041778 0000	0.11	4877.98	0	CCG-2	0	Y		ROBERTS	ST		0	1000	2625	0	0000	0	3	Vacant
041779 0000	0.52	22653.03	0	CCG-2	0	Y					1000	1000	16335	0	0000	0	3	Vacant
041782 0000	0.32	13783.02	0								0	1	6675	0	0000	0	3	Vacant
041783 0010	0.22	9660.49	2084	CCG-2	0	Y	5544	AGESON	RD		1292	1200	7500	35600	1931	3	3	SFR
041783 0013	0.44	19290.99	0	RLD-G	6000	Y					0	1	10220	0	0000	2	2	SFR
041783 0015	0.24	10453.83	0	CCG-2	0	Y		JERRY	AV		1000	1000	7500	0	0000	0	4	Vacant
041783 0020	0.60	26287.03	825	CCG-2	0	Y	7445	NEW KINGS	RD		1492	1410	62500	6774	1970	2	1	Commercial
041783 0025	0.25	10857.22	0	CCG-2	0	Y					0	1000	7500	0	0000	1	2	SFR
041793 0000	0.08	3465.39	0								0	1	1515	0	0000	0	4	Vacant
041795 0000	0.22	9776.62	0	RLD-G	6000	Y					0	1	2670	0	0000	0	4	Vacant
041797 0000	0.15	6723.60	0								0	1	1440	0	0000	0	4	Vacant
041798 0000	0.31	13643.05	0					LORIDA	AV		0	1	2670	0	0000	0	4	Vacant
041798 0050	0.34	14786.14	0				5858	PATTERSON	AV		0	1	2880	0	0000	0	4	Vacant
041799 0000	0.68	29423.17	3059	CCG-2	0	Y	7525	NEW KINGS	RD		1291	1200	23100	113256	1970	3	3	Commercial
041804 0000	0.13	5828.22	1548	CCG-2	0	Y	7623	NEW KINGS	RD		1192	1100	89639	45777	1979	3	1	Commercial
041804 0000	0.84	36677.58	1548	CCG-2	0	Y	7623	NEW KINGS	RD		1192	1100	89639	45777	1979	3	1	Commercial
041804 0000	0.80	34808.11	1548	CCG-2	0	Y	7623	NEW KINGS	RD		1192	1100	89639	45777	1979	3	1	Commercial
041804 0000	7.15	311506.20	1548	CCG-2	0	Y	7623	NEW KINGS	RD		1192	1100	89639	45777	1979	3	1	Commercial
041805 0000	0.27	11971.72	2508	CCG-2	0	Y	7641	NEW KINGS	RD		1292	1200	8352	58800	1952	3	3	SFR
041915 0550	0.29	12448.11	2341				7093	BISHOP HATCHER	DR	E	100	101	10213	79800	1967	3	3	SFR
041915 0560	0.20	8624.68	870	RLD-G	6000	Y	5718	SOUTEL	DR		100	101	8374	28800	1967	2	2	SFR
041915 0810	0.20	8710.42	1916				5752	SOUTEL	DR		100	1	8925	77200	1968	3	3	SFR
041915 0820	0.24	10561.52	1440				5764	SOUTEL	DR		100	101	8925	51100	1968	3	3	SFR
041915 0830	0.23	9949.59	1596				5776	SOUTEL	DR		100	101	8708	54200	1968	4	3	SFR
042013 0000	0.68	29503.09	17120	CCG-2	0	Y	5522	SOUTEL	DR		1692	1600	42471	73283	1968	2	1	Commercial
042024 0000	0.37	16037.86	0	CCG-2	0	Y		SOUTEL	DR		1000	1000	16899	0	0000	0	1	Vacant
042049 0000	0.28	12037.31	0	CCG-2	0	Y		SOUTEL	DR		1000	1000	19635	0	0000	4	4	Commercial
083320 0000	14.47	630278.06	1967	RR	43560	N	6443	OLD KINGS	RD		6100	9941	217050	12800	1916	2	4	SFR
083389 0000	1.03	44713.13	4642	RR	43560	Y	6351	KINGS	RD	N	100	103	9270	145200	1954	0	4	Vacant



# **Sewer System Distribution Map**

# WATER LINES



IMESON

SPORTSMAN CLUB

PRITCHARD

1 295

OLD KINGS

SOUTEL

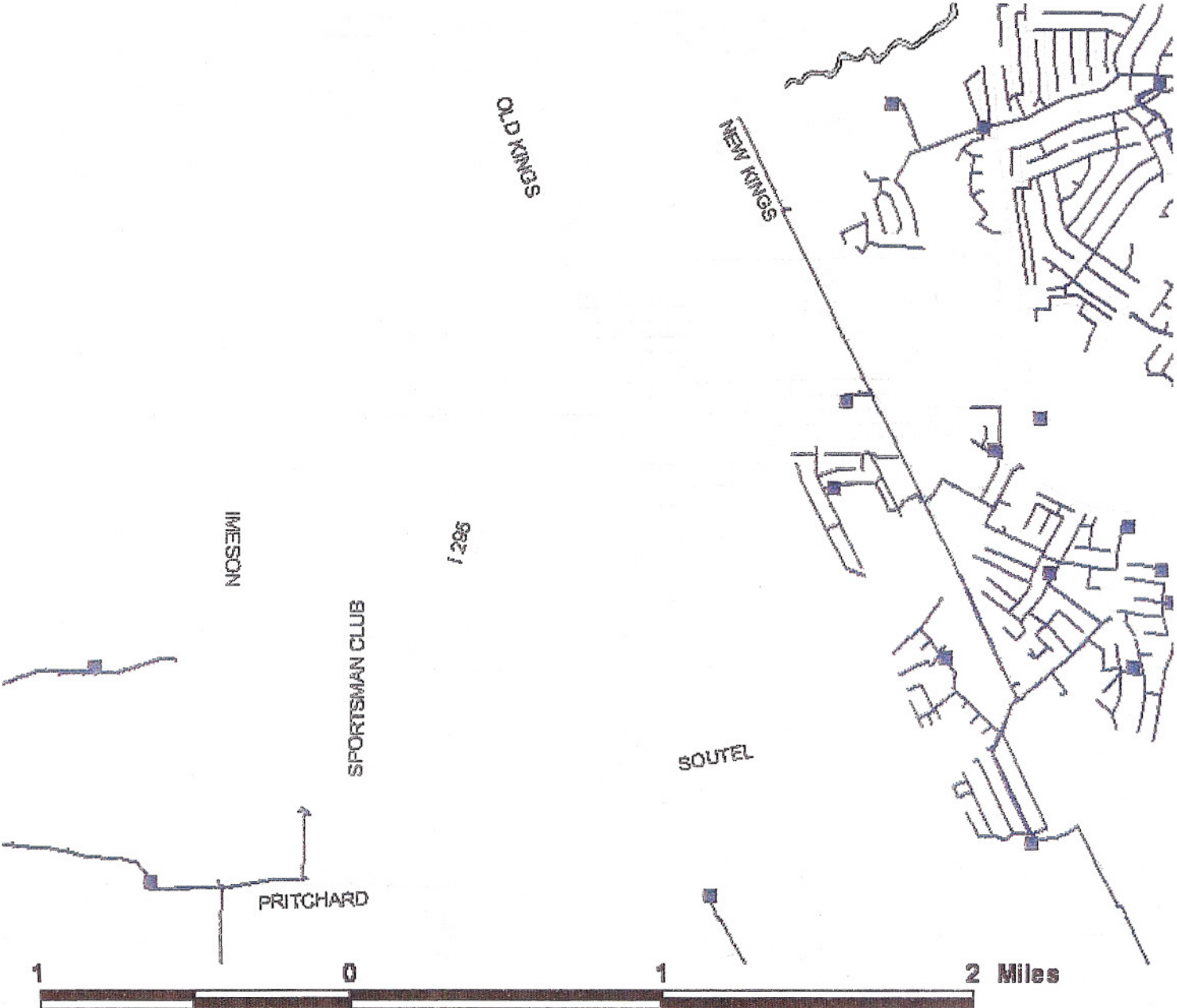
NEW KINGS



**LEGEND**  
Water Lines  
Roads

# **Water System Distribution Map**

# SEWER / PUMP STATIONS



## LEGEND

- Sewer Lines**
- JEA (Blue line)
  - PRIVATE (Red line)
  - UNKNOWN (Black line)
- Sewer Pump Stations**
- JEA (Blue square)
  - PRIVATE (Black triangle)
- Roads**





ATTACHMENT

1 Introduced by the Council President at the request of  
2 Mayor and Council Member Mia Jones:

3  
4

**RESOLUTION 2007-726-A**

6 A RESOLUTION FINDING THAT A NECESSITY EXISTS  
7 FOR THE REHABILITATION, CONSERVATION, OR  
8 REDEVELOPMENT OF BLIGHTED AREAS WITHIN THE  
9 CITY; APPROVING A FINDING OF NECESSITY AND  
10 BLIGHT STUDY FOR THE NEW KINGS ROAD CORRIDOR;  
11 PROVIDING AN EFFECTIVE DATE.

12

13 **WHEREAS**, it is in the public's interest that the City of  
14 Jacksonville carry out a redevelopment project within certain lands  
15 within the City; and

16 **WHEREAS**, the City desires to exercise those powers provided in  
17 Florida Statutes Chapter 163, Part III, to identify and rectify  
18 blighted areas; and

19 **WHEREAS**, the City has previously adopted a Neighborhood Action  
20 Plan for the Lem Turner/Ribault Scenic Drive Neighborhood and is  
21 finalizing the Neighborhood Action Plan for the Sherwood  
22 Forest/Lincoln Villas areas; and

23 **WHEREAS**, the City in conjunction with the Jacksonville  
24 Regional Chamber of Commerce has conducted a Social Compact Study;  
25 and

26 **WHEREAS**, the Plans and the Study demonstrate the need for  
27 nationally-recognized commercial retail services and for enhanced  
28 opportunities for employment; and

29 **WHEREAS**, the Council previously approved Resolution 2006-357

1 approving the blight study and finding a blighted area for the  
2 "Soutel/Moncrief Retail Redevelopment Area"; it is the intent to  
3 combine both Soutel/Moncrief and New Kings Road Corridor into one  
4 community redevelopment plan to better redevelop and utilize  
5 resources in the area; and

6 **WHEREAS**, barriers exist to fulfilling such needs, including  
7 the requirement that there be environmental remediation in the  
8 area; and

9 **WHEREAS**, the City wishes to take action to implement the  
10 tenets of the Plans, and redevelop the areas described below; and

11 **WHEREAS**, section 24.01 of the City's Charter designates the  
12 Jacksonville Economic Development Commission ("JEDC") as the sole  
13 community redevelopment agency for the City according to Part III,  
14 Chapter 163, Florida Statutes; and

15 **WHEREAS**, prior to adoption of the finding of blight, Florida  
16 Statute 163.346 requires a notice to taxing authorities, as that  
17 term is defined, be sent. **Now therefore**,

18 **BE IT RESOLVED** by the Council of the City of Jacksonville:

19 **Section 1. New Kings Road Corridor Area Defined.** It is  
20 hereby found and determined that (i) the area of land lying within  
21 the city limits of the City of Jacksonville identified as **attached**  
22 **Exhibit 1**, which shall be known as the "New Kings Road Corridor  
23 Redevelopment Area", is a slum or blighted area and qualifies as an  
24 eligible project under Chapter 163, Part III, Florida Statutes, and  
25 (ii) the rehabilitation, conservation, and redevelopment of such  
26 area is necessary in the interest of the public health, safety,  
27 morals or welfare of the residents of the City of Jacksonville.  
28 This finding is based on studies formalized in the Finding of  
29 Necessity identified in the New Kings Road Corridor Redevelopment  
30 Area, Community Redevelopment Area Finding of Necessity **revised on**  
31 **file** with the Legislative Services Division.

1           **Section 2.           Priority of Redevelopment.** The first priority  
2 for the redevelopment area is to foster redevelopment of the retail  
3 corridor as a means of community development.

4           **Section 3.           Community Redevelopment Agency.** It is hereby  
5 found and determined that there is a need for a community  
6 redevelopment agency to carry out the redevelopment of the New  
7 Kings Road Corridor Redevelopment Area. Pursuant to section 24.01  
8 of the Charter, the JEDC has been designated as the sole community  
9 redevelopment agency for the City, and therefore the JEDC shall act  
10 as the community redevelopment agency under the provisions of  
11 Chapter 163, Part III, Florida Statutes, for the redevelopment of  
12 the New Kings Road Corridor Redevelopment Area. The Chairman of  
13 the JEDC shall appoint an advisory committee to make  
14 recommendations to the JEDC regarding the implementation of the  
15 area's Community Redevelopment Plan to be adopted by separate  
16 Ordinance. The District Council Member for the area will make  
17 recommendations to the JEDC Chairman for the appointment of the  
18 advisory committee members, and the JEDC Chairman will appoint the  
19 committee members pursuant to Article 24 of the Charter.

20           **Section 4.           Effective Date.** This resolution shall become  
21 effective upon signature by the Mayor or upon becoming effective  
22 without the Mayor's signature.

23 Form Approved:

24  
25 /s/ Debra Braga\_\_\_\_\_

26 Office of General Counsel

27 Legislation Prepared By: Debra Braga

28  
29 G:/shared/LEGISCC/2007/res/cra blight reso for new kings  
30  
31





## **APPENDIX 3**

### **KINGSOUTEL CROSSING LEGAL DESCRIPTION**

# **APPENDIX 4**

## **TAX INCREMENT PROJECTIONS**



# APPENDIX 5

## PRIVATE REALM DESIGN STANDARDS

## Kings Road Corridor Design Guidelines

### Overall Design Objectives

1. To promote architectural and site design standards that enhance the overall aesthetic appearance of the Kings Road Corridor.
2. To promote a strong community identity through visually cohesive design and land development patterning.
3. To support the development of projects that embrace human scale design and encourage social interaction.
4. To promote redevelopment and reinvestment of the Kings Road Corridor.

### SECTION 1 - SITE DESIGN

#### Design Principle

Commercial site designs shall be conceived, first and foremost, to provide services to the residents of the community while, at the same time, blending into the overall fabric of the community. The design of commercial development shall also create safe, attractive and functional places. Quality commercial development begins with good site design.

#### 1.1 Development Analysis and Development Plan Submission Requirements

Proper site development analysis is a fundamental requirement of quality site design. The following guidelines have been created to help facilitate better analysis and, consequently, better commercial site design.

- 1.1.1 All proposed commercial projects shall verify and provide public transportation access points interior to a project site where deemed appropriate by City staff.
- 1.1.2 Where deemed appropriate, commercial site developments shall be designed to provide cross-vehicular and pedestrian access between adjacent commercial development sites, and pedestrian connections to adjacent residential and public school developments.
- 1.1.3 Where possible, commercial site developments shall provide interconnected green spaces with adjacent development sites to encourage cross-pedestrian linkages and create visual cohesiveness between commercial sites.

#### 1.2 Building Massing and Placement

- 1.2.1 Buildings shall, to the greatest extent possible, be ‘massed’ against the primary arterial or collector roadways to create a “street wall” effect.
- 1.2.2 No more than forty percent (40%) of the frontage of a parcel shall be open to parking, stormwater or internal green space.
- 1.2.3 Building placement shall, to the greatest extent possible, screen mass parking areas from primary views, both from an external and internal viewpoint.
- 1.2.4 Building massing and scale shall be designed in proportion to adjacent properties to support the visual and functional effect of the street environment. Building setbacks from back of curb shall be designed to create and facilitate an active pedestrian environment

- 1.2.5 Building organization and placement shall be designed to create and emphasize views and focal points from external roadways.
- 1.2.6 Building organization and placement shall be designed to create internal pedestrian green spaces, plazas or other functional gathering spaces. These spaces shall be designed to support the pedestrian environment.
- 1.2.7 Buildings shall be sited to foster efficient site functioning as well as interconnectivity for pedestrian and vehicular traffic.
- 1.2.8 Outparcel buildings shall be sited to screen large areas of parking from the public view.
- 1.2.9 Typical “strip” shopping centers shall have a variety of elevations with staggered setbacks.

### 1.3 General Development Guidelines

- 1.3.1 Buildings that are developed at roadway intersection corners shall be designed to address the intersection whether or not pedestrian access is provided from the exterior roadway.
- 1.3.2 The maximum building and parking coverage of any site shall not exceed ninety percent (90%) of the site or internal block area, excluding all required landscape buffer areas

## SECTION 2 ARCHITECTURAL DESIGN

### 2.1 Design Principle

Architectural Design is the fundamental building block to the visual and physical integrity of our communities.

### 2.2 Design Goals

- 2.2.1 The architecture of a commercial project shall be designed to influence and encourage social interaction while creating a safe, aesthetic and functional environment.
- 2.2.2 The design shall focus on both physical and functional permanency in its physical building placement as well as its material structural design.
- 2.2.3 The design shall contribute to the quality of the overall community in which it is placed rather than “stand out” in the community.
- 2.2.4 The design to serve as an identifier for a project or individual commercial business.
- 2.2.5 The design shall take into account rehabilitation of a building to allow for adaptive reuse in the future.

### 2.3 Architectural Facade Massing

Architectural Massing is “the overall composition of the exterior of the major volumes of the building, especially when a structure has major and minor elements”.

- 2.3.1 The design of commercial architecture must take into account that architecture is viewed three-dimensionally and not as a flat elevation facade from the street side only.

- 2.3.2 All commercial buildings shall reflect the actual floors within the building through use of window location, facade breaks, facade setbacks, balconies, etc.
- 2.3.3 No multi-story building shall have blank walls facing any public street, neighborhood or other internal commercial areas that can be viewed by the public.
- 2.3.4 For all multi-story buildings, the building elevations shall clearly reflect a bottom (1st) floor and its representative interior height, a middle ground consisting of all floors above the first floor and a visually pronounced building top that consists of a defined building facade.
- 2.3.5 The design of new and retrofitted commercial buildings shall take into account the immediate off-site surrounding structures, and provide mass, height and building elevations, so as to create substantially compatible scale with adjacent structures.
- 2.3.6 Building massing shall be designed to address pedestrian scale by reducing the scale and proportion of the visual “monolithic box” through variations in wall heights, facade articulations and varied roof planes and pitches.

#### 2.4 Architectural Facade and Articulation Design

- 2.4.1 Building facades shall be designed to provide visual interest through detail and ornamentation that is viewed at both the immediate pedestrian level as well as from a distance.
- 2.4.2 Building facades shall provide vertical expression of architectural or structural bays through a change in plane that shall be at a minimum of eighteen inches (18”) in width for single story structures and a minimum of twenty four inches (24”) in width for all structures exceeding two (2) stories.
- 2.4.3 Plane changes can be accomplished through the use of reveals, projecting ribs and/or offsets in the building facade.
- 2.4.4 All ground floor facades that face a public street or internal private streets, shall have arcades, display windows, entry areas, awnings or other such features along a minimum of sixty percent (60%) of the horizontal length of the facade.
- 2.4.5 All parapets and roof planes that conceal flat roofs must extend around all sides of a building that is visible from any public street, internal public drive, abutting adjacent commercial office buildings, retail stores or residential areas.
- 2.4.6 The height of the concealing wall shall fully screen all HVAC roof mounted equipment.
- 2.4.7 All local retail stores and commercial businesses that face a public street, or internal private streets, shall have transparent windows for at least fifty percent (50%) of the commercial business frontage.
- 2.4.8 Building facade fenestration and roof planes shall be designed to fully wrap and continue the visual line to the architecture on all sides of the building that can be viewed by the general public. If a facade is not visible to public view, either presently or potentially in the future, detailed fenestration of the building facade is not required but is encouraged.
- 2.4.9 Where architecturally and historically appropriate, windows should be designed to have shutters, window surrounds or casings, or window headers.

- 2.4.10 All doors shall be recessed in the facade elevation to provide an identifiable entry point for each customer entry.
- 2.4.11 All doors shall be architecturally and historically compatible to the period or style of the primary building architecture for which it is providing entrance. The use of decorative door surrounds is strongly encouraged.
- 2.4.12 Buildings shall be designed and constructed of materials that ensure the sustained longevity of the structure.
- 2.4.13 Exterior built-up veneer facades constructed of plywood or plywood-like materials that are surface-covered by only stucco or plaster shall not be permitted. This form of construction is highly susceptible to rot and deterioration due to poor sealing of materials and moisture build-up in the layers.
- 2.4.14 All materials proposed for exterior construction shall be treated and rated for exterior construction.
- 2.4.15 No structural or ornamentation elements that come in contact with the natural ground or finish grade of a paved surface shall be constructed of plywood or strand-board composite materials.
- 2.4.16 Proper moisture barriers and sealants shall be required where architectural elements are exposed to rain, drainage patterns or irrigation water sources.
- 2.4.17 No building facade, window, or door shall be constructed of any material that may cause visual glare or reflection of light back onto any public roadway or internal access drive.
- 2.4.18 All commercial buildings shall have clearly defined and visible customer entrances featuring no less than three of the following:
- Canopies or Porticos
  - Arcades
  - Raised cornice parapets over the doorways
  - Peaked roof forms
  - Arches
  - Display windows
- 2.4.19 The orientation, site placement, primary entry locations, pedestrian accessways and physical building architecture shall take into account and address the physical needs of the pedestrian in the design of all exterior elements, scale and coverage with respect to providing protection from the intense solar exposure and substantial seasonal rains.
- 2.4.20 The design of the architectural facades and roof planes shall not be designed as a physical background for signage.

## **2.5 General Architectural Design Standards**

- 2.5.1 Buildings shall be constructed of materials that minimize exterior maintenance and operating costs.
- 2.5.2 The design of all buildings shall take into account the future re-adaptability of other uses and shall not be designed for one (1) sole use.
- 2.5.2 Building floor heights, mechanical equipment heights and fire protection methods shall be designed to allow for conversion to other uses.



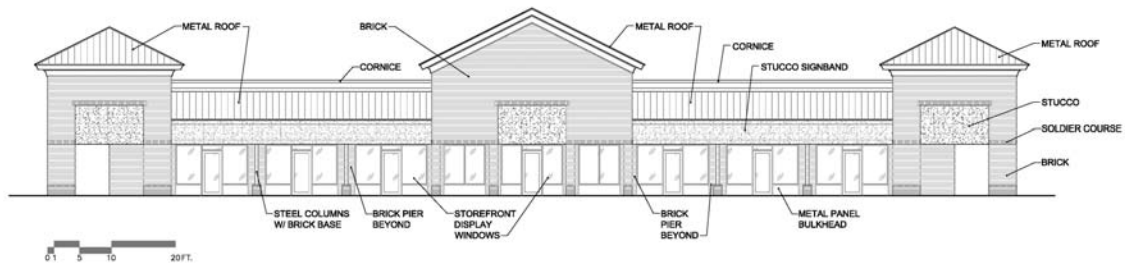
- 2.5.3 For all commercial structures that face any public rights-of-way or are the primary front facade of a structure, all roof drainage shall be designed to be internal to the structure.
- 2.5.4 Exterior roof drainage systems that are not visible from any public rights-of-way or pedestrian view are permitted to drain directly onto grassed areas or paved vehicular surface areas.
- 2.5.5 Ground areas drains shall be required to collect and direct water to the master stormwater collection system of for the project.
- 2.5.6 All buildings shall be designed to completely conceal all mechanical HVAC equipment, pump structures, cooling towers, cable/dish structures or any other mechanical equipment.
- 2.5.7 The application of “Green Design” standards in construction as well as the use of recycled materials in the physical construction of new buildings is strongly encouraged.
- 2.5.8 Buildings shall be designed to express their primary uses (retail, residential, office, etc.) with respect to scale of facade elements, building facade articulation and step-backs, window and door placements and organization of pedestrian access points.

# KingSoutel Crossing (Corridor) Community Redevelopment Plan

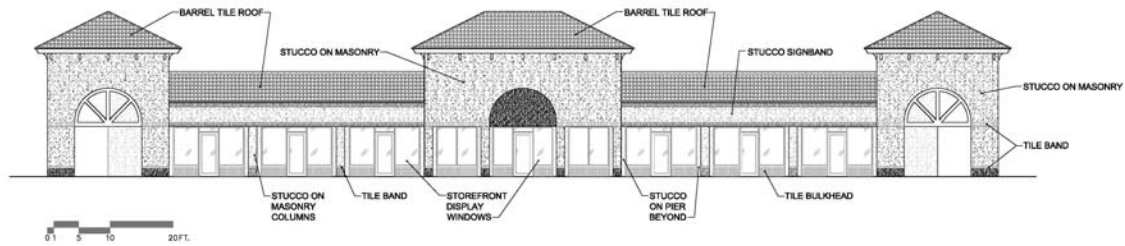
## Existing



## Metal Roof



## Barrel Tile Roof



## Traffic Circulation